



Warrenton Planning Commission
AGENDA

December 9, 2021 | 6 PM | City Hall – Commission Chambers

*****The meeting will be broadcast via Zoom at the following link*****

<https://us02web.zoom.us/j/89594092173?pwd=VG5sMFFTVExqTWI1dXVXSTBFbWw2UT09>

Meeting ID: 895 9409 2173

Passcode: 612659

Dial in number: 253-215-8782

1. Attendance
2. Flag Salute
3. Public Comment Period on *Non-Agenda* Items
4. Approval of minutes of November 11, 2021
 - *Action Item: Motion to Adopt*
5. **PUBLIC HEARING:** Site Design Review SDR-21-7 and Conditional Use Permit CUP-21-1
 - Applicant: Craig Gilbert, PM Design
 - Proposal: To construct a new 2,100 square foot building, drive through, and associated site improvements to place a Popeye's Louisiana Kitchen at 1771 SE Ensign Lane on Tax Lot 81027CB01901.
 - *Action Item: Recommendation to approve with conditions*
6. **PUBLIC HEARING:** Appeal AP-21-1 of Administrative Decision on SIG-21-7
 - Applicant: Mike Moha, Warrenton-Hammond School District
 - Proposal: Appeal of decision to deny sign application SIG-21-7.
 - *Action Item: Recommendation to deny the appeal*
7. **DISCUSSION ITEMS:**
 - Variances and Setbacks
8. Staff Announcements & Project Updates
9. **Next Meeting:** January 13, 2022

MINUTES
Warrenton Planning Commission
November 4, 2021
6:00 p.m.
Warrenton City Hall - Commission Chambers
225 S. Main
Warrenton, OR 97146

Chair Mitchell called the meeting to order at 6:01 p.m. and led the public in the Pledge of Allegiance.

Commissioners Present: Chair Paul Mitchell, Mike Moha, Chris Hayward (via Zoom), Ken Yuill (via Zoom), Kevin Swanson (via Zoom, left at 8:01 p.m.), and Lylla Gaebel (via Zoom)

Absent: Commissioner Christine Bridgens

Staff Present: Planning Director Scott Hazelton, City Manager Linda Engbretson (via Zoom), Building Official Van Wilfinger, and Secretary Rebecca Sprengeler

PUBLIC COMMENT ON NON-AGENDA ITEMS - None

APPROVAL OF MINUTES

A. Planning Commission Meeting Minutes – 9.9.21

Commissioner Yuill made the motion to approve the minutes as written. Motion was seconded and passed unanimously.

Mitchell – aye; Moha – aye; Hayward – aye; Yuill – aye; Swanson – aye; Gaebel - aye

PUBLIC HEARING

Open Public Hearing: Chair Mitchell opened the public hearing in the matter of a fill violation at 135 SW 1st Street. Formalities followed. Commissioner Yuill noted he visited the site but feels it will not affect his judgement. Chair Mitchell stated the same.

Staff Report: Planning Director Scott Hazelton gave a PowerPoint presentation about the violation. He stated the property owner of record is Kevin Gramson and the permittee is Loren Gramson. The first stop work order was mailed certified on August 18. A second notice was mailed certified on September 2. Loren Gramson was given notice of the meeting by letter on October 28. He stated the first and second notices came from the building official in the absence of a Planning Director. He noted the violation includes lack of requested stormwater plan, lack of erosion control measures and erosion control plans, and work beyond the scope of the submitted application. He outlined options for the planning commission and noted Warrenton Municipal Code (WMC) 15.04.260 regarding violation penalties.

Questions of Staff: None

Applicant Presentation: Mr. Gramson noted he takes issue with the point of work beyond the scope of the permit. He stated he had a permit to fill, remove fill, and grade. He explained that more fill was placed than he thought was needed. He contacted building department for another permit, then received a stop work order. He stated he has not done anything since. He noted during a meeting last week he received notice of this meeting. He was informed the second permit was not approved, but he had already finished the work. He noted the hand drawn map he submitted with the application. He stated the property has become an eyesore that he would like to get rid of. He feels he made the effort to do something positive for the city. He discussed his development work in Warrenton and lack of affordable housing. He stated he did not have the permit he thought he did. He asked the commission to consider his honesty in contacting the city for a second permit. He stated he hired an engineer and they have discussed addressing the issues. He has put in a silt fence, wattles, and bales of hay. He stated no water is impacting anyone. He noted there is a pool in the middle of the property to catch water to drain down. He is hoping to burn the house down with the assistance of the fire department then reduce the height of the fill. He stated all that they need is reasonable time to get the physical work done. He noted a drainage pipe in the city right-of-way and two catch basins that he protected. He stated he wants to get this done right.

Questions of Applicant: Commissioner Yuill asked how much fill the first permit allowed. Mr. Gramson thinks he asked for 250 cubic yards. He noted when he realized there was more, he contacted the building department for an additional permit. He discussed the permit applications. Commissioner Swanson asked for a timeline to comply. Mr. Gramson estimated between 30 and 60 days to get plans from his engineer. He explained he can do work that Public Works Director Collin Stelzig suggested as soon as he can get equipment. Commissioner Yuill noted his research on the area and asked if Mr. Gramson's property is affected by surrounding property's drainage. Mr. Gramson stated yes and discussed drainage from two houses and an apartment building. Commissioner Gaebel asked why the city was not contacted after he received the notices. Mr. Gramson noted he visited the building department while the building official was out of office and a meeting was held last week, but he thought he had the proper permits. Commissioner Gaebel asked why contact was not made in August. Mr. Gramson noted there has been no equipment on the property since receiving the stop work order.

Public Testimony: Van Wilfinger noted the permit was pulled for approximately 250 cubic yards and stated it did not seem like it would be an issue. He drove by the site and witnessed an excessive amount of material. He sent a stop work notice. After discussion with Mr. Wilfinger, Mr. Gramson submitted revised plans to the building department. Mr. Wilfinger then received communication from Mr. Stelzig that the amount of fill would require a stormwater report and civil plan to address stormwater drainage. Mr. Wilfinger instructed Mr. Gramson to hire someone to provide a stormwater report to stabilize the site. He noted a second stop work notice was mailed certified and not signed for. It was returned then resent. He noted a recent meeting with Mr. Gramson to discuss issues and provide notice of the hearing. Commissioner Yuill asked how much fill is on the property. Mr. Wilfinger explained he is new to the city and not as familiar with the residential areas as his work is focused on commercial projects. He relied on other staff for estimated amount of material. Commissioner Yuill made comments about

determining the amount fill material in order to know how much to remove. It was noted the fill material is predominantly sand and other soil. Commissioner Hayward asked if the fill was monitored. Mr. Wilfinger stated there was not somebody monitoring on behalf of Mr. Gramson. Mr. Hazelton clarified that what is being looked at is the erosion and stormwater plans have not been submitted as requested by staff since August. Mr. Hazelton noted there have been negative impacts on adjacent properties reported and further emphasized the need to review a stormwater plan. Chair Mitchell asked what was proposed by Mr. Stelzig. Mr. Hazelton noted Mr. Stelzig has discussed the project with the engineer, but the city still needs the required plans to be submitted. There was brief discussion about removing the excess fill. Brief discussion followed about stormwater plans are required for all fill permits. City Manager Linda Engbretson (via Zoom) clarified an issue in the code between fill and stockpiling. Discussion followed about stockpile versus fill. Commissioner Yuill noted concern that the permits were not in the meeting packet. Ms. Engbretson noted they want people to be in compliance by obtaining erosion control plans; they cannot ignore code requirements. Mr. Hazelton restated the need for erosion control and stormwater plans. Discussion continued. Chair Mitchell would have liked more documentation.

Applicant Rebuttal: Mr. Gramson noted copies of the original permit application for 250 yards. He estimated about 600 yards of actual fill. He stated he contacted staff prior to the first stop work order. He noted the fill material was sand and one load of dirt. He noted discussion with a neighbor about drainage in the area in the past. He discussed the person he believes reported negative drainage impacts and wants to know who reported it. He stated he was told by Mr. Wilfinger to grade as quick as he could. He noted his permit was for grading, removal, and fill; there was nothing about stormwater. He noted the stormwater report will be required before any building, so he might as well do it now. Mr. Gramson is working with his engineer to address the drainage as instructed by Mr. Stelzig. He has talked with a neighbor about draining across his property and he noted a culvert issue he would like the city to address. He explained the extra material could be used for fill once the house is burned. Commissioner Gaebel asked about the delayed response to the first notice requesting erosion and stormwater control. Mr. Gramson explained when he received his permit, nothing was mentioned about it. He briefly discussed his original site map submittal. He stated stormwater was never a part of the permit and he stopped work immediately. He had already notified the city of the excess fill and noted he was unsure of the fill amount. Commissioner Hayward asked how fill was brought to the property. Mr. Gramson stated dump truck. Commissioner Hayward asked about the size. Mr. Gramson stated it varied and they were not worried about how much was on each truck. He explained lack of monitoring at first and noted approximate overage. Brief discussion followed about the excess fill amount. Commissioner Swanson asked what will happen to come into compliance. Mr. Gramson noted hiring an engineer and discussed a swale and water storage. Commissioner Swanson asked about a timeline for the engineer to address the issues and provide a final report. Mr. Gramson estimated 30 to 60 days and noted they will do what the engineer suggested. He also stated when it said stop work, they were already done with the fill, and he was instructed to grade as quickly as possible. He noted they planted grass and the material is pretty packed.

Close Public Hearing: Chair Mitchell closed the public hearing.

Deliberation of Commissioners: Commissioner Yuill noted the ditch has not been cleaned out

and noted concern about impact on surrounding properties. He asked about a timeline for the city to clean the ditch. Mr. Hazelton noted public work's rotating ditch maintenance schedule. Brief discussion followed. There was brief discussion about the property address. Mr. Hazelton clarified the person Mr. Gramson alluded to previously did not make the report about negative impacts. He also noted the engineer was retained early this week. Commissioner Swanson asked why the fill amount was guessed and not measured or adhered to. Mr. Hazelton noted staff does not have the capacity to monitor fill. Discussion continued about stormwater reports being required for fill permits. Ms. Engbretson briefly discussed WMC regarding grading. She noted they are updating the stormwater and drainage plans that may be addressed at a joint meeting with the city commission. She feels it has gotten to the point where submitting a stormwater plan should be required with any fill permit. Commissioner Hayward commented on the issuance of a permit without a stormwater plan. Ms. Engbretson restated the need for the stormwater report that has been requested since August. Discussion followed about the stop work order. Mr. Hazelton stated error on staff's part does not absolve Mr. Gramson from following the WMC. Ms. Engbretson discussed communication issues and a shift to written communication. She noted the issue is not the fill amount, it is the requested plans in order to move forward and mitigate impacts. Discussion followed about the meeting notice. Mr. Hazelton confirmed the first stop work notice was signed for on September 20 and sent by email as well. Commissioner Gaebel noted Mr. Gramson has had the request for stormwater and erosion control since August. Commissioner Swanson feels the stop work order is not clear. Ms. Engbretson clarified it is to stop work on the property and work on submitting plans to the city. Commissioner Yuill discussed his concerns about what can be done in the short term to address negative impacts on the south neighbor. Brief discussion followed. Commissioner Moha noted to be in compliance, a report must be supplied. He noted removing fill and making suggestions to address the issues are not relevant to this decision. Mr. Hazelton noted a stormwater plan will address the negative impacts. Commissioner Gaebel noted Mr. Gramson is not in compliance but has hired an engineer to bring him into compliance. She suggested finding he is not in compliance and a no-fine 45 to 60-day grace period. Discussion followed.

Commissioner Hayward made the motion for 60 days with no fine and impose a fine of \$1,000 if that is not reached within that 60 days. Motion was seconded and failed.

Commissioner Moha believes there was no intentional ill will on either side. He agrees with 60 days and a \$500 fine.

Mitchell – nay; Moha – nay; Hayward – aye; Yuill – nay; Swanson – aye; Gaebel – aye

Discussion followed about the fine and timeframe.

Chair Mitchell made the motion that they allow Mr. Gramson 60 days. If within 60 days, Mr. Gramson does not reach a conclusion satisfactory to the city, then we impose a \$500 a day fine that is retroactive to today's date. Discussion followed.

Commissioner Hayward made a motion to strike the retro active portion of the chairman's motion and make it \$500 per day after a 60-day period to get this work done. Motion was seconded and passed unanimously.

Mitchell – aye; Moha – aye; Hayward – aye; Yuill – aye; Swanson – aye; Gaebel – aye

Open Public Hearing: Chair Mitchell opened the public hearing in the matter of a fill violation at 115 SW 1st Street. Formalities followed. No conflicts of interest were declared. Commissioner Yuill noted he visited site but feels it will not affect his decision. Chair Mitchell stated the same.

Staff Report: Mr. Hazelton presented his staff report for the fill violation at 115 SW 1st Street owned by Rick Newton. He noted the first stop work order was mailed certified and signed for in August. The second was mailed certified and signed for in September. The hearing notice was mailed in October. He outlined the fill violation noting a request for stormwater plans and erosion control plans. He clarified there was no application submitted; there was communication with staff about stockpile of clean gravel. The material was different than and exceeded what was originally discussed. He noted options for the commission as outlined in the presentation.

Questions of Staff: Commissioner Swanson asked about the material. Mr. Hazelton clarified it was mixed material from underneath a road, not clean gravel as originally discussed with staff. Commissioner Yuill asked if there is runoff going onto other properties. Mr. Hazelton stated he has received no complaints regarding this property about additional runoff at this time.

Applicant Presentation: Mr. Newton noted Patricia Caples is co-owner of the property. He noted his long-term friendship with Commissioner Yuill and stated it does not affect them to disagree. Mr. Newton noted past issues of flooding and a roof collapse. He noted his frustration and explained the situation. Ryan Lampi with Big River requested to place material from under a road on Mr. Newton's property. Mr. Newton discussed this with the building department and was told he did not need a permit. Mr. Newton noted he originally estimated 150-250 yards of fill. When this amount was exceeded, he contacted the building department and was told he was fine. Additional material was placed. Mr. Newton visited the building department again to discuss the issue. He met with Mr. Stelzig to discuss plans provided by Mr. Lampi to address the first stop work notice. Mr. Newton estimated there was 590 yards, while Mr. Lampi estimated about 500. Mr. Newton brought the plans to the building department. He was asked for bigger plans and directed to start with public works. Mr. Newton expressed confusion about what the city wanted to address the stop work order. He noted meetings with Mr. Stelzig to clarify what the city and noted engineered plans were requested. He feels this could have been avoided and noted his frustration. He requested Ms. Caples be added as co-owner. He noted observing decreased runoff from his property. He further noted his frustration with the city. He stated Mark Mead cannot do the engineering until January. Geoff Liljenwall cannot do the engineering until November. Mr. Newton noted he and his engineer do not know what the city wants.

Questions of Applicant: Commissioner Moha asked for clarification if there was no permit. Mr. Newton confirmed. Mr. Hazelton read the fill request email from Mr. Newton dated July 19. Mr. Hazelton stated stockpiling of clean material does not require a permit. Commissioner Swanson asked how long the material will be on the property and if there plans for it. Mr. Newton explained it could be used as surcharge as he plans to build eventually. He stated he intends to put a French drain completely around the property. He is unsure of a timeline. He explained his frustration and noted he thought he was doing the right thing. Ms. Caples explained Big River requested to dump material removed from the street project onto their property, saving the city

money on the project. She noted they were told they did not need a fill permit. They contacted the contractor after the stop work order. Mr. Newton stated he was not running the job, Mr. Lampi was. He restated he does not know what the city wants and noted Big River is a licensed and bonded contractor that he believes can provide the plans. Commissioner Swanson asked why Mr. Lampi is not in attendance to testify. Mr. Newton stated he did not know he needed to bring him. Commissioner Gaebel noted the stop work notices that requested erosion and stormwater plans. Ms. Caples stated it was handed over to Big River; it's their job. Mr. Newton noted plans from Mr. Lampi he understands to be stormwater and erosion control plans. Commissioner Gaebel stated it is the applicant's responsibility to see that work is done. Ms. Caples noted the plans from Mr. Lampi. Commissioner Gaebel asked when the plans were submitted to the city. Mr. Newton noted he is not sure. He did not know he needed an engineer. He thought Big River was qualified. He intends to hire Mark Mead. He stated he tried to do the right thing but received bad direction.

Public Testimony: Ms. Engbretson noted she cannot verify if the plans in question have been received by staff. Commissioner Moha asked if Mr. Hazelton has seen the plans and if they would be acceptable. Mr. Hazelton noted he has seen them; they are not acceptable. Mr. Wilfinger noted an email from Mr. Newton addressed to himself and Mr. Stelzig regarding stockpile of gravel. He noted the WMC does not require a permit for stockpile of clean material. He explained the plans in question are not a stormwater report. Mr. Wilfinger stated he had not reviewed the plans yet because they needed to be reviewed by public works. He noted there was no permit because what was proposed did not require a permit. Mr. Newton did respond to the first stop work notice. He was directed to work with Mr. Lampi to develop acceptable plans to submit to Mr. Stelzig for approval. Mr. Wilfinger noted nothing acceptable was provided so a second notice was sent. Commissioner Yuill asked if there is runoff onto other properties. Ms. Engbretson noted there is not much concern about runoff on this property. The concern is treating everyone the same. The report is still required by WMC. Brief discussion followed. Mr. Hazelton added they are not addressing the contents of the reports, they are addressing the fact the reports were never submitted. Commissioner Swanson asked if Mr. Lampi would have the proper paperwork to support the plans in question. Ms. Engbretson noted as of now her communication with staff is that nothing has been received.

Applicant Rebuttal: Mr. Newton stated he does not believe, in his email, he ever said he was going to stockpile clean gravel on the lot. Commissioner Gaebel asked to have the email read again. Mr. Hazelton read the full email from Mr. Newton and the response from Mr. Wilfinger dated August 12, 2021. There was brief discussion about a possible response from Mr. Stelzig. Chair Mitchell asked how long it will take to get a report. Mr. Newton noted Mr. Liljenwall estimated mid to end of November. Brief discussion followed. Commissioner Yuill asked if the plans prepared by Mr. Lampi were submitted to public works. Mr. Newton stated he has no idea, noting several visits to public works.

Close Public Hearing: Chair Mitchell closed the public hearing.

Deliberation of Commissioners: Commissioner Yuill noted a lot of hearsay and would like to hear from Mr. Stelzig. He suggested extending the hearing until next month. Commissioner Swanson agreed and would like to hear from Mr. Lampi. He feels there is not enough

information to make an informed decision. Commissioner Gaebel noted Mr. Wilfinger's email was very specific about what was expected. Commissioner Hayward noted there is no issue getting the engineering and suggested providing reasonable time until January. Commissioner Moha noted they cannot force people to come answer questions. Mr. Hazelton noted additional email communication. Chair Mitchell suggested giving Mr. Newton until January and having the city provide an update at that meeting. Commissioner Gaebel feels they are being inconsistent between the two violations. Mr. Hazelton noted an email from Mr. Stelzig that outlines what is needed.

Commissioner Gaebel moved that we give 60 days grace period from tomorrow's date and on the 61st date, that we levy a fine of \$500 per day if it has not been cured and do it in conjunction with the city's requirements. Motion was seconded and passed with majority in favor.

Mitchell – aye; Moha – aye; Hayward – aye; Yuill –nay; Swanson – aye; Gaebel - aye

STAFF ANNOUNCEMENTS & PROJECT UPDATES

Mr. Hazelton noted he is happy to have the first meeting under their belts. He asked for feedback on the presentation. Commissioner Gaebel thought it was helpful. Commissioner Hayward noted there are often documents in the record that are difficult to see. Commissioner Yuill would like a camera to see physical documents while on Zoom. Commissioner Swanson noted he needed to leave the meeting for a work meeting. Mr. Hazelton discussed several recent variance applications to build into the setbacks. He asked for guidance on whether or not to continue allowing variances to extend into the setbacks and if he should research amending the setbacks. He noted this will be discussed further in a joint meeting. He further explained the purpose of a variance is for hardships due to confines of the zoning code and the property, not a desire for a bigger house or garage. Commissioner Yuill asked Mr. Hazelton to send an email with questions that will be discussed at the joint meeting. Mr. Hazelton noted side yard setbacks are for emergency vehicle access. Mr. Hazelton feels variances should be difficult to obtain. He also discussed possible changes to the development review process regarding conditions of approval.

There being no further business, Chair Mitchell adjourned the meeting at 8:07 p.m.

Next Meeting: December 9, 2021

APPROVED:

ATTEST:

Paul Mitchell, Chair

Rebecca Sprengeler, Secretary



December 1st, 2021

To: Warrenton Planning Commission
From: Scott Hazelton, Planning Director
Re: Site Design Review Application SDR-21-7 and CUP-21-1

BACKGROUND & STAFF RECOMMENDATION

On September 17, 2021, Craig Gilbert of PM Design submitted the application listed above on behalf of the property owner North Cost Retail LLC to construct a new 2,100 square foot building and associated site improvements to place a Popeye's Louisiana Kitchen at 1771 SE Ensign Lane on Tax Lot 81027CB01901. The property is adjacent to Walmart and the Fibre Federal Credit Union, in the General Commercial (C-1) Zone.

The application was deemed complete on September 22, 2021. Notice for a Type III public hearing was mailed on November 2, 2021 and was published in the Columbia Press on November 12, 2021. Multiple public comments have been received in opposition due to traffic concerns. Written comments are attached. The primary purpose and intent of the staff report is to make findings on whether the application satisfies criteria and standards specified in the Warrenton Municipal Code (WMC). The City maintains consistency with Comprehensive Plan provisions through the enactment and application of land use regulations. Hence, the staff report does not make specific findings on compliance with Comprehensive Plan Policies or provisions, but adheres to the WMC chapter order listed below.

The applicant has submitted plans that satisfy the WMC and staff at this point of review. Staff recommends this application favorable for approval to the Planning Commission. Conditions of approval will require the applicant to continue to meet WMC and Public Works requirements before submittal for building plans.

The staff report will provide a breakdown of the applicable WMC and if the applicant addressed the criteria to staff's satisfaction. Any items not deemed acceptable have been addressed with new submittals from the applicant or as a condition of approval. Please review this code analysis and staff report in full before the meeting.



Figure 1: Popeyes site outlined in the C-1 Zone

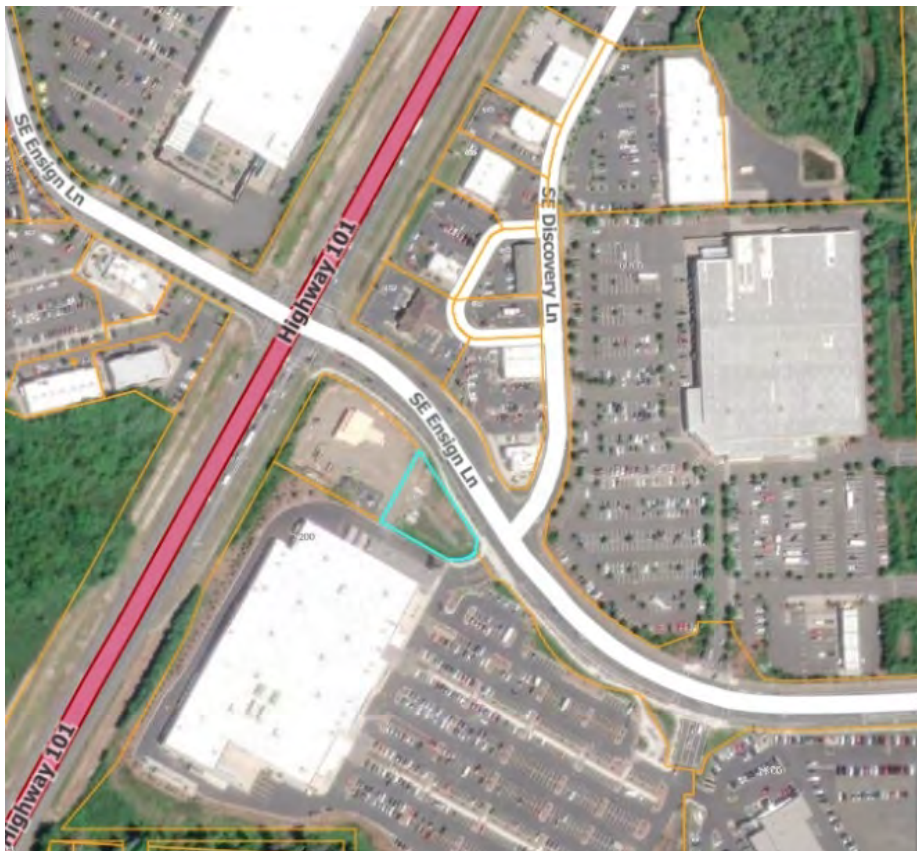


Figure 2: Popeye's Site, Aerial

APPLICABLE CRITERIA AND STANDARDS

Chapter 16.40 General Commercial (C-1) District
Chapter 16.120 Access and Circulation
Chapter 16.124 Landscaping, Street Trees, Fences, and Walls
Chapter 16.128 Vehicle and Bicycle Parking
Chapter 16.136 Public Facilities Standards
Chapter 16.140 Stormwater and Surface Water Management
Chapter 16.144 Signs
Chapter 16.152 Grading, Excavating, and Erosion Control Plans

FINDINGS

Chapter 16.40 General Commercial (C-1) District

16.40.020: Permitted Uses

Staff Finding: This criterion is met.

- Eating and drinking establishments are a permitted use:

16.40.030: Conditional Uses

Staff Finding: This criterion has been met with the submittal of a conditional use permit.

- New drive through/drive-up facility or substantially improved as defined by 25% of assessed value.

16.404.040 Development Standards

Staff Finding: This criterion is met.

- There are no setback requirements for a commercial use that does not abut a residential use.
- This property does not abut a residential use.
- The proposed building is shorter than 45 feet.

Staff Conclusion for Chapter 16.40: General Commercial District

Staff finds that the criteria of Chapter 16.40 General Commercial District has been met by the applicant. At this time there are no concerns regarding this chapter of Warrenton Municipal Code from staff.

Chapter 16.120 Access and Circulation

16.120.020.C Access Permit Required

Staff Finding: This criterion is met.

- Applicant is proposing to use existing and private access.

16.120.020.D Traffic Study Requirements

Staff Finding: This criterion is met.

- The applicant submitted a traffic study prepared by Lancaster Engineering, which is

attached in the submitted application.

16.120.020.H Number of Access Points

Staff Finding: This criterion is met.

- Applicant is proposing to use existing and private access.

16.120.020.I Shared Driveways

Staff Finding: This criterion is met.

- Applicant is proposing to use existing and private access.
- Condition of Approval #4: Include a copy of easement for access between Popeyes and Walmart.

16.120.020.L Fire Access and Circulation

Staff Finding: This criterion is met.

- Applicant addressed comments from pre-application meeting.

16.120.020.M Vertical Clearance

Staff Finding: This criterion is met.

- Applicant proposed acceptable vertical clearance.

16.120.020.N Vision Clearance

Staff Finding: This criterion is met.

- Applicant proposed acceptable vision clearance.

16.120.020.O Construction

Staff Finding: This criterion is met.

16.120.030 Pedestrian Access and Circulation.

Staff Finding: This criterion is met.

- Pedestrian circulation is provided by sidewalks connecting the parking areas with the building. Design concerns will be addressed by public works during the civil review.

Staff Conclusion for Chapter 16.120: Vehicular Access and Circulation

Staff finds that the criteria of Chapter 16.120 Vehicular Access and Circulation has been met by the applicant. At this time there are no concerns regarding this chapter of Warrenton Municipal Code from staff. Conditions of approval provided by the Public Works department regarding this chapter will be addressed before submitting for building plans.

Chapter 16.124 Landscaping, Street Trees, Fences, and Walls

16.124.070 New Landscaping.

Staff Finding: This criterion has been met.

- The applicant has provided a landscaping plan that is satisfactory.

16.124.080 Street Trees

Staff Finding: This criterion has been met.

- The applicant has provided a landscaping plan that is satisfactory.

Staff Conclusion for Chapter 16.124: Landscaping, Street Trees, Fences, and Walls

Staff finds that the criteria of Chapter 16.128: Landscaping, Street Trees, Fences, and Walls has been met by the applicant. At this time there are no concerns regarding this chapter of Warrenton Municipal Code from staff. Conditions of approval provided by the Public Works department regarding this chapter will be addressed before submitting for building plans.

Chapter 16.128 Vehicle and Bicycle Parking

16.128.030 Vehicle Parking Standards.

Staff Finding: These criteria are met.

- The applicant has proposed 17 parking spots The WMC requires 12 spots for a 48 seat restaurant.

16.128.040.A Bicycle Parking Requirements.

Staff Finding: This criterion is met.

- The applicant has proposed 2 bicycle spots. This satisfies the criteria.

Staff Conclusion for Chapter 16.128 Vehicle and Bicycle Parking

Staff finds that the criteria of Chapter 16.128 Vehicle and Bicycle Parking has been met by the applicant. At this time there are no concerns regarding this chapter of Warrenton Municipal Code from staff. Conditions of approval provided by the Public Works department regarding this chapter will be addressed before submitting for building plans.

Chapter 16.136 Public Facilities Standards

16.136.020 Transportation Standards

Staff Finding: These criteria are met or will be met during construction.

16.136.040: Sanitary Sewer and Water Service Improvements

Staff Finding: This criterion will be met during civil engineering review.

16.136.050 Storm Drainage Improvements

Staff Finding: This criterion will be met by Condition of Approval #3

16.136.060: Utilities

Staff Finding: This criterion has been met

- The applicant has proposed sufficient utility easements.

16.136.070 Easements

Staff Finding: This criterion has been met.

- The applicant has proposed sufficient easements.

Staff Conclusion for Chapter 16.136 Public Facilities Standards

Staff finds that the criteria of Chapter 16.136 Public Facilities Standards has been met by the applicant. At this time there are no concerns regarding this chapter of Warrenton Municipal Code from staff. Conditions of approval provided by the Public Works department regarding this chapter will be addressed before submitting for building plans.

Conclusion, Recommendation, & Conditions of Approval

Conclusion and Recommendation

The findings above demonstrate that the proposed design of the Popeye’s Louisiana Kitchen development is generally consistent with the C-1 zoning district development standards and the design standards of the development code. Staff recommends that the Planning Commission approve SDR-21-7 and CUP-21-1 subject to the following conditions of approval:

Conditions of Approval

1. Submit grading permit
2. Submit erosion and sediment control plan
3. Submit an engineered stormwater report
4. Provide a copy of the easement providing access between Walmart and Popeye’s.
5. Abide by all testimony and plans submitted.
6. Pay any remaining fees.

Suggested Motion:

I motion to approve the Site Design Review SDR-21-7 and CUP-21-1 for the new Popeye’s Louisiana Kitchen Facility, located on Tax Lot 81027CB01901, based on the findings in the staff report, Conditions of Approval, and discussion.

Attachments

- 1 Site Design Review Application
- 2 Site Design Review Narrative
- 3 Site Design Review Plan Set
- 4 Pre-Application Memo
- 5 Mailed Public Notice
- 6 Published Public Notice

**CITY OF WARRENTON
PLANNING AND BUILDING DEPARTMENT**

**COMMERCIAL SITE DESIGN
APPLICATION**

To be accompanied by a Site Plan Map, copy of property deed and if applicable, a Letter of Authorization.

OFFICE USE ONLY	
FILE # _____	FEE \$ _____
ZONING DISTRICT _____	
RECEIPT # _____	
DATE RECEIVED _____	

The site plan review process is a method for assuring compliance with the City of Warrenton Comprehensive Plan and Development Code, and to ensure wise utilization of natural resources, and the proper integration of land uses utilizing appropriate landscaping or screening measures. A commercial enterprise must also consider traffic circulation patterns, off-street parking, refuse containers, safe exit and entrance to the business, building height, dust control, future widening of major thoroughfares, and signs. Please answer the questions as completely as possible.

.....

Legal Description of the Subject Property: Township 8N, Range 10W, Section(s) SW 1/4 of 27, Tax Lot(s) 81027CB01901

Property street address 1771 SE Ensign Lane, Warrenton, OR 97146

I/WE, THE UNDERSIGNED APPLICANT(S) OR AUTHORIZED AGENT, AFFIRM BY MY/OUR SIGNATURE(S) THAT THE INFORMATION CONTAINED IN THE FOREGOING APPLICATION AND ASSOCIATED SUBMISSIONS IS TRUE AND CORRECT.

APPLICANT:

Printed Name: PM Design (Craig Gilbert)

Signature: Craig D Gilbert Date: 8/31/2021

Address: 19120 SE 34th Street, Suite 115 Phone: 360-719-1953

City/State/Zip: Vancouver, WA 98683 Fax: _____

PROPERTY OWNER (if different from Applicant):

Printed Name: North Coast Retail LLC (Wes Giesbrecht)

Signature: Wes Giesbrecht Date: 9/13/21

Address: P.O. Box 791 Phone: 206-749-9600

E-mail Address: atlin@qwestoffice.net

City/State/Zip: Mercer Island, WA 98040 Fax: _____

1. In detail, please describe your proposal:

The proposal is to develop the site for a Popeye's Louisiana Kitchen restaurant with parking, driveways, landscaping and a drive-through. The proposed Popeye's development will provide a 2,111-sf one-story building with a drive-through. The restaurant will provide dine-in and take-out service and will provide 24 indoor seats. The restaurant will operate during the hours of 10:30 a.m. to 11 p.m. Deliveries will occur in the morning or afternoon.

2. Describe what type of business, commodity sold or manufactured, or service you are proposing.

ASSEMBLY A-2 (RESTAURANT) Fast food with inside dining and a drive-through window.

Current number of employees: 7

Projected number of customers per day 350 meals/day

Days of operation 7 days a week Hours of operation 10:30 am - 11:00 pm

Number of shipments/deliveries per day per week 2x per week

By what method will these be arriving/sent? Delivery truck

3. Does this property have an existing business or businesses? No

If yes, please list the business names and their addresses, and note these businesses on your site plan map.

4. Is there a residence or residences on this property? No

If yes, please list the number of residences and please show these structures on your site plan map.

5. Availability of services: City water Yes, City sewer Yes

6. If you are an existing business, are materials or merchandise currently being stored on site?

N/A

Where and how do you propose to store materials or merchandise for sale or processing?

Items will be stored in the walk-in and in the back of house area on metro racks.

7. What percentage of the property is currently landscaped? 23.7 %

What percentage of the property do you propose to landscape as part of this project? 22.7%

8. How do you intend to irrigate the existing and proposed landscaping?

The existing Southeasterly corner and Southwesterly property line appears to be landscaped and irrigated. The remaining portion of parcel appears naturally vegetated, and does not appear to be irrigated. There is an easement dedicated to Walmart over the landscaped area. As such the existing landscape area may be irrigated via the same system serving the entire Walmart development. There is an existing 8" water stub to the subject parcel off an existing water line in the driveway off SE Ensign Lane. Proposed project will either wye off the 8" water stub, or tap the existing water line servicing Walmart development.

9. Signs require the submittal of a separate application, which may be submitted in conjunction with this site plan application. Understood

10. Please explain how you propose to provide for the drainage of this property, or explain why no additional drainage consideration is necessary.

Stormwater treatment will be proposed onsite prior to discharge via underground storm drain conveyance to existing ODOT ditch along HWY 101.

11. Please provide the type of development on the neighboring properties.

- North: Improved Commercial Land - Wauna Federal Credit Union
- South: Improved Commercial Land - Wal-Mart
- East: Improved Commercial Land - Costco Wholesale
- West: Improved Commercial Land - Fibre Federal Credit Union

12. Provide samples of the building materials for the exterior of the building with detail description of where each type and color will be used in the construction and finishing of the building. Understood

13. Will all parking for your business be provided on the property? Yes No All parking must be shown on your site plan map. If off-street parking is to be provided on another property, please attach a copy of the parking easement or agreement from the property owner; or will off-street parking be provided along the abutting street.

14. How does this request comply with the Warrenton Development Code Chapter 16, Section 16.40 (General Commercial)? The following uses and their accessory uses are permitted in the C-1 zone if the uses conform to the standards in Sections 16.40.040 through 16.40.060, Chapters 16.124, 16.212 and other applicable Development Code standards, and other City laws: #7. Eating and drinking establishment.

15. Orientation of proposed building(s) (see Section 16.116.030 in the Warrenton Development Code)

The principal building entrance is oriented toward the primary focal point of the property/development.

16. Please address (on separate sheet of paper) all applicable sections of Design Standards (copy attached) out of the Warrenton Development Code. See attached

PLEASE UNDERSTAND THAT THIS APPLICATION WILL NOT BE OFFICIALLY ACCEPTED UNTIL DEPARTMENT STAFF HAS DETERMINED THAT THE APPLICATION IS COMPLETELY FILLED OUT AND THE SITE PLAN MAP REQUIREMENTS HAVE BEEN COMPLETED.

Return Application To:

**City of Warrenton
Planning and Building Department
PO Box 250
225 S. Main Street
Warrenton, Oregon 97146**

**Phone: 503-861-0920
Fax: 503-861-2351**

MAP INSTRUCTIONS AND CHECKLIST

A Site Plan Map, which shows all existing and proposed structures and parking areas, must accompany this application. The following checklist identifies the specific information which should be included on this map.

- ___ Title the map "Commercial Site Design".
- ___ The map may be drawn on 8 ½ x 11 or 8 ½ x 14 inch white paper.
- ___ Township, Range, Section and Tax Lot number of the subject property(ies) shall be included.
- ___ North arrow, date, and map scale in one inch intervals (1" = 20') shall be noted.
- ___ Shape, dimensions, and square footage of the parcel shall be shown. Draw the property line with a solid black line and label adjacent street(s), if any.
- ___ Identify existing and proposed easements with a dotted line.
- ___ Identify the location and direction of all water courses and drainage ways, as well as the location of the 100-year floodplain, if applicable.
- ___ Illustrate all existing buildings and their sizes.
- ___ Illustrate all proposed new construction with dashed lines (include dimensions).
- ___ Illustrate parking area with number of spaces and access drive areas. If off-street parking is to be provided, even in part, on another property, please show its location on your site plan map, and attach a copy of the parking easement or agreement from the adjoining property owner.
- ___ Illustrate the entrance and exit points to the property, pattern of traffic flow, loading and unloading area, sidewalks and bike paths.

- ___ Illustrate the existing or proposed location, height, and material of all fences and walls.
- ___ Illustrate existing or proposed trash and garbage container locations, including type of screening.
- ___ Name of the person who prepared the map.
- ___ Location, type and height of outdoor lighting.
- ___ Location of mailboxes if known.
- ___ Locations, sizes, and types of signs (shall comply with Chapter 16.144 of the Warrenton Development Code).
- ___ Map shall show entire tax lot plus surrounding properties.
- ___ Identification of slopes greater than 10%.
- ___ location, condition and width of all public and private streets, drives, sidewalks, pathways, right-of-ways, and easements on the site and adjoining the site.
- ___ Identify designated flood hazard area(s).
- ___ Show wetland and riparian areas, streams and/or wildlife areas.
- ___ Any designated historic and cultural resources areas on the site and/or adjacent parcels or lots.
- ___ Location, size and type of trees and other vegetation on the property.



Site Plan Narrative

1771 SE Ensign Lane
Warrenton, Oregon 97146



POPEYE'S
1771 SE Ensign Lane
Warrenton, OR 97146

SITE PLAN REVIEW

9/09/2021

Ambrosia QSR, Inc., applicant

Table of Contents

I.		REVIEW REQUESTED	
II.		PROJECT LOCATION AND IDENTIFICATION	
III.		PROJECT DESCRIPTION	
IV.		LAND USE REVIEWS	
	1.	Conditional Uses	
	2.	Type III Procedures	
	3.	Consolidated Review and Decision	
	4.	Site Design Review Application	
	5.	Conditional Use Application	
	6.	Other Applicable Design Standards	

ATTACHMENTS

PLANS

1. Commercial Site Design
2. Preliminary Site Development Plan
3. Preliminary Architectural Floor Plan
4. Preliminary Grading Plan
5. Preliminary Landscape Plan
6. Architectural Elevations, Renderings and Materials
7. Sign Package

APPENDICES

- A. Reciprocal Access
- B. Easements with Covenants and Restrictions
- C. Traffic Generation Estimate

PROJECT NARRATIVE

I. REVIEW REQUESTED

The applicant is requesting review and approval of the following applications:

1. Site Development Review

II. PROJECT LOCATION AND IDENTIFICATION

The project site is located at 1771 SE Ensign Lane, Warrenton, Oregon, 97146. The site is Assessor's Parcel # 81027CB019001, is zoned C1 General Commercial and is 0.5 acres in size.

One driveway access from the Walmart parking lot would serve the subject site. Interconnectivity would additionally be provided with the surrounding tax lots, which together comprise a shopping center.

III. PROJECT DESCRIPTION

The proposal is to develop the site for a Popeye's Louisiana Kitchen restaurant with parking, driveways, landscaping and drive-through. The proposed Popeye's development will provide a 2,133 sf one-story building with a drive-through. The restaurant will provide dine-in and take-out service, and will provide 48 indoor seats. The restaurant will operate during the hours of 10:30 a.m. to 11 p.m. Deliveries will occur in the morning or afternoon.

This site is east of the Oregon Coast Highway. The minimum setback is 25 feet per City of Warrenton. Additional requirements include a setback of 35 feet from the line of non-aquatic vegetation and a minimum setback of 50 feet from adjacent resource zones. Neither of the additional requirement is applicable to our site.

The project will provide a pedestrian walkway from the access/entrance road parking area west of the building to the main entrance.

The surrounding planning overlay zones (I1, R10, A5, RH) are as follows: I1 General Industrial Zone to the East across Ensign Ln & South, R10 Intermediate Residential Zone to the Northwest across Hwy 101, A5 Lake and Freshwater Wetland West across Dolphin Ave, RH High Density Residential South of Ensign Ln.

Construction of the project will start on 02/01/2022. The project will include construction of associated parking areas, landscaping, sidewalk connections to the public right-of-way, driveway access connections to the shared private access drive, and required utility connections.

IV. **LAND USE REVIEWS - struck through code section are not applicable to this project.**

1. *Conditional Uses*

16.40.030-060 Conditional Uses and Related Standards.

16.40.030

The following uses and their accessory use may be permitted in the C-1 zone when approved under Chapter 16.220 and shall comply with Sections 16.40.040 through 16.40.060 and Chapters 16.124 (Landscaping) and 16.212 (Site Design Review): **Sections addressed below in Parts 4 and 6**

- A. Only the following uses and their accessory uses are permitted along Highway 101, SE Marlin and SW Dolphin Avenues, and shall comply with the above noted sections and Chapter 16.132:
1. ~~Cabinet, carpenter, woodworking or sheet metal shops.~~
 2. ~~Processing uses such as bottling plants, bakeries and commercial laundries.~~
 3. ~~Research and development establishments.~~
 4. ~~Wholesale storage and distribution facilities, including cold storage.~~
 5. ~~RV Park.~~
 6. New drive-through/drive-up facility or substantially improved as defined by 25% of assessed value. **The Popeye's project meets condition 16.40.030-A-6 as the improvement value exceeds 25% of the assessed value.**
 7. ~~Similar uses as those stated in this section.~~

16.40.040

The following development standards are applicable in the C-1 zone:

- A. Density Provisions.
1. Minimum lot size, commercial uses: none. **Met**
 2. Minimum lot width, commercial uses: none. **Met**
 3. Minimum lot depth, commercial uses: none. **Met**
 4. Maximum building height: 45 feet. **Met-Building is 21'10" at highest point.**
 5. Commercial uses, maximum lot coverage: none. **Met**
- B. Setback Requirements.
1. Minimum front yard setback, commercial uses: none ~~except where adjoining a residential zone, in which case it shall be 15 feet.~~ See Section 16.40.050 for maximum front yard setback for commercial uses. **Met-site is not adjoining a residential zone.**
 2. Minimum side and rear yard setbacks, commercial uses: none ~~except where adjoining a residential zone in which case there shall be a visual buffer strip of at least 10 feet wide to provide a dense evergreen landscape buffer which attains a mature height of at least eight feet. Such buffers must conform to the standards in Chapter 16.124, Landscaping, Street Trees, Fences and Walls.~~ **Met-site is not adjoining a residential zone.**
- C. Landscaping requirements shall comply with Chapter 16.124 of the Development Code. **Section addressed below in Part 6.**

16.40.050

The following design standards are applicable in the C-1 zone:

- A. Any commercial development shall comply with Chapter 16.116 of the Development Code.
- B. ~~Lots fronting onto U.S. Highway 101 shall have a setback of at least 50 feet between any part of the proposed building and the nearest right-of-way line of U.S. Highway 101.~~ **Lot does not have US Highway 101 frontage.**
- C. ~~Signs in General Commercial Districts along Fort Stevens Highway/State Highway 104 (i.e., S. Main Avenue, N. Main Avenue, NW Warrenton Drive, and Pacific Drive) shall comply with the special sign standards of Section 16.144.040.~~ **Lot does not adjoin the listed roads.**
- D. ~~Maximum front yard setback for commercial buildings in the C-1 zone along Fort Stevens Highway/State Highway 104 shall be 10 feet.~~ **Lot does not adjoin the listed roads.**
- E. ~~Maximum front yard setback for commercial buildings in the C-1 zone adjacent to existing or planned transit stops shall be 10 feet.~~ **Lot is not adjacent to a planned transit stop.**
 - 1. ~~The Community Development Director may allow a greater front yard setback when the applicant proposes extending an adjacent sidewalk or plaza for public use, or some other pedestrian amenity is proposed between the building and public right-of-way, subject to Site Design Review approval. (Ord. 1225 § 3, 2019)~~ **A plaza for public use is not planned for this project.**

16.40.060

- A. ~~Outside sales and service areas shall be approved by the Warrenton Planning Commission if not enclosed by suitable vegetation, fencing or walls.~~ **Site does not have outside sales and service areas.**
- B. ~~Outside storage areas shall be enclosed by suitable vegetation, fencing or walls, in conformance with Chapter 16.124.~~ **Site does not have outside storage areas.**
- C. All uses shall comply with access and parking standards in Chapters 16.116 and 16.128 except as may be permitted by conditional use or variance. **Addressed below in Part 6.**
- D. Signs shall comply with standards in Chapter 16.144. **Addressed below in Part 6.**
- E. ~~All development shall comply with the wetland and riparian area protection standards of Chapter 16.156.~~ **Site is not in a wetland or riparian area.**
- F. All other applicable Development Code requirements shall also be satisfied. **Noted.**
- G. ~~RV parks shall comply with Chapter 16.176 and all applicable State and Federal laws and regulations.~~
- H. ~~Prior to undertaking disposal, the dredging project proponent shall consult with the Army Corps and Oregon DSL to determine if the disposal site contains wetlands that are regulated under permit programs administered by those agencies. If the site contains regulated wetlands, the dredging project proponent shall either alter the disposal site boundaries to avoid the wetlands and leave an acceptable protective buffer, or obtain the necessary Corps and DSL permits to fill the wetlands.~~
- I. ~~State licensed medical marijuana dispensaries and recreational marijuana retail outlets shall be located only east of Highway 101 and at least 1,000 feet from any public or private school, church, public park, or child care center, and operate exclusively as a single building~~

~~occupant or with other licensed medical marijuana dispensaries or recreational marijuana retail outlets. (Ord. 1196-A § 2, 2015)~~

2. Type III Procedures

16.208.050 Type III Procedure

- A. Pre-application Conference. **Conducted on 8-5-2021**
- B. Application Requirements.
- C. Notice of Hearing.
- D. Conduct of the Public Hearing.
- E. The Decision Process.
- F. Notice of Decision.
- G. Final Decision and Effective Date.
- H. Appeal.

We have thoroughly reviewed Warrenton Municipal Code Section 16.208.050 regarding Type III Procedures, and will comply with all requirements, submittals and notifications.

3. Consolidated Review and Decision

16.208.070 General Provisions.

- A. 120-Day Rule.
- B. Time Computation.
- C. Pre-Application Conferences.
- D. Applications.
- E. Community Development Director's Duties.
- F. Amended Decision Process.
- G. Re-submittal of Application Following Denial

We have thoroughly reviewed Warrenton Municipal Code Section 16.208.070 regarding Consolidated Reviews and the Decision Process, and understand and will comply with the processes and actions delineated in the municipal code.

4. Site Design Review Application

16.212.040 Site Design Review

- A. Application Review Procedure.
- B. Application Submission Requirements.
- C. Review Criteria.

We have thoroughly reviewed Warrenton Municipal Code Section 16.212.040 regarding Site Design Review requirements and expectations. Our submittals and drawings will meet or exceed these requirements.

16.220 Conditional Use Application

16.220.010 Purpose-Noted

16.220.020 Authority to Grant or Deny Conditional Uses-Understood

16.220.030 Review Criteria

A. Before a conditional use is approved findings will be made that the use will comply with the following standards:

1. The proposed use is in conformance with the Comprehensive Plan.

A. This project is in conformance with the Comprehensive Plan as the site is zoned C-1 (General Commercial), and as per 16.0.020 A-7, eating and drinking establishments are an allowed use in a C-1 zone. The drive-thru portion of this project is a Conditionally allowed use in the C-1 zone per 16.40.030 A-6. All parts of the development are more than 50 ft from the Highway 101 right-of-way line as per 16.40.050

B. This project complies with all the parking, signage, wetland, and other Development Code requirements as listed in 16.40.060 C-E in the following manner:

C. Parking: 16.116 and 16.128—six parking spots are required, 17 are provided

D. Signage 16.144-No sign will be over 45 feet in height. As the site has two street frontages, and will comply with the following signage limits: 32 sf cumulative of signage is allowed, and an addition sign not larger than 32 sq ft. is allowed.

E. Wetland and Riparian Protection 16.156—Site is not located in a wetland or riparian corridor.

2. The location, size, design and operating characteristics of the proposed use are such that the development will be compatible with, and have a minimal impact on, surrounding properties.

This project is compatible with the surrounding area in the following ways:

- **Location: Site is currently unused, vacant land in proximity to an existing Walmart and Costco. This is one of the few undeveloped or under-developed parcels in an otherwise developed commercial area. This project will bring another food choice to the customers already using the above mentioned retailers.**
- **Size and Design: The characteristics of the site are suitable for the proposed use in both size and design. This is an approximately .5 acre lot with a proposed 2,111 ft quick serve restaurant with a drive thru. There are other restaurants on the east side of SE Ensign Lane, but this will be the first food offering on the west side of SE Ensign lane.**
- **Operating Characteristics: The majority of the activity will be contained in the building or in the drive-thru lanes. There are no nearby residential developments, so menu board and speaker noise is not expected to be an issue. Restaurant intends to be open 10:30am to 11:00 pm 7 days a week.**
- **Minimal Impact: new project will have minimal impact on the surrounding properties. All parking and drive-thru queuing is on-site. The hours of operation are within the hours of operation for the big box stores in the vicinity,**

3. The use will not generate excessive traffic, when compared to traffic generated by uses permitted outright, and adjacent streets have the capacity to accommodate the traffic generated.
The additional traffic generated by the project will be negligible. In a traffic study conducted on 11-22-2019, the adjacent streets and intersections have the capacity to accommodate any extra trips. This project is an ancillary use to the large retailers in the area, and much of the traffic will be cross-over from the retailers. , the maximum need for queuing for a quick serve restaurant is estimated to be eight cars. The site as designed provides queuing for thirteen cars.
4. Public facilities and services are adequate to accommodate the proposed use.
The site and proposed use will have adequate public facilities and services to accommodate the proposed use. Public facilities and infrastructure already exist in the area for the large retailers. Please see the attached preliminary utility plan.
5. The site's physical characteristics, in terms of topography, soils and other pertinent considerations, are appropriate for the use.
The new parking lot will be paved. The site's topography is appropriate for the use. The site is generally flat with a slight slope to the southwest, and will not require excessive grading or additional disturbance. The underlying soils are sandy and mixed, and are stable to provide an adequate foundation for the restaurant and associated structures, and provide adequate constructability for the project.
6. The site has an adequate area to accommodate the proposed use. The site layout has been designed to provide for appropriate access points, on-site drives, public areas, loading areas, storage facilities, setbacks and buffers, utilities or other facilities which are required by City ordinances or desired by the applicant.
The site has adequate area to accommodate the project. The queuing of the drive-through will all be on-site. The setback of 25' for front yard structures, 35 feet from the line of non-aquatic vegetation, an is at least 50 feet from adjacent resource zones. An adequate number of parking spaces is provided. Landscape islands and planter bays with a minimum of 25 sq ft and min 5 ft in width will be provided.
7. The use is appropriate at the proposed location. Several factors which should be considered in determining whether or not the use is appropriate include: accessibility for users (such as customers and employees); availability of similar existing uses; availability of other appropriately zoned sites; and the desirability of other suitably zoned sites for the intended use.
The quick serve restaurant use is appropriate to the proposed location. The QSR will complement the large retailer on the adjoining lot, providing more dining options for employees and customers. The increase in traffic will be negligible, as there will be a significant number of shared trips. Placing a QSR on this lot will provide a restaurant within reasonable walking distance for those employees and customers. There are currently at least two fast food restaurants on the north side of SE Ensign Lane. This project will provide a choice on the south side of SE Ensign Lane.

- B. Transportation System Facilities and Improvements. **There is non planned widening of highways, roads, or bridges associated with this project.**
1. ~~Construction, reconstruction, or widening of highways, roads, bridges or other transportation facilities that are (a) not designated in the City's adopted Transportation System Plan ("TSP"), or (b) not designed and constructed as part of an approved subdivision or partition, are allowed in most districts (see Section 16.20.040 for a list of districts that allow transportation facilities and improvements) subject to a conditional use permit and satisfaction of all of the following criteria:~~
 - a. ~~The project and its design are consistent with the City's adopted TSP, or, if the City has not adopted a TSP, consistent with the State Transportation Planning Rule, OAR 660-012 ("the TPR").~~
 - b. ~~The project design is compatible with abutting land uses in regard to noise generation and public safety and is consistent with the applicable zoning and development standards and criteria for the abutting properties.~~
 - c. ~~The project design minimizes environmental impacts to identified wetlands, wildlife habitat, air and water quality, cultural resources, and scenic qualities, and a site with fewer environmental impacts is not reasonably available. The applicant shall document all efforts to obtain a site with fewer environmental impacts, and the reasons alternative sites were not chosen.~~
 - d. ~~The project preserves or improves the safety and function of the facility through access management, traffic calming, or other design features.~~
 - e. ~~The project includes provisions for bicycle and pedestrian access and circulation consistent with the Comprehensive Plan, the requirements of this Development Code, and the TSP or TPR.~~
 2. ~~State Transportation System Facility or Improvement Projects. The State Department of Transportation ("ODOT") shall provide a narrative statement with the application demonstrating compliance with all of the criteria and standards in this section. Where applicable, an environmental impact statement or environmental assessment may be used to address one or more of these criteria.~~ **This project is not subject to ODOT jurisdiction.**
 3. ~~Proposal Inconsistent with TSP/TPR. If the City determines that the proposed use or activity or its design is inconsistent with the TSP or TPR, then the applicant shall apply for and obtain a plan and/or zoning amendment prior to or in conjunction with conditional use permit approval. The applicant shall choose one of the following options:~~
 - a. ~~If the City's determination of inconsistency is made prior to a final decision on the conditional use permit application, the applicant shall withdraw the conditional use permit application; or~~
 - b. ~~If the City's determination of inconsistency is made prior to a final decision on the conditional use permit application, the applicant shall withdraw the conditional permit application, apply for a plan/zone amendment, and re-apply for a conditional use permit if and when the amendment is approved; or~~
 - c. ~~If the City's determination of inconsistency is made prior to a final decision on the conditional use permit application, the applicant shall~~

~~submit a plan/zoning amendment application for joint review and decision with the conditional use permit application, along with a written waiver of the ORS 227.178 120-day period within which to complete all local reviews and appeals once the application is deemed complete; or~~
d. ~~If the City's determination of inconsistency is part of a final decision on the conditional use permit application, the applicant shall submit a new conditional use permit application, along with a plan/zoning amendment application for joint review and decision.~~ **We believe our proposal to be consistent with the Warrenton TSP/TPR. Please see the attached traffic report dated 11-22-2019. The traffic impact will be negligible.**

4. ~~Expiration.~~ A conditional use permit for transportation system facilities and improvements shall be void after two years. **Not expected to require a CUP for transportation system facilities.**

C. Drive-Up/Drive-Through Facility.

1. Purpose. Where drive-up or drive-through uses and facilities are allowed, they shall conform to all of the following standards, which are intended to calm traffic, provide for adequate vehicle queuing space, prevent automobile turning movement conflicts, and provide for pedestrian comfort and safety. **Our site plan follows these standards.**

2. Standards. Drive-up and drive-through facilities (i.e., driveway queuing areas, customer service windows, teller machines, kiosks, drop-boxes, or similar facilities) shall meet all of the following standards:

- a. The drive-up or drive-through facility shall orient to and receive access from a driveway that is internal to the development and not a street, as generally illustrated. **The entrance is located on an internal driveway, not a public street.**
- b. The drive-up or drive-through facility shall not be oriented to street corner. **Although the site is on a corner, the drive-in and drive-through are oriented toward the interior of the lot and toward the shared parking lot.**
- c. The drive-up or drive-through facility shall not be located within 20 feet of a street right-of-way. **Our site plan meets this requirement. The drive-through is not within 20 ft of a street right-of-way.**
- d. Drive-up and drive-through queuing areas shall be designed so that vehicles will not obstruct any street, fire lane, walkway, bike lane, or sidewalk. **The queuing lane is designed to this standard.**
- e. ~~Along Highway 101, between SE Marlin and SE Dolphin Avenues, no new drive up or drive through facility is allowed within 400 linear feet of another drive up or drive through facility, where the existing drive up or drive through facility lawfully existed as of the date of an application for a new drive up or drive through facility. (Ord. 1233 § 2, 2020; Ord. 1225 § 11, 2019)~~ **This project is not located between the two streets listed above.**

16.220.040 Application- **Understood**

16.220.050 Procedures-**Noted**

- 16.220.060 Compliance with Conditions of Approval-**Project will comply with all Conditions of Approval.**
- 16.220.070 Time Limit on a Permit for Conditional Use-**Noted. We understand the one-year limitation and the need for written request for an extension.**
- 16.220.080 Limitations on Refiling an Application-**Noted. We understand the one-year waiting period for a substantially similar application that has been previously denied.**

6. Other Applicable Design Standards

16.116.030 Design Standards

The City's development design standards are for the commercial district corridors along US Highway 101, Alternate Highway 101, SW & SE Dolphin, SE Marlin Avenue, E Harbor Drive, S Main Avenue, SE Ensign Lane, Pacific Drive, and SE Neptune. The standards in this section apply only to property in the commercial corridors listed above; lands outside of these commercial corridors are not subject to these standards. The following standards will apply to a new large retail establishment and/or complex that meets or exceeds 30,000 square feet of planned building footprint or gross floor area, whichever is greater; or has a façade length that is 100 feet or more along any street frontage; or to new mini-storage units regardless of size or façade length. An existing large retail establishment of said square footage or length or greater shall comply with these standards if proposed renovations or improvements exceed 50 % of the market value. **Not applicable to this project as it is not considered a large retail complex or establishment. The building footprint does not exceed 30,000 sf, the façade length is less than 100' along a roadway, and project is not a mini-storage facility.**

16.116.030 Access and Circulation

The City's development design standards are for the commercial district corridors along US Highway 101, Alternate Highway 101, SW & SE Dolphin, SE Marlin Avenue, E Harbor Drive, S Main Avenue, SE Ensign Lane, Pacific Drive, and SE Neptune. The standards in this section apply only to property in the commercial corridors listed above; lands outside of these commercial corridors are not subject to these standards. The following standards will apply to a new large retail establishment and/or complex that meets or exceeds 30,000 square feet of planned building footprint or gross floor area, whichever is greater; or has a façade length that is 100 feet or more along any street frontage; or to new mini-storage units regardless of size or façade length. An existing large retail establishment of said square footage or length or greater shall comply with these standards if proposed renovations or improvements exceed 50 % of the market value. **Not applicable to this project as it is not considered a large retail complex or establishment. The building footprint does not exceed 30,000 sf, the façade length is less than 100' along a roadway, and project is not a mini-storage facility.**

16.124 Landscaping

16.124.050

Fences and Walls

- A. General Requirements. All fences and walls shall comply with the standards of this section. The City may require installation of walls and/or fences as a condition of development approval, in accordance with Chapter 16.220, Conditional Use Permits, or Chapter 16.212, Development Review and Site Design Review. Walls built for required landscape buffers shall comply with Section 16.124.030.
- B. Dimensions.
1. The maximum allowable height for fences and walls in the City of Warrenton is six feet, as measured from the lowest grade at the base of the wall or fence, except that retaining walls and terraced walls may exceed six feet when permitted as part of a site development approval, or as necessary to construct streets and sidewalks. ~~Refer to paragraph 4 of this subsection for additional fence standards for residential uses.~~
 2. ~~Fences in the General Industrial (I-1) Zone may exceed six feet if necessary to protect the welfare of the general public (i.e., airport runway safety, military, coast guard, or homeland security defense facilities, etc.) but not for protection of private property (i.e., auto repair lots, equipment yards, woodworking shops, etc.). Barbed and razor wire fencing is prohibited in all zones except as necessary to enclose livestock or to protect the welfare of the general public (not private property).~~ **Not and industrial zone.**
 3. A building permit is required for walls exceeding four feet in height and fences exceeding six feet in height, in conformance with the Uniform Building Code.
 4. ~~The height of fences and walls within a required front yard setback area for residential uses shall not exceed four feet (except decorative arbors, gates, etc.), as measured from the grade closest to the street right of way. Walls may exceed this height in accordance with paragraph 1 of this subsection. Chain link fences and other open style fences with at least 50% transparency or open space are allowed a maximum height of six feet within a required front yard setback area.~~ **Not a residential zone.**
 5. Walls and fences to be built for required buffers shall comply with Section 16.124.030.
 6. Fences and walls shall comply with the vision clearance standards of Section 16.120.020.
- C. Maintenance. For safety and for compliance with the purpose of this chapter, walls and fences required as a condition of development approval shall be maintained in good condition, or otherwise replaced by the owner. **Fences and walls are not part of the current site plan for this project. We have read and understood the design guidelines and will comply if a fence or wall becomes part of the project.**

16.124.060

Landscape Conservation

- A. Applicability. All development sites containing significant vegetation, as defined below, shall comply with the standards of this section. The purpose of this section is to incorporate significant native vegetation into the landscapes of development. The use of mature, native vegetation within developments is a preferred alternative to removal of

vegetation and re-planting. Mature landscaping provides summer shade and wind breaks, and allows for water conservation due to larger plants having established root systems.

- B. Significant Vegetation. “Significant vegetation” means:
1. Significant Trees and Shrubs. Individual trees located within a mapped wetland area as depicted on the 1” = 400’ maps entitled *City of Warrenton Wetland Conservation Plan Inventory* dated October 17, 1997 with a trunk diameter of 18 inches or greater, as measured four feet above the ground (DBH), and all plants within the drip line of such trees and shrubs, shall be protected. Other trees may be deemed significant, when nominated by the property owner and designated by the City Commission as “Heritage Trees” (i.e., by virtue of site, rarity, historical significance, etc.).
 2. Exceptions. Protection shall not be required for (a) plants listed as non-native, invasive plants by the Oregon State University Extension Service in the applicable OSU bulletins for Clatsop County, or (b) as otherwise excepted by Chapter 16.156, *Wetland and Riparian Corridor Development Standards*.
- C. Mapping and Protection Required. All mapped wetland and riparian areas shall be protected in accordance with Chapter 16.156 and other applicable sections of this Code.
- D. Protection Standards. All of the following protection standards shall apply to significant vegetation areas:
1. Protection of Significant Trees. Significant trees identified as meeting the criteria in subsection (B)(1) of this section shall be retained whenever practicable. Preservation may become impracticable when it would prevent reasonable development of public streets, utilities, or land uses permitted by the applicable land use district.
 2. Conservation Easements and Dedications. When necessary to implement the Comprehensive Plan, the City may require dedication of land or recordation of a conservation easement to protect sensitive lands, including groves of significant trees and mapped wetland and/or riparian areas.
- E. Construction. All areas of significant vegetation and mapped wetland and riparian areas shall be protected prior to, during, and after construction. Grading and operation of vehicles and heavy equipment is prohibited within significant vegetation areas, wetlands, and riparian areas, except as approved by the City for installation of utilities or streets, or in accordance with other approved plans.
- F. Exemptions. The protection standards in this section shall not apply in the following situations:
1. Dead, Diseased, and/or Hazardous Vegetation. Vegetation that is dead or diseased, or poses a hazard to personal safety, property or the health of other trees, may be removed. Prior to tree removal, the applicant shall provide a report from a certified arborist or other qualified professional (i.e., a certified member of the Oregon Loggers Association) to determine whether the subject tree is diseased or poses a hazard, and any possible treatment to avoid removal, except as provided by paragraph 2 of this section.
 2. Emergencies. Significant vegetation may be removed in the event of an emergency when the vegetation poses an immediate threat to life or safety.
 3. Licensed Timber Operations. Logging operations that have been permitted by the Oregon Department of Forestry as being consistent with the Oregon Forest

~~Practices Rules and Statutes.~~ **Project site is not in an area of significant vegetation, significant trees, sensitive lands, nor mapped wetland/riparian area. Timber/logging statutes are not applicable.**

16.124.070

New Landscaping

- A. Applicability. This section shall apply to all developments within the City of Warrenton.
- B. Landscaping Plan Required. For every new development in the City of Warrenton requiring a City permit, a landscape plan is required. All landscape plans shall include the following minimum required details (see Section 16.212.040 for additional landscape plan requirements for projects requiring site design review):
1. Legal description (e.g., assessor parcel number, copy of warranty deed, etc.) for the subject property;
 2. Property lines with the location and general description (height and type of material) of existing and proposed fences and other buffering or screening materials;
 3. The location of existing and proposed terraces or retaining walls;
 4. The location of existing and proposed plant materials;
 5. Wetland and/or riparian area boundaries on the property, if any;
 6. Existing and proposed structures;
 7. Driveway and adjoining roadway widths, descriptions, and locations; and
 8. Prevailing drainage patterns for the property.
 9. Other information as deemed appropriate by the Community Development Director. An arborist's report may be required for sites with mature trees that are protected under this chapter and/or Chapter 16.156 of this Code. **The project's landscape plans will contain the information required above.**
- C. Landscape Area Standards. The minimum percentage of required landscaping equals:
1. ~~Residential districts: 20% of the site.~~
 2. Commercial districts: 15% of the site shall be landscaped according to the requirements of this section. **More than 15% of the site shall be landscaped.**
 3. ~~General industrial districts: a minimum of 20% of the site shall be landscaped. The use of mature, native vegetation within developments is a preferred alternative to removal of vegetation and re-planting.~~
- D. Landscape Materials. Landscape materials include trees, shrubs, groundcover plants, turf grasses (e.g. grass sod or seed), and outdoor hardscape features, as described below:
1. Natural Vegetation. Natural vegetation shall be preserved or planted where practicable.
 2. Plant Selection. A combination of deciduous and evergreen trees, shrubs, turf grasses, and groundcovers shall be used for all planted areas, the selection of which shall be based on local climate, exposure, water availability, and drainage conditions. As necessary, soils shall be amended to allow for healthy plant growth.
 3. "Non-native, invasive" plants, as per Section 16.124.020, shall be prohibited.
 4. Hardscape features (i.e., patios, decks, plazas, etc.) may cover up to 50% of the required landscape area. ~~Swimming pools, sports courts and similar active recreation facilities may not be counted toward fulfilling the landscape requirement.~~

5. Non-plant Groundcovers. Bark dust, chips, aggregate or other non-plant groundcovers may be used, but shall cover no more than 50% of the area to be landscaped. "Coverage" is measured based on the size of plants at maturity or after five years of growth, whichever comes sooner.
6. Tree Size. Trees shall have a minimum caliper size of one and one-half inches or greater (two inches for required street trees) at time of planting.
7. Shrub Size. Shrubs shall be balled and burlapped and sized to fit in multi-gallon containers.
8. Groundcover Size. Groundcover plants shall be sized and spaced so that they grow together to cover a minimum of 30% of the underlying soil within two years.
9. ~~Significant Vegetation. Significant vegetation preserved in accordance with Section 16.124.020 may be credited toward meeting the minimum landscape area standards. Credit shall be granted on a per square foot basis. The street tree standards of Section 16.124.040 may be waived when trees preserved within the front yard provide the same or better shading and visual quality as would otherwise be provided by street trees.~~
10. Stormwater Facilities. Stormwater facilities (e.g., detention/retention ponds and swales) shall be landscaped with water tolerant, native plants. **Read and understood. The project's landscape plan will comply with the above requirements.**
- E. Landscape Design Standards. All yards, parking lots and required street tree planter strips shall be landscaped in accordance with the provisions of Sections 16.124.010 through 16.124.050. Landscaping shall be installed with development to provide erosion control, visual interest, buffering, privacy, open space and pathway identification, shading and wind buffering, and to help control surface water drainage and improvement of water quality, based on the following standards:
 1. Yard Setback Landscaping. Landscaping shall satisfy the following criteria:
 - a. Provide visual screening and privacy within side and rear yards while leaving front yards and building entrances mostly visible for security purposes.
 - b. Use shrubs and trees as wind breaks, as appropriate.
 - c. Retain natural vegetation, as practicable.
 - d. Define pedestrian pathways and open space areas with landscape materials.
 - e. Provide focal points within a development, such as signature trees (i.e., large or unique trees), hedges and flowering plants.
 - f. Use trees to provide summer shading within common open space areas, and within front yards when street trees cannot be provided.
 - g. Use a combination of plants for year-long color and interest.
 - h. Use landscaping to screen outdoor storage and mechanical equipment areas, and to enhance graded areas such as berms, swales and detention/retention ponds.
 2. Parking Areas. A minimum of eight percent of the combined area of all parking areas, as measured around the perimeter of all parking spaces and maneuvering areas, shall be landscaped. Such landscaping shall consist of an evenly distributed mix of shade trees with shrubs and/or groundcover plants. "Evenly distributed" means that the trees and other plants are distributed around the parking lot perimeter and between parking bays to provide a partial canopy. At a minimum, one tree per five

parking spaces total shall be planted to create a partial tree canopy over and around the parking area. All parking areas with more than 20 spaces shall include landscape islands with trees to break up the parking area into rows of not more than 12 contiguous parking spaces. All landscaped areas shall have minimum dimensions of four feet by four feet to ensure adequate soil, water, and space for healthy plant growth.

3. Buffering and Screening Required. Buffering and screening are required under the following conditions:
 - a. Parking/Maneuvering Area Adjacent to Streets and Drives. Where a parking or maneuvering area is adjacent and parallel to a street or driveway, a decorative wall (masonry or similar quality material), arcade, trellis, evergreen hedge, or similar screen shall be established parallel to the street or driveway. The required wall or screening shall provide breaks, as necessary, to allow for access to the site and sidewalk by pedestrians via pathways. The design of the wall or screening shall also allow for visual surveillance of the site for security. Evergreen hedges used to comply with this standard shall be a minimum of 36 inches in height at maturity, and shall be of such species, number and spacing to provide the required screening within one year after planting. Any areas between the wall/hedge and the street/driveway line shall be landscaped with plants or other groundcover. All walls shall be maintained in good condition, or otherwise replaced by the owner.
 - b. Parking/Maneuvering Area Adjacent to Building. Where a parking or maneuvering area, or driveway, is adjacent to a building (other than a single-family, two-family, or three-family dwelling), the area shall be separated from the building by a raised pathway, plaza, or landscaped buffer not less than four feet in width. Raised curbs, bollards, wheel stops, or other design features shall be used to protect buildings from being damaged by vehicles. When parking areas are located adjacent to residential ground-floor living space (except for a single-family residence, duplex, and triplex), a landscape buffer is required to fulfill this requirement.
 - c. Screening of Mechanical Equipment, Outdoor Storage, Service and Delivery Areas, and Automobile-Oriented Uses. All mechanical equipment, outdoor storage and manufacturing, and service and delivery areas, shall be screened from view from all public streets and residential districts. Screening shall be provided by one or more the following: decorative wall (i.e., masonry or similar quality material), evergreen hedge, non-see-through fence, or a similar feature that provides a non-see-through barrier. Walls, fences, and hedges shall comply with the vision clearance requirements and provide for pedestrian circulation, in accordance with Chapter 16.120, Access and Circulation. (See Section 16.124.050 for standards related to fences and walls.) **Read and understood. The project's landscape plan will comply with the above requirements.**
- F. Maintenance and Irrigation. The use of drought-tolerant plant species is encouraged, and may be required when irrigation is not available. Irrigation shall be provided for plants that are not drought-tolerant. If the plantings fail to survive, the property

owner shall replace them with healthy plantings of shrubs, flowering plants and/or trees. All other landscape features required by this Code shall be maintained in good condition, or otherwise replaced by the owner. **Read and understood. The project's landscape plan will comply with the above requirements.**

- G. Additional Requirements. Additional buffering and screening may be required for specific land uses, as identified by Division 2, and the City may require additional landscaping through the conditional use permit process (Chapter 16.220). **Read and understood. The project's landscape plan will comply with the above requirements.**

16.124.080

Street Trees

Street trees shall be planted for all developments that are subject to land division or site design review. Requirements for street tree planting strips are provided in Chapter 16.136, Public Facilities Standards. Planting of unimproved streets shall be deferred until the construction of curbs and sidewalks. Street trees shall conform to the following standards and guidelines:

- A. Growth Characteristics. Trees shall be selected based on growth characteristics and site conditions, including available space, overhead clearance, soil conditions, exposure, and desired color and appearance. The following should guide tree selection:
1. Provide a broad canopy where shade is desired.
 2. Use low-growing trees for spaces under utility wires.
 3. Select trees which can be "limbed-up" where vision clearance is a concern.
 4. Use narrow or "columnar" trees where awnings or other building features limit growth, or where greater visibility is desired between buildings and the street.
 5. Use species with similar growth characteristics on the same block for design continuity.
 6. Avoid using trees that are susceptible to insect damage, and avoid using trees that produce excessive seeds or fruit.
 7. Select trees that are well-adapted to the environment, including soil, wind, sun exposure, and exhaust. Drought-resistant trees should be used in areas with sandy or rocky soil.
 8. Select trees for their seasonal color, as desired.
 9. Use deciduous trees for summer shade and winter sun. **Read and understood. The project's landscape plan will comply with the above requirements.**
- B. Caliper Size. The minimum caliper size at planting shall be one and one-half inches diameter breast height (dbh) (two inches for required street trees), based on the American Association of Nurserymen Standards. **Read and understood. The project's landscape plan will comply with the above requirements.**
- C. Spacing and Location. Street trees shall be planted within existing and proposed planting strips, and in sidewalk tree wells on streets without planting strips. The Community Development Director or Planning Commission may approve planting of street trees in other areas upon submission of a landscaping plan that demonstrates

comparable (or greater) benefits to the neighborhood. Street tree spacing shall be based upon the type of tree(s) selected and the canopy size at maturity. In general, trees shall be spaced no more than 30 feet apart, except where planting a tree would conflict with existing trees, retaining walls, utilities and similar physical barriers. **Read and understood. The project's landscape plan will comply with the above requirements.**

- D. Soil Preparation, Planting and Care. The developer shall be responsible for planting street trees, including soil preparation, groundcover material, staking, and temporary irrigation for two years after planting. The developer shall also be responsible for tree care (pruning, watering, fertilization, and replacement as necessary) during the first two years after planting. **Read and understood. The project's landscape plan will comply with the above requirements.**
- E. Assurances. The City shall require the developer to provide a performance and maintenance bond, or cash deposit, in an amount determined by the City-appointed engineer, to ensure the planting of the tree(s) and care during the first two years after planting. **Read and understood. The project's landscape plan will comply with the above requirements.**

16.128 Vehicle and Bicycle Parking

- A. General Provisions. **The parking plan for this project meets all the criteria listed in 16.128.030-A 1-10. Backing onto a street or right of way will not be required. Drive aisles will be clearly marked. Employee parking spaces will not be required as site has fewer than 20 parking spaces. Site is not adjacent to a transit stop.**
- B. Parking Location and Shared Parking. **All parking for this project is within 200' of the store's entrance. All parking for this project is on Popeye's parcel.**
- C. Parking Stall Standard Dimensions and Compact Car Parking.

Required Parking	Provided Parking
1 SPACE PER 4 SEATS OR 1 SPACE PER 100 SQ. FT. OF GROSS LEASEABLE FLOOR AREA, WHICHEVER IS LESS --2,111/100=21.11 (22 SPACES) --21 SEATS/4=5.25 (6 SPACES) REQUIRED	--1 ADA SPACE --1 SERVICE WINDOW WAITING SPACE. --13 STANDARD SPACES --2 COMPACT SPACES TOTAL = 17 SPACES
All parking stalls comply or exceed required dimensions on Figure 16.128.030.C Standard: 19' x 9' Compact: 16' x 8'	

- D. Disabled Person Parking Spaces-**Project is required to have one accessible parking space. Project complies. Our ADA space meets or exceeds the minimum required 18'x 9' with 8' loading space requirement for van accessible spaces.**

Our site plan includes a bicycle parking rack for two bicycles near the main entrance on the west side of the building.

16.140 Stormwater and Surface Management

The stormwater system for the site is being developed to retain any natural drainage, ensure proper drainage, manage surface water, and control erosions and sedimentation. No stormwater will be channeled into a sewer line. All drainage will be designed to accommodate flow during a 100-year storm event. We will submit an erosion control plan to the City of Warrenton Engineering Dept. for approval before beginning construction. We understand that minimum storm sewer size is eight inches in diameter in compliance with 16.140.050-A-2.

16.144 Signs

We have read and understand Chapter 16.144 Signs of the Warrenton Municipal Code. We will meet the requirements. See the attached preliminary signage plan attached.

16.152 Grading, Excavating, and Erosion Control Plans

The Project's grading, excavating, and erosion control plans will meet or exceed the criteria established in 16.152.010 thru 16.152.150 of the Warrenton Municipal Code. A plan review and permit will be obtained prior to any clearing, grading, stripping, excavating, or filling.



Community & Economic Development Department

Permit Checklist

June 2019

The following is a general checklist that applies to all Community & Economic Development Department submittals. The checklist is intended to assist prospective applicants with navigating the permit process. In each case, there may be additional submittal requirements that are identified in the pre-application notes.

The burden of proof rests with the applicant to demonstrate compliance with applicable portions of the Development Code.

- Signed Application & Fee (Site Design Review, Conditional Use, Variance, etc)
- Site Plan (3 copies | 11 x 17 or larger to scale)
- Landscape & Parking Plan (if not indicated on site plan)
- Engineering Review Form & Deposit
- Self-addressed, stamped envelopes for public notice (Type 2 & Type 3)
- Impact Study (Type 2 & Type 3)
- Pre-application Notes Response Letter
- Narrative of findings that addresses applicable criteria
- Preliminary utility and stormwater plan

After all materials have been verified, a completeness letter will be issued with a basic timeline for review. After completion, public notice will be published and mailed to the affected property owners and agencies.

The checklist itself is a requirement and must be submitted and date stamped by city staff.



September 13, 2021

Popeye's Louisiana Kitchen Impact Study

Introduction:

As required by Warrenton Municipal Code, Section 16.208.050.B.2.e requirements for all Type III applications, the applicant has prepared this impact study. Many of these items are also discussed in the project narrative responses also provided with this application.

The applicant is proposing to develop the site for a Popeye's Louisiana Kitchen restaurant with parking, driveways, landscaping and a drive-through. The proposed Popeye's development will provide a 2,111-sf one-story building with a drive-through. The restaurant will provide dine-in and take-out service and will provide 24 indoor seats. The restaurant will operate during the hours of 10:30 a.m. to 11 p.m. Deliveries will occur in the morning or afternoon. A site plan is included in the application.

Transportation:

This development should have no significant impact on the City's transportation system. The additional traffic generated by the project will be negligible. In a traffic study conducted on 11-22-2019, the adjacent streets and intersections have the capacity to accommodate any extra trips. This project is an ancillary use to the large retailers in the area, and much of the traffic will be crossover from the retailers. The maximum need for queuing for a quick serve restaurant is estimated to be eight cars. The site as designed provides queuing for thirteen cars.

Drainage System

This development will drain to an existing treatment swale located in ODOT ROW. We will show that the existing treatment swale has the capacity to treat runoff from this property, or provide additional treatment. This plan will be finalized by the applicant and approved by the City of Warrenton Public Works Department prior to the commencement of construction.

Parks:

The proposed development will have negligible impacts, positive or negative, to the city's parks.

Water System:

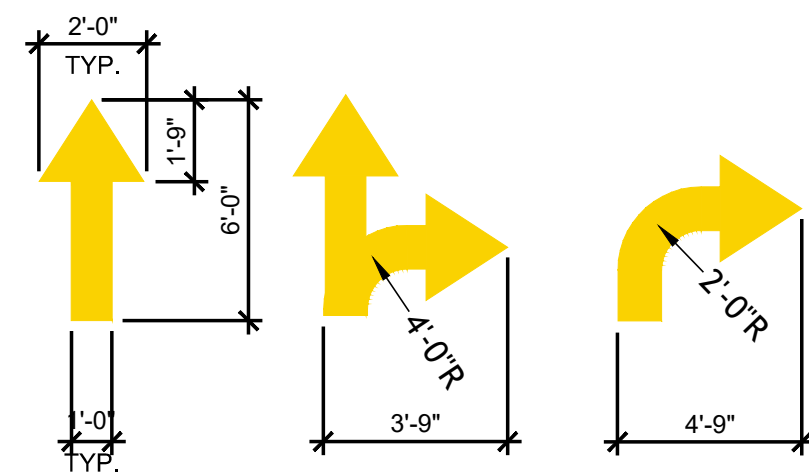
The city water system is proposed to service the property. No concerns about impacts to the water system have been raised by city staff. It is assumed that for a small development such as this, that the existing city water system has the capacity needed in terms of flows and storage needs.

Sanitary Sewer System:

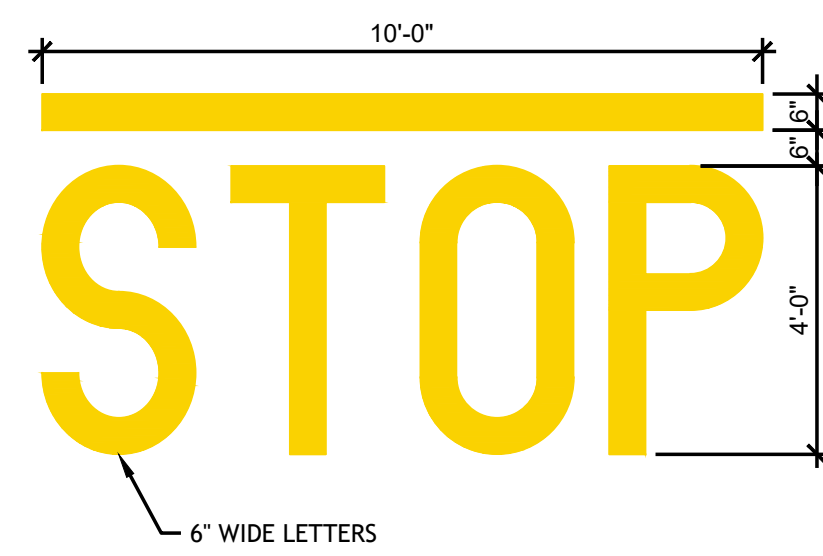
The minimal contribution of wastewater flows from the proposed restaurant to the city's sewer system is assumed insignificant and the capacity is available to serve the development.

Noise Impacts:

The proposed project consists of existing platted lot within the GC (General Commercial) zone. Noise impacts are assumed negligible. The noise impacts are assumed the same as for the adjoining credit union with a drive-thru to the West.



NOTES:
 1. REVERSE ARROWS FOR OPPOSITE DIRECTION OF FLOW
 2. ALL FLOW ARROWS TO BE PAINTED PER CITY REQUIREMENTS, SOLID YELLOW REFLECTIVE TRAFFIC PAINT AS PER DIMENSIONS ABOVE



PAINTED TRAFFIC ARROWS

3/8" = 1'-0"
 02-SITE-AD150-06

PAINTED STOP SIGN

3/8" = 1'-0"
 02-SITE-AD150-06

GEOTECHNICAL NOTE

NO SCALE

THE GENERAL CONTRACTOR IS TO OBTAIN A COPY OF THE GEOTECH REPORT, AND BE AWARE OF ALL APPLICABLE CONDITIONS THEREIN PRIOR TO START OF CONSTRUCTION. IN CASES WHERE THERE IS A CONFLICT BETWEEN SOIL PREPARATION AND CONSTRUCTION SPECIFICATIONS IN THE GEOTECH REPORT AND ON THE STRUCTURAL OR CIVIL DRAWINGS OF THE APPROVED SET, THE MOST RESTRICTIVE SPECIFICATION WILL TAKE PRECEDENCE. THE GENERAL CONTRACTOR IS RESPONSIBLE FOR CONTACTING THE DESIGN TEAM ARCHITECT OF RECORD TO OBTAIN CLARIFICATION ON ANY SPECIFICATIONS NOT UNDERSTOOD PRIOR TO START OF CONSTRUCTION. THE GENERAL CONTRACTOR IS TO REQUEST SOIL PREPARATION INSPECTIONS ONLY WHEN SPECIFICALLY REQUIRED IN THE GEOTECH REPORT, OR BY AN AGENCY OF AUTHORITY, OR ELSEWHERE IN THE APPROVED CONSTRUCTION DOCUMENTS.

- 1 (N) CONCRETE DRIVE-THRU LANE WITH PREMOLDED BITUMINOUS NON-EXTRUDING EXPANSION JOINTS AT 10' O.C.
- 2 INSTALL (N) GREASE INTERCEPTOR, REFER TO PLUMBING DRAWING, P-1 AND CIVIL DRAWING, C600.
- 3 INSTALL (N) CLEARANCE BAR, UNDER SEPARATE PERMIT BY OTHERS, REFER TO STRUCTURAL DRAWING, S-7.
- 4 INSTALL (N) DIGITAL PRE-SELL BOARD, UNDER SEPARATE PERMIT BY OTHERS, REFER TO STRUCTURAL DRAWING, S-7 & ELECTRICAL DRAWING, E-6. CONTRACTOR TO PROVIDE NECESSARY ELECTRICAL AS REQUIRED.
- 5 INSTALL (N) ORDER CONFIRMATION UNIT, UNDER SEPARATE PERMIT BY OTHERS, REFER TO STRUCTURAL DRAWING, S-7 & ELECTRICAL DRAWING, E-6. CONTRACTOR TO PROVIDE NECESSARY ELECTRICAL AS REQUIRED.
- 6 INSTALL (N) ORDER CONFIRMATION UNIT CANOPY, UNDER SEPARATE PERMIT BY OTHERS, REFER TO STRUCTURAL DRAWING, S-7 & ELECTRICAL DRAWING, E-6. CONTRACTOR TO PROVIDE NECESSARY ELECTRICAL AS REQUIRED.
- 7 INSTALL (N) DIGITAL MENU BOARD, UNDER SEPARATE PERMIT BY OTHERS, REFER TO STRUCTURAL DRAWING, S-7 & ELECTRICAL DRAWING, E-6. CONTRACTOR TO PROVIDE NECESSARY ELECTRICAL AS REQUIRED.
- 8 INSTALL (N) SAFETY BOLLARD, SEE DETAIL 9/A-0.2.
- 9 INSTALL (N) ENTRANCE DIRECTIONAL SIGN, UNDER SEPARATE PERMIT. CONTRACTOR TO PROVIDE NECESSARY ELECTRICAL AS REQUIRED.
- 10 INSTALL (N) EXIT DIRECTIONAL SIGN, UNDER SEPARATE PERMIT. CONTRACTOR TO PROVIDE NECESSARY ELECTRICAL AS REQUIRED.
- 11 INSTALL (N) DRIVE-THRU DO NOT ENTER SIGN, UNDER SEPARATE PERMIT. CONTRACTOR TO PROVIDE NECESSARY ELECTRICAL AS REQUIRED.
- 12 PAINT (N) PAVEMENT DIRECTIONAL ARROWS. SEE DETAIL 20/A-0.1.
- 13 INSTALL (N) LANDSCAPING AT MENU BOARD PER POPEYES STANDARDS, REFER TO LANDSCAPE PLANS.
- 14 (N) ACCESSIBLE PARKING, 48" WHEEL STOP, SIGNAGE AND SIDEWALK ACCESS FEATURES. G.C. TO VERIFY NO PONDING AT ACCESSIBLE AISLE. SEE DETAIL 15/A-0.2.
- 15 (N) PYLON SIGN PACKAGE, UNDER SEPARATE PERMIT BY OTHERS, REFER TO ELECTRICAL DRAWING, E-6. CONTRACTOR TO PROVIDE NECESSARY ELECTRICAL AS REQUIRED.
- 16 NECESSARY ELECTRICAL AS REQUIRED. (N) ACCESSIBLE CONCRETE WALKWAY, WITH 2% MAX CROSS SLOPE AND 5% MAX RUNNING SLOPE, FOR (N) ACCESSIBLE PATH OF TRAVEL. G.C. TO VERIFY NO PONDING AT ACCESSIBLE ROUTE.
- 17 NEW (N) ACCESSIBLE CURB RAMP, SEE SHEET A00.06.
- 18 NEW (N) DETECTABLE WARNING SURFACES.
- 19 (N) STRIPED CONCRETE ACCESSIBLE ACCESS PATH. G.C. TO VERIFY NO PONDING AT ACCESSIBLE ROUTE.
- 20 (N) ACCESSIBLE PATH TO RIGHT OF WAY AT SE. ENSIGN LANE. G.C. TO VERIFY NO PONDING AT ACCESSIBLE ROUTE.
- 21 (N) ACCESSIBLE PATH TO ACCESSIBLE PARKING. G.C. TO VERIFY NO PONDING AT ACCESSIBLE ROUTE.
- 22 (N) POLE MOUNTED SITE LIGHT ON CONCRETE PIER. REFER TO STRUCTURAL DRAWING, S-6 & ELECTRICAL DRAWING, E-6. CONTRACTOR TO PROVIDE NECESSARY ELECTRICAL AS REQUIRED.
- 23 (N) TRASH ENCLOSURE, PER CITY REQUIREMENTS. REFER STRUCTURAL DRAWING, S-6.
- 24 INSTALL (N) BICYCLE RACK WITH SPACE FOR 2 BICYCLES. EACH BICYCLE PARKING SPACE SHALL BE AT LEAST SIX FEET LONG AND TWO FEET WIDE WITH AN OVERHEAD CLEARANCE OF AT LEAST SEVEN FEET. SEE DETAIL 5/A-0.2.
- 25 EXISTING TRANSFORMER TO BE RELOCATED BY PACIFIC POWER, SEE ELECTRICAL SHEET E6.0.
- 26 RELOCATED TRANSFORMER LOCATION BY PACIFIC POWER, SEE ELECTRICAL SHEET E6.0.
- 27 PACIFIC POWER EASEMENT, SEE ALTA.

SITE PLAN KEYNOTES

NO SCALE

REQUIRED TOTAL PARKING						
	1 SPACE PER 4 SEATS	1 SPACE PER 100 SF	BUILDING SIZE (SF)	MIN. REQ.	MAX. REQ.	PROVIDED
TOTAL PARKING	24 SEATS/4	2,111/100	2111	6	21	17

*REQUIRED MAXIMUM PARKING IS BASED ON WARRENTON DEVELOPMENT CODE, TABLE 16.128.030.A 1 SPACE PER 100 SQ. FT. OF GROSS FLOOR AREA.
 MINIMUM PARKING IS BASED ON WARRENTON DEVELOPMENT CODE, TABLE 16.128.030.A 1 SPACE PER 4 SEATS.
 ** ACCESSIBLE PARKING REQUIREMENTS ARE BASED ON 2019 OSSC TABLE 1106.1

ACCESSIBLE PARKING

	REQUIRED	PROVIDED
ACCESSIBLE VAN PARKING (TABLE 1106.1)	1	1

PARKING CALCULATIONS

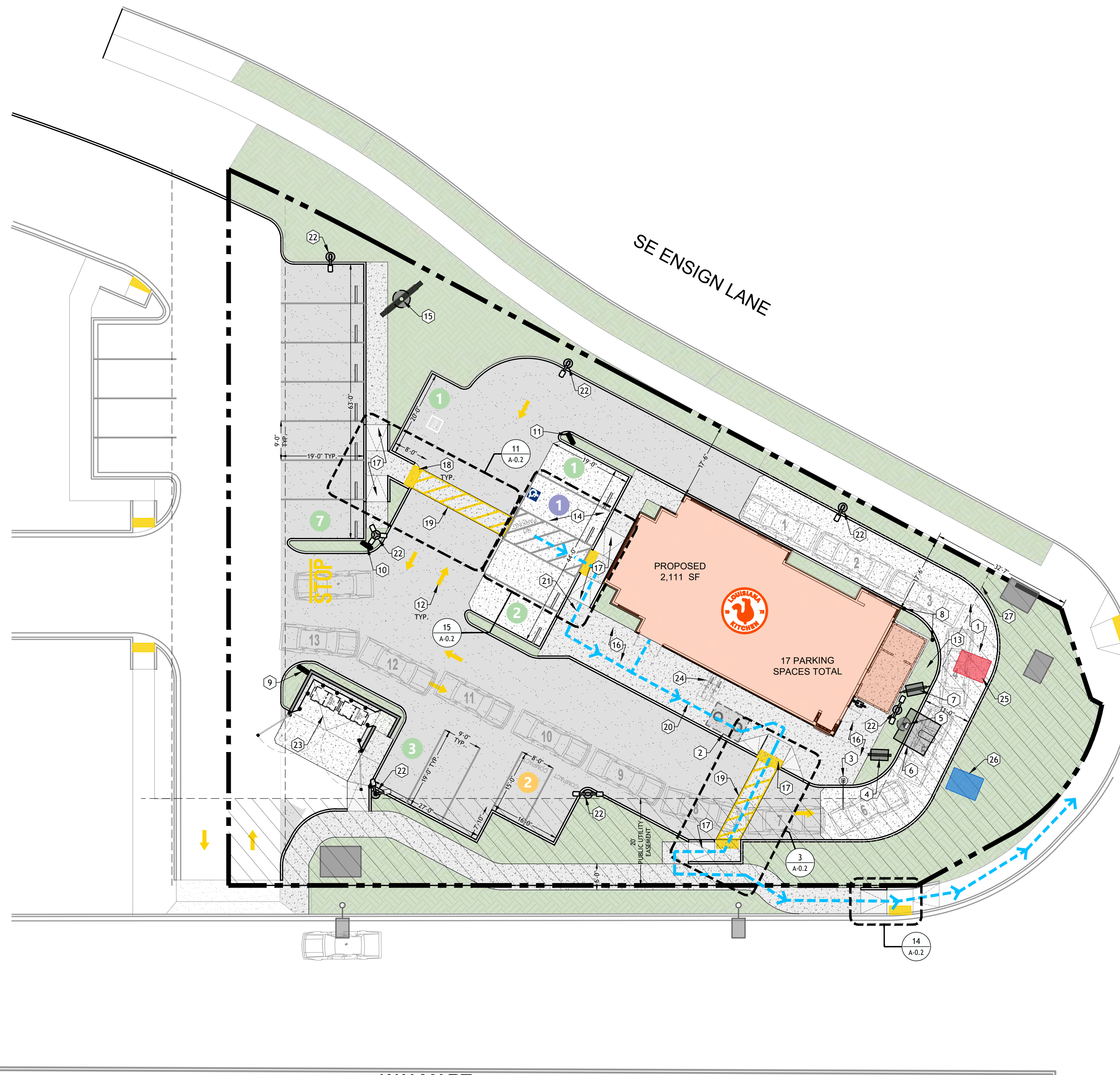
NO SCALE

KEY	DESCRIPTION
(E) STRIPED SAFETY PATH, ACCESS PATH, OR CROSSWALK	
(N) CONCRETE PAVED SURFACE	
(N) ASPHALT PAVEMENT	
(E) EASEMENT. SEE ALTA SURVEY	
POLE MOUNTED SITE LIGHT	
ACCESSIBLE PATH OF TRAVEL	
PROPERTY LINE	
(E) FENCE LINE	
#	STANDARD PARKING
#	COMPACT PARKING
#	ACCESSIBLE PARKING
#	ACCESSIBLE VAN PARKING

NOTE: NOT ALL SYMBOLS MAY BE APPLICABLE TO, AND USED, IN THIS DRAWING.

SITE PLAN LEGEND

NO SCALE



SITE PLAN

1" = 15'-0"

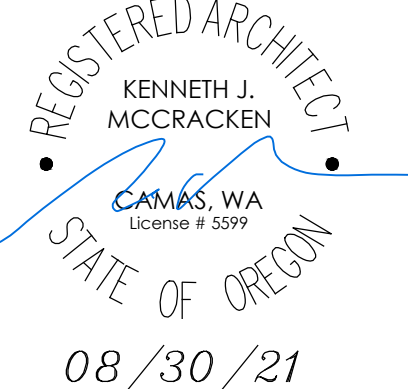
5

KEN MCCrackEN, ARCHITECT

19120 SE 34TH STREET
 SUITE 115
 VANCOUVER, WA 98683
 PROJECT CONTACT: ERICK PETERSON
 PHONE: 360.980.8238
 EMAIL: EPETERSON@PMDCINC.COM

KEN MCCrackEN, ARCHITECT

SEAL:



CONSULTANT:



LOUISIANA KITCHEN
 SITE NO.: 00xxx
 1771 SE Ensign Lane
 Warrenton, Oregon 97146

REV DATE DESCRIPTION

REV	DATE	DESCRIPTION
Δ	07.29.21	PRE-APPLICATION

DRAWN BY: CDG

CHECKED BY: ...

ARCH. PROJECT NO.:

AMB21002.0

SHEET NAME:

SITE PLAN

SHEET NUMBER:

A-0.1

- 1 Introduction** 1
- 2 General Requirements** 2
 - 2.1 Definitions and Abbreviations** 2
 - 2.2 Code Compliance and Permitting** 9
 - 2.3 Construction Standards** 9
 - 2.4 Design Submittal Requirements** 9
 - 2.4.1 General Plan Requirements** 9
 - 2.4.2 Utility Information** 10
 - 2.4.3 Street Plans** 11
 - 2.4.4 Water, Sanitary Sewer and Storm Drain Plans** 12
 - 2.4.5 Storm Water Management System Plans** 12
 - 2.4.6 Additional Storm Water Design Documentation** 13
 - 2.4.7 Refuse Enclosure Plans** 13
 - 2.5 Phased Project Submittals** 13
 - 2.6 Design Exceptions** 13
 - 2.6.1 Design Exception Submittal Requirements** 13
 - 2.6.2 Appeal Process** 14
 - 2.7 Design Approval** 14
 - 2.8 Construction Surety** 14
 - 2.9 Construction Submittal** 15
 - 2.10 Construction Coordination and Inspection** 15
 - 2.11 As-Built Documents** 16
 - 2.12 Final Project Acceptance** 16
- 3 Transportation Facilities** 17
 - 3.1 City Streets** 18
 - 3.1.1 Street Classifications** 18
 - 3.1.2 Vehicular Zone** 18
 - 3.1.2.1 Vehicular Zone Cross Section 18
 - 3.1.2.2 Design Speed 19
 - 3.1.2.3 Pavement Section 20
 - 3.1.2.4 Crown 20
 - 3.1.2.5 Vertical Clearance 20

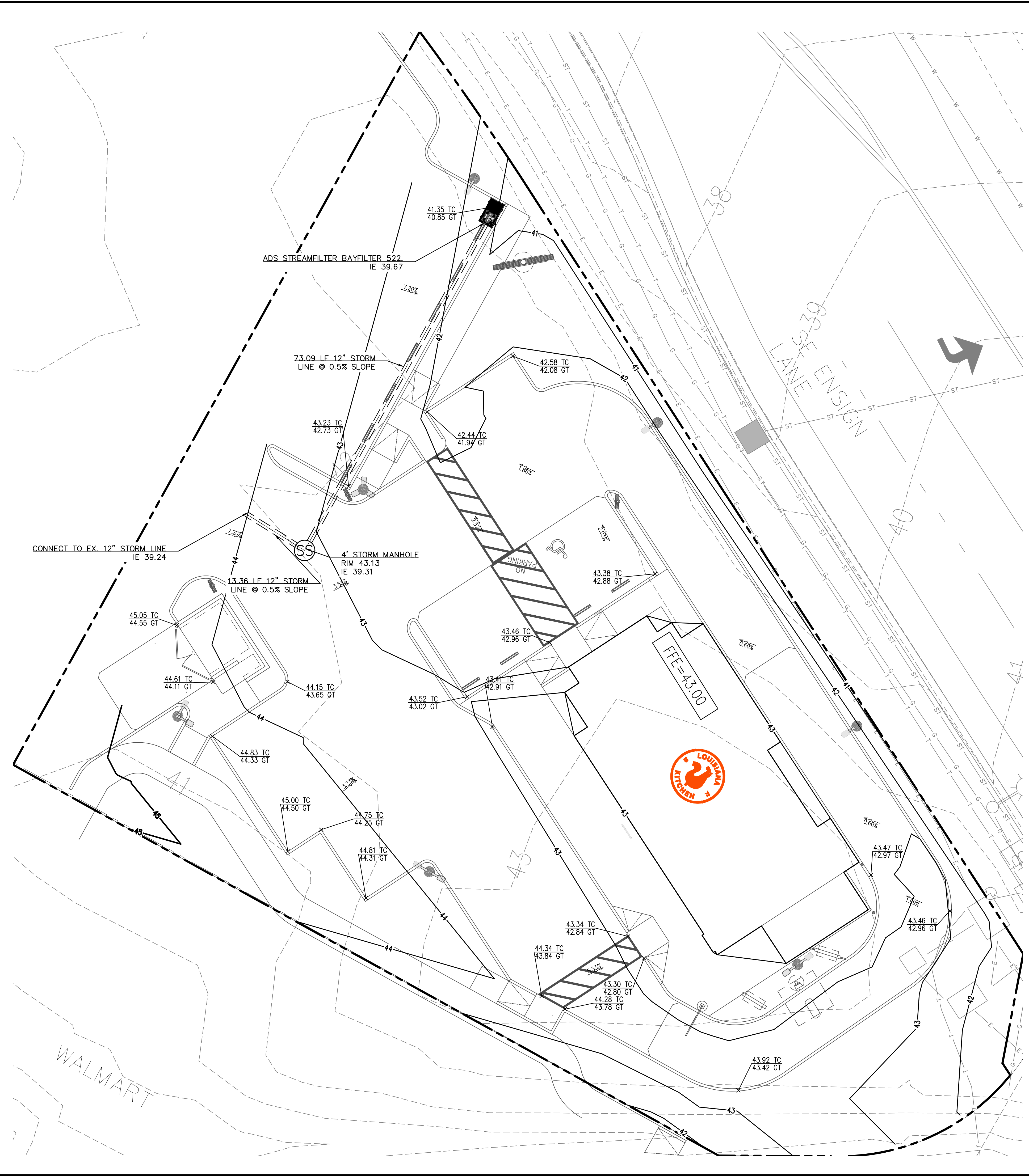
- 3.1.2.6 On-Street Parking..... 20
- 3.1.2.7 Signing and Striping..... 20
- 3.1.3 Walking Zone 20**
 - 3.1.3.1 Walking Zone Cross Section 21
 - 3.1.3.2 Curb, Gutter and Edge Area 21
 - 3.1.3.3 Landscaped Buffer 21
 - 3.1.3.4 Sidewalk 21
 - 3.1.3.5 Differential Grade Sidewalks..... 22
 - 3.1.3.6 Grading Adjacent to Sidewalks 22
- 3.1.4 Horizontal Alignment of Streets 22**
- 3.1.5 Vertical Alignment of Streets 23**
- 3.1.6 Roadway Intersections 24**
 - 3.1.6.1 Intersection Geometry 24
 - 3.1.6.2 Clear-Vision Area..... 25
- 3.1.7 Illumination and Signalization 25**
- 3.1.8 Access to City Streets 25**
 - 3.1.8.1 Driveway Access..... 25
 - 3.1.8.2 Access from Alleys and Private Streets 26
 - 3.1.8.3 Controlled Access..... 26
- 3.1.9 Cul-de-sacs 26**
- 3.1.10 Alleys..... 27**
- 3.1.11 Roadway Drainage 27**
- 3.2 Privately Maintained Streets 27**
- 3.3 Non-Motorized Facilities 28**
 - 3.3.1 Sidewalks 28
 - 3.3.2 Bicycle Facilities 29
 - 3.3.3 Multi-Use Pathways 29
 - 3.3.4 Public Stairways..... 29
 - 3.3.5 Street Crossing..... 29
- 3.4 Transit Facilities 30**
- 3.5 Landscaping within the Right-of-Way 30**
 - 3.5.1 **General Requirements 30**

- 3.5.2 Clearance 30
- 3.5.3 Street Tree Installation..... 30
- 3.5.4 Suggested Types of Trees 30
- 3.6 Transportation Facility Construction and Maintenance Responsibilities 31
- 3.7 Formal Acceptance of Transportation Facilities..... 32
- 4 Sanitary Sewers 33
 - 4.1 Design Flows 33
 - 4.2 Sanitary Sewer Pipes 34
 - 4.2.1 Sanitary Sewer Pipe Material..... 34
 - 4.2.2 Sanitary Sewer Main Pipe Size..... 34
 - 4.2.3 Sanitary Sewer Pipe Slope..... 34
 - 4.2.4 Sanitary Sewer Depth..... 35
 - 4.2.5 Sanitary Sewer Laterals..... 35
 - 4.3 Manholes and Cleanouts..... 35
 - 4.3.1 Location..... 35
 - 4.3.2 Standard Manhole..... 36
 - 4.3.2.1 Manhole Channels 36
 - 4.3.3 Drop Manhole..... 36
 - 4.3.4 Manhole Scour Protection 37
 - 4.3.5 Manhole corrosion protection..... 37
 - 4.3.6 Manhole odor control 37
 - 4.3.7 Connections to Existing Manholes..... 37
 - 4.4 Sanitary Sewer Alignment..... 37
 - 4.4.1 Sanitary Sewer Easements 37
 - 4.4.2 Clearance from Other Utilities..... 37
 - 4.5 Sanitary Sewer Connections 39
 - 4.5.1 Connection of Sanitary Sewer Laterals..... 40
 - 4.5.1.1 Backwater Valves 40
 - 4.5.2 Connection of New Sanitary Sewer Mains 40
 - 4.5.3 Pre-Treatment Requirements for Sanitary Sewer Connections 40
 - 4.5.3.1 Food Service Establishments..... 40
 - 4.5.3.2 Industrial users..... 41

- 4.6 Low Pressure Sewer Systems** 41
 - 4.6.1 Design of Low Pressure Sewer Systems** 42
 - 4.6.1.1 Public System 42
 - 4.6.1.2 Private System..... 42
- 4.7 Wastewater Pump Stations** 42
 - 4.7.1 General Requirements** 42
 - 4.7.2 City Water** 42
 - 4.7.3 Check Valve and Pigging Vault** 43
 - 4.7.4 Transfer Switch** 43
 - 4.7.4.1 Flow meter 43
 - 4.7.5 Control Panels**..... 43
 - 4.7.6 Electrical Supply**..... 44
 - 4.7.7 Control System** 45
 - 4.7.8 Power Generator** 46
 - 4.7.9 Control Building**..... 46
 - 4.7.11 Wet Well** 47
 - 4.7.12 Site Requirements** 47
 - 4.7.13 Pumps, Piping, and Controls** 48
 - 4.7.14 Telemetry Equipment**..... 49
- 4.8 Sanitary Sewer System Testing** 49
- 5 Storm Water Management Systems**..... 51
 - 5.1 General Guidelines** 51
 - 5.2 Storm Water Design Documentation** 51
 - 5.3 Temporary Storm Water and Erosion Control**..... 51
 - 5.4 Permanent Storm Water Management System** 51
 - 5.4.1 Flow Calculations and Sizing Methodology** 51
 - 5.4.2 Inlets, Catch Basins and Storm Water Pre-Treatment** 52
 - 5.4.3 Storm Drain Manholes** 52
 - 5.4.4 Storm Drain Pipes** 52
 - 5.4.5 Connection of Private Drains to the Public Storm Drain**..... 52
 - 5.4.6 Vegetated Storm Water Facilities** 53
 - 5.4.7 Retention and Detention Facilities** 53
 - 5.4.7.1 Basin..... 53

- 5.4.7.2 Detention Flow Control Structures 53
- 5.4.8 Storm Drain System Easements 54
- 5.4.9 Operation and Maintenance Plans 55
- 5.4.10 Storm Drain System Testing 55
- 6 Water Systems 56
- 6.1 City Pressure Zones 56
- 6.2 Design Flow Rates 56
- 6.3 Water Pipes 57
- 6.3.1 Water Pipe Size 57
- 6.3.2 Water Pipe Material 57
- 6.3.3 Water Pipe Depth 58
- 6.4 Water Line Alignment 58
- 6.4.1 Water Line Easements 59
- 6.4.2 Clearance from Other Utilities 59
- 6.5 Valves 60
- 6.6 Fittings 61
- 6.7 Fire Hydrants 61
- 6.8 Combination Air/Vacuum Release Valves 62
- 6.9 Water Service Lines 62
- 6.9.1 City Service Lines and Meters 62
- 6.9.2 Fire Service Lines 64
- 6.9.3 Fire Vaults 65
- 6.10 Backflow Prevention 65
- 6.11 Boosters Stations 65
- 6.12 Water System Testing 65
- 7 Refuse and Recycling Routing and Enclosures 67
- 7.1 Access 67
- 7.2 Enclosure Design 68
- 7.2.1 Enclosure and Apron Concrete Pad 68
- 7.2.2 Construction and Design 68
- 7.2.3 Storm Water Collection 69
- 7.2.4 Wastewater Pollution Prevention 69

J:\POPEYE'S\JM-AMB21002.0 - POPEYE'S WARRENTON, OR\CURRENT DRAWINGS\WCE 24X36 MASTER.DWG 9/16/2021 9:24 AM RYAN MELLETT

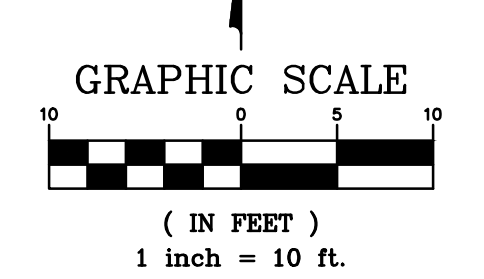


GRADING LEGEND:

- PROPERTY LINE
- PROPOSED CURB & GUTTER
- LIMITS OF FULL DEPTH SAWCUT
- EXISTING CONTOUR
- PROPOSED CONTOUR

	ADA RAMP		GREASE TRAP
	BOLLARD		CLEAN-OUT
	TRAFFIC/ACCESSIBLE SIGN		DOUBLE CLEAN-OUT
	DIRECTIONAL SIGN		GAS METER
	SPEAKER BOX		WATER METER
	MENU BOARD		IRRIGATION METER
	LIGHT POLE		MANHOLE
	BUILDING UP LIGHTS		CURB INLET
	AREA DRAIN		DRAINAGE SLOPE AND DIRECTION

× XXXX EX EXISTING SPOT ELEVATION
 × XXXX TP TOP OF PAVEMENT ELEVATION
 × XXXX TC TOP OF CURB
 × XXXX GT GUTTER
 × XXXX GR GROUND
 × XXXX FF FINISH FLOOR
 × XXXX SW SIDEWALK
 × XXXX TG TOP OF GRATE



19120 SE 34th St., Suite 115
Vancouver, WA 98683
Ph. 214-491-1830
John Measels, PE
CIVIL ENGINEER



LOUISIANA KITCHEN - POPEYES
S.E. ENSIGN LANE & S.E. DISCOVERY LANE
WARRENTON, OR 97146

FLOODNOTE
ACCORDING TO THE F.I.R.M. NO. 41007C0218F, THE SUBJECT PROPERTY LIES IN ZONE X AN AREA WITH MODERATE TO LOW RISK, PER MAP REVISION DATED JUNE 20, 2018.

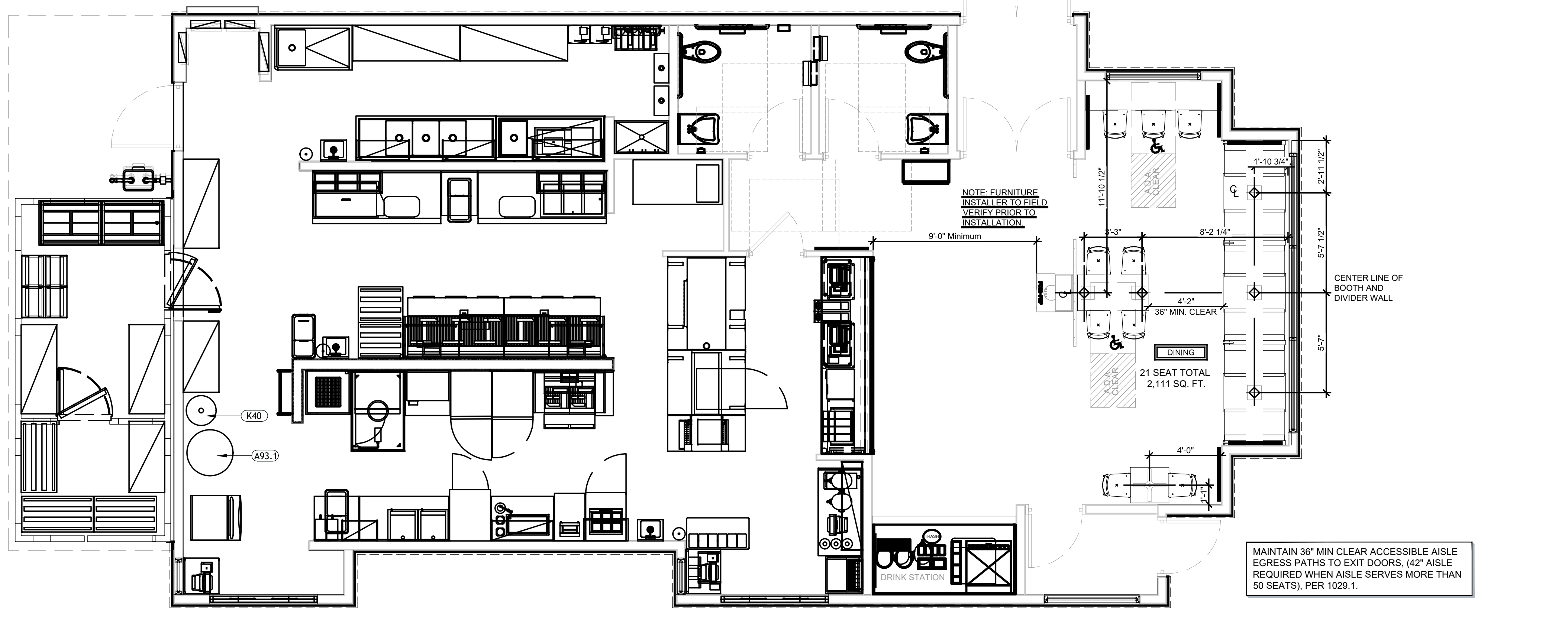
CAUTION NOTICE TO CONTRACTOR
THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS ARE BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND, WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE APPROPRIATE UTILITY COMPANY AT LEAST 7 DAYS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES HORIZONTALLY AND VERTICALLY WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS.



PROJECT NO.	DATE	DESCRIPTION
JM-AMB21002.0	09/14/21	INITIAL SUBMITTAL
DRAWN BY:		
CHECKED BY:		
ISSUE DATE:	09/14/21	

GRADING PLAN

C 300



SPECIFICATIONS

DIVISION 11: EQUIPMENT

GENERAL PROVISIONS

- SCOPE: COORDINATE WITH THE INSTALLATION OF ALL EQUIPMENT ITEMS SHOWN ON PLANS AND SCHEDULED IN EQUIPMENT SCHEDULE (EXCEPT AS NOTED AS INSTALLED BY KITCHEN CONTRACTOR) WHICH ARE FURNISHED BY THE OWNER OR UNDER SEPARATE CONTRACT. EQUIPMENT SCHEDULE LISTS TRADES RESPONSIBLE FOR FURNISHING, INSTALLING AND FINAL CONNECTION.
- SUBMISSIONS: PROVIDE THE OWNER, AT THE COMPLETION OF THIS CONTRACT, WITH AN "OWNER'S MANUAL" SO LABELED. THE MANUAL SHALL CONSIST OF A THREE-RING LOOSE-LEAF BINDER CONTAINING ALL PRINTED MATTER SUCH AS: GUARANTEE CARDS, CLEANING INSTRUCTIONS, NOTICES TO OWNER, OPERATING MANUALS, SERVICE AGENTS AND MAINTENANCE INSTRUCTIONS THAT MAY BE CONTAINED IN THE SHIPPING CARTON OF EQUIPMENT AND SPECIALTIES.
- DELIVERY AND STORAGE: RECEIVE, UNLOAD, AND SAFEGUARD THE EQUIPMENT. COORDINATE SHIPPING TIME WITH OWNER.
- PROTECTION AND CLEANING: SURFACES SHALL BE CLEANED BEFORE FINAL INSPECTION.

MATERIALS

- SEE EQUIPMENT SCHEDULE

PERFORMANCE

- INSTALL EQUIPMENT ACCORDING TO NFPA 96 AND MANUFACTURER'S INSTRUCTIONS, PROVIDE FACTORY AUTHORIZED START & ADJUSTMENT.

KOLPAK REFRIGERATION

GENERAL:
 INTERIOR FINISH: 26 GAUGE EMBOSSED GALVALUME
 EXTERIOR FINISH: 26 GAUGE EMBOSSED GALVALUME
 INTERIOR FLOORS: ERA 100 SMOOTH ALUMINUM
 INSULATION: 4" 100% FOAMED IN PLACE POLYURETHANE
 THERMOMETER: 2" DIAL THERMOMETER
 SCREDS: METAL FOR OUTDOOR MODELS

OUTDOOR PACKAGE REQUIREMENTS:
 OUTDOOR UNITS REQUIRE THE OUTDOOR PACKAGE FOR REFRIGERATION AND ROOF SEALING SYSTEM. OUTDOOR UNITS REQUIRE A FIELD SUPPLIED AND FIELD INSTALLED CONDENSATE DRAINLINE (INCLUDING HEAT TAPE ON FREEZER DRAINS).

OUTDOOR APPLICATIONS ARE DETERMINED BY THE LOCAL AND REGULATORY REQUIREMENTS FOR THAT AREA. OUTDOOR KOLPAK FOR HIGH WIND AREAS SHALL REFER TO: MANUFACTURER'S INSTALLATION INSTRUCTIONS

DIVISION 12: FURNISHINGS

GENERAL PROVISIONS

- SCOPE: COORDINATE INSTALLATION OF ARTWORK, SEATING, FREE-STANDING CABINETS AND SHELVING, WINDOW TREATMENT, FLOOR MATS, AND ACCESSORIES WHICH ARE FURNISHED UNDER SEPARATE CONTRACT TO THE OWNER. IF REQUESTED THROUGH THE CONTRACT, INSTALL DECOR, SEATING, FREE-STANDING CABINETS AND SHELVING, WINDOW TREATMENT, FLOOR MATS AND/OR ACCESSORIES.
- NOTES: DETAILS AND MATERIALS SHOWN ON THE APPROVED DECOR DRAWINGS CONFLICTING WITH THE STANDARD PLANS SHALL BE BROUGHT TO THE ATTENTION OF THE ARCHITECT IMMEDIATELY PRIOR TO COMMENCEMENT OF THE INSTALLATION. VERIFICATION OF ADA COMPLIANCE WILL BE NECESSARY.
- DELIVERY AND STORAGE: RECEIVE AND SAFEGUARD OWNER SUPPLIED ITEMS ON THE JOB SITE IF REQUESTED.

PERFORMANCE

- INSTALLATION: PREPARE SURFACES TO RECEIVE THESE MATERIALS AND COOPERATE WITH THE INSTALLATION OF DECOR MATERIALS AS SHOWN ON THE DECOR DRAWINGS.

SEATING PLACEMENT

1/4" = 1'-0" **1**

NOTES

- SYMBOL**
- TEXT EQUIPMENT No.
- IT IS THE RESPONSIBILITY OF THE G.C. TO COORDINATE DELIVERY, UNCRATING, POSITIONING, FINAL HOOK-UP AND REMOVAL OF TRASH OF ALL OWNER SUPPLIED KITCHEN EQUIPMENT.
 - ALL KITCHEN EQUIPMENT TO BE INSTALLED AS PER MANUFACTURER'S SPECIFICATIONS.
 - DECOR ITEMS SUPPLIED BY OWNER AND INSTALLED BY GENERAL CONTRACTOR.

SEATING

SEATS:	24
GROUPS:	6
RATIO:	4.0
BUILDING SQUARE FOOTAGE	
KITCHEN (NET):	983
DINING/RESTROOM (NET):	667
TOTAL (NET) :	1,650
TOTAL (GROSS) :	2,111
WALK-IN SQUARE FOOTAGE	
WALK-IN (NET):	148

NOTES:

- FLOOR MATS SHOULD BE PLACED IN THE THE AREAS DESCRIBED BELOW:
 ENTRY WAY MATS: 3M NOMAD, AVAILABLE SIZES 3'x5' OR 4'x6'
 KITCHEN FLOOR MATS: MATRIX 'GRIP ROCK'
 LOCATIONS:
 -INSIDE WALK-IN COOLER (3'-0" x 5'-6")
 -OUTSIDE THE WALK-IN COOLER (3'-0" x 4'-0")
 -ICE MACHINE (3'-0" x 4'-0")
 -3 COMPARTMENT SINK (3'-0" x 7'-6")
 -CONDIMENT COUNTER (3'-0" x 8'-10")
- EQUIPMENT SUBSTITUTIONS REQUIRE POPEYES PRE-APPROVAL. SUBMIT CUT SHEETS TO POPEYES DEPARTMENT

KITCHEN EQUIPMENT, SEATING, AND SMALLWARES PACKAGE

- TRIMARK (EQUIPMENT, SEATING, & SMALLWARES)
 CONTACT: BOB PODNER; PHONE: (972) 516-1421;
 BPODNER@TRIMARKUSA.COM
- WASSERSTROM & SONS INC. (EQUIPMENT & SEATING)
 CONTACT: JOE BUSH ; PHONE: (614) 737-8534;
 JOEBUSH@WASSERSTROM.COM
- WASSERSTROM & SONS INC. (SMALLWARES)
 CONTACT: JEFF RAUPPLE; PHONE: (614) 737-8381;
 JEFFRAUPPLE@WASSERSTROM.COM
- HOCKENBERGS (EQUIPMENT, SEATING, & SMALLWARES)
 CONTACT: TED MANDS; PHONE: (770) 594-2626 X 4123;
 TEDM@HOCKENBERGS.COM
- H & K INTERNATIONAL (EQUIPMENT, SEATING, & SMALLWARES)
 CONTACT: MORGAN KIZER; PHONE: (214) 818-3556;
 Morgan.Kizer@hki.com
- CONCEPT SERVICES (EQUIPMENT, SEATING, & SMALLWARES)
 CONTACT: ERIC SMITH PHONE: (512) 343-3100/CELL (512) 573-2192;
 ERICSMITH@CONCEPTSERV.COM

SEATING PACKAGE

- SEATING CONCEPTS
 CONTACT:
 JEFF ALLISON; DIRECT (800) 421-2036 CELL (417) 793-9465;
 JALLISON@SEATING-CONCEPTS.COM
 KERRY STUDDER; PHONE: (815) 730-7980 EXT2223;
 KSTUDDER@SEATING-CONCEPTS.COM
- MSW RESTAURANT FURNISHINGS
 CONTACT:
 ADAM MURRAY; DIRECT (417) 673-1901 X1040/CELL (417) 388-1616;
 ADAM@MSWINC.COM
- CASABLANCA DESIGN GROUP
 CONTACT:
 MARK WIRZ; DIRECT (770) 423-9575 CELL (770) 778-1426;
 MARK.WIRZ@CDG.US.COM
- JBI INTERIORS
 CONTACT: ANDY BRADY; PHONE (770) 329-6805
 ABRADY@JBI-INTERIORS.COM

INTERIOR SIGN PACKAGE-BY.G.C.

AREA	SIGN NAME	QTY	MOUNTING LOCATION	COMMENTS
FRONT OF HOUSE	HOURS OF OPERATION	2	CUSTOMER ENTRANCE DOORS, 48" AFF	MOUNT ON WINDOW NEXT TO DOOR, IF POSSIBLE
	DOOR TRAFFIC FLOW SIGNS			
	IN	2	CUSTOMER ENTRANCE DOORS, 48" AFF	MOUNT DIRECTLY ABOVE PUSH BAR OF ENTRANCE DOORS
	OUT	2	CUSTOMER ENTRANCE DOORS, 48" AFF	MOUNT DIRECTLY ABOVE PUSH BAR OF ENTRANCE DOORS
	LOBBY FLOW SIGN	2	SITS ON TOP OF SERVICE COUNTER	3 SIDED SIGN, "ORDER HERE", "PAY HERE", "NEXT REGISTER"
	QUEUEING LINE FLOW SIGN	1	TOP OF QUEUEING LINE POLE	2 SIDED SIGN, "ENTER HERE", "PLEASE COME AGAIN"
	RESTROOM SIGNS		LATCH-SIDE WALL, MTD, C 60" A.F.F.	HANDICAPPED
	"HANDICAP WOMEN"	1	EXTERIOR OF WOMEN'S RESTROOM DOOR, 60" AFF	MOUNT TO CENTER LINE OF SIGN
	"HANDICAP MEN"	1	EXTERIOR OF MEN'S RESTROOM DOOR, 60" AFF	MOUNT TO CENTER LINE OF SIGN
	"RESTROOMS"	1	VISIBLE TO CUSTOMERS IN DINING AREA, 60" AFF	MOUNT TO CENTER LINE OF SIGN
	"EMPLOYEES MUST WASH HANDS..."	2	INTERIOR OF BOTH RESTROOMS DOOR, 60" AFF	
	"NO SMOKING"	5	DINING ROOM	
	"EMPLOYEES ONLY"	1	ENTRANCE BETWEEN KITCHEN AND DINING ROOM	
	TEA URN SIGNS			
	"UNSWEETENED TEA"	1	HANGS ON DISPENSER	2 SIDED SIGN
"SWEETENED TEA"	1	HANGS ON DISPENSER	2 SIDED SIGN	
"WATER"	1	HANGS ON DISPENSER	2 SIDED SIGN	
BACK OF HOUSE	BIN DECALS			
	"FLOUR"	1	PLACE ON FLOUR BIN	STICKER APPLICATION
	"RICE"	1	PLACE ON RICE BIN	STICKER APPLICATION
	"FILTER"	1	PLACE ON FILTER POWDER BIN	STICKER APPLICATION
	3 COMPARTMENT SINK DECALS			
	"WASH"	1	PLACE ON BACK SPLASH OVER APPROPRIATE SINK	STICKER APPLICATION
	"RINSE"	1	PLACE ON BACK SPLASH OVER APPROPRIATE SINK	STICKER APPLICATION
	"FILTER"	1	PLACE ON BACK SPLASH OVER APPROPRIATE SINK	STICKER APPLICATION
	"HOT! USE CAUTION"	6	MICROWAVE WINDOW, OVEN, PRODUCT DISPLAY AREAS	STICKER APPLICATION
	"MUST BE 18..."	4	FRYER, FILTER, MARINATOR, (IF APPLICABLE)	STICKER APPLICATION
FRYER LABELS: (1,2,3,4,5,6)	1	PLACE ON APPROPRIATE FRYER	STICKER APPLICATION	
SECURITY SIGNS	"STOP! ALL VENDORS MUST..."	1	EXTERIOR OF BACK DOOR, 60" AFF	
	"WARNING! ONLY MANAGERS..."	1	EXTERIOR OF BACK DOOR, 48" AFF	
	"MANAGERS...ACCESS TO SAFE"	1	EXTERIOR OF BACK DOOR, 48" AFF	
	"CHEMICAL STORAGE ONLY"	1	ON OR OVER AREA TO STORE CHEMICALS	MOUNT TO SHELVING UNIT OR WALL
OTHER	HANDICAP PARKING SIGN	2	DESIGNATED PARKING SPOT (S) 84" TO BOT. OF SIGN	

KEN McCracken, ARCHITECT
 Architecture • Program Management • Permitting
 19120 SE 34TH STREET SUITE 115
 VANCOUVER, WA 98683
 PROJECT CONTACT: ERICK PETERSON
 PHONE: 360.980.8258
 EMAIL: EPETERSON@PMDCINC.COM

KEN MCCRACKEN, ARCHITECT
 SEAL: REGISTERED ARCHITECT
 KENNETH J. MCCRACKEN
 CAMAS, WA
 License # 5599
 STATE OF OREGON
 08/30/21
 KJM/xxxx

CONSULTANT:

LOUISIANA KITCHEN
 19 72
 POPEYES
 LOUISIANA KITCHEN

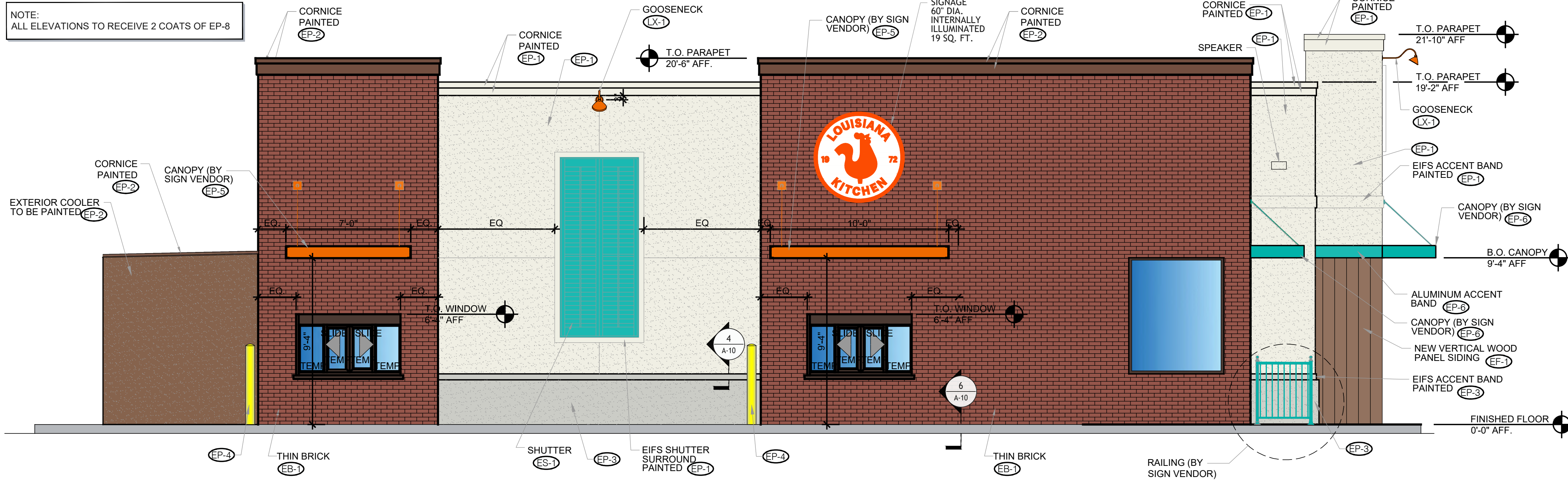
LOUISIANA KITCHEN
 SITE NO.: 00xxx
 1771 SE Ensign Lane
 Warrenton, Oregon 97146

REV	DATE	DESCRIPTION
△	07.29.21	PRE-APPLICATION

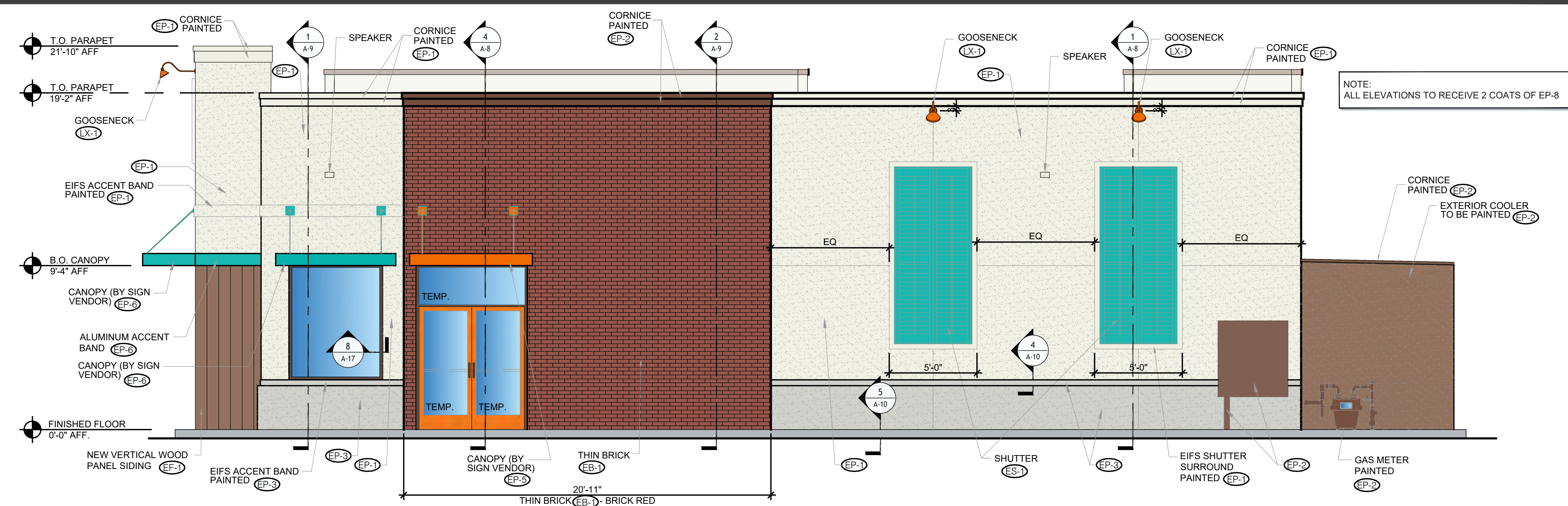
DRAWN BY: CDG
 CHECKED BY: ...
 ARCH. PROJECT NO.:
AMB21002.0
 SHEET NAME:
SEATING PLACEMENT
 SHEET NUMBER:
A-18

PRINTED BY: CADDEPT
 DRAWING NAME: FOX 1846 03 A18.DWG
 PRINT DATE: Sep 13, 2021 - 7:53am

NOTE:
ALL ELEVATIONS TO RECEIVE 2 COATS OF EP-8



DRIVE-THRU ELEVATION 1/4" = 1'-0" 1



MAIN ENTRY ELEVATION 1/4" = 1'-0" 2

SECTION TITLE
ALUMINUM I-FRAMED ENTRANCES AND STOREFRONTS YKK AP PRODUCT SERIES YKK AP YES 45 FS/FI SERIES

PART 1 GENERAL

1. SECTION INCLUDES: ALUMINUM STOREFRONT, INCLUDING:

- 1.1. YKK AP SERIES YES 45 FI STOREFRONT SYSTEM (INSULATING GLAZING)

1. RELATED SECTIONS:

- 1.1. SINGLE SOURCE REQUIREMENT: ALL PRODUCTS LISTED BELOW SHALL BE BY THE SAME MANUFACTURER.

2. SYSTEM PERFORMANCE DESCRIPTION

- 2.1. PERFORMANCE REQUIREMENTS: PROVIDE ALUMINUM STOREFRONT SYSTEMS THAT COMPLY WITH PERFORMANCE REQUIREMENTS INDICATED, AS DEMONSTRATED BY TESTING MANUFACTURER'S ASSEMBLIES IN ACCORDANCE WITH TEST METHOD INDICATED.
- 2.2. AIR INFILTRATION: WHEN TESTED IN ACCORDANCE WITH ASTM E 283 AT DIFFERENTIAL STATIC PRESSURE OF 6.24 PSF (299 PA), COMPLETED STOREFRONT SYSTEMS SHALL HAVE MAXIMUM ALLOWABLE INFILTRATION OF:
 - 2.2.1. 0.06 CFM/FT² (1.10 M³/H · M²) FOR YES 45 FI SYSTEMS.
 - 2.2.2. WATER INFILTRATION: NO UNCONTROLLED WATER WHEN TESTED IN ACCORDANCE WITH ASTM E 331 AT TEST PRESSURE DIFFERENTIAL OF:
 - 2.2.2.1. 10 PSF (479 PA) FOR YES 45 FI SYSTEMS.
 - 2.2.3.1. FIELD TESTED IN ACCORDANCE WITH AAMA 503. FASTENER HEADS MUST BE SEALED AND SEALED AGAINST SILL FLASHING ON ANY FASTENERS THAT PENETRATE THROUGH THE SILL FLASHING.
- 2.4. WIND LOADS: COMPLETED STOREFRONT SYSTEM SHALL WITHSTAND WIND PRESSURE LOADS NORMAL TO WALL PLANE INDICATED:
 - 2.4.1. EXTERIOR WALLS:
 - 2.4.1.1. POSITIVE PRESSURE: PER LOCAL CODE.
 - 2.4.1.2. NEGATIVE PRESSURE: PER LOCAL CODE.
 - 2.4.1.3. INTERIOR WALLS (PRESSURE ACTING IN EITHER DIRECTION): PER LOCAL CODE.
- 2.5. DEFLECTION: MAXIMUM ALLOWABLE DEFLECTION IN ANY MEMBER WHEN TESTED IN ACCORDANCE WITH ASTM E 330 WITH ALLOWABLE STRESS IN ACCORDANCE WITH AA SPECIFICATIONS FOR ALUMINUM STRUCTURES.

2.5. DEFLECTION: MAXIMUM ALLOWABLE DEFLECTION IN ANY MEMBER WHEN TESTED IN ACCORDANCE WITH ASTM E 330 WITH ALLOWABLE STRESS IN ACCORDANCE WITH AA SPECIFICATIONS FOR ALUMINUM STRUCTURES.

2.6. THERMAL MOVEMENT: PROVIDE FOR THERMAL MOVEMENT CAUSED BY 180 DEGREES F (82.2 DEGREES C) SURFACE TEMPERATURE, WITHOUT CAUSING BUCKLING STRESSES ON GLASS, JOINT SEAL FAILURE, UNDUE STRESS ON STRUCTURAL ELEMENTS, DAMAGING LOADS ON FASTENERS, REDUCTION OF PERFORMANCE, OR DETRIMENTAL EFFECTS.

2.7. THERMAL PERFORMANCE: WHEN TESTED IN ACCORDANCE WITH AAMA 1503.1 AND NFRC 100:

- 2.7.1. CONDENSATION RESISTANCE FACTOR (CRF F): A MINIMUM OF 44 FOR YES 45 FI SYSTEMS.
- 2.7.2. THERMAL TRANSMITTANCE U VALUE: 0.51 BTU/HR/FT²/F OR LESS FOR YES 45 FI SYSTEMS.

NOTE: THERMAL PERFORMANCE FOR THE GLAZED SYSTEM AS A WHOLE WILL BE AFFECTED BY THE CHARACTERISTICS OF THE GLASS SPECIFIED.

SUBMITTALS

- 2.1. GENERAL: PREPARE, REVIEW, APPROVE, AND SUBMIT SPECIFIED SUBMITTALS IN ACCORDANCE WITH "CONDITIONS OF THE CONTRACT" AND DIVISION 1 SUBMITTALS SECTIONS, PRODUCT DATA, SHOP DRAWINGS, SAMPLES, AND SIMILAR SUBMITTALS ARE DEFINED IN "CONDITIONS OF THE CONTRACT"
- 2.2. PRODUCT DATA: SUBMIT PRODUCT DATA FOR EACH TYPE STOREFRONT SERIES SPECIFIED AS REQUIRED BY THE ARCHITECT OF RECORD.
- 2.3. SUBSTITUTIONS: WHENEVER SUBSTITUTE PRODUCTS ARE TO BE CONSIDERED, SUPPORTING TECHNICAL DATA, SAMPLES, AND TEST REPORTS MUST BE SUBMITTED TEN (10) WORKING DAYS PRIOR TO BID DATE IN ORDER TO MAKE A VALID COMPARISON.
- 2.4. SHOP DRAWINGS: PROVIDE TO THE ARCHITECT OF RECORD IF REQUESTED. SHOP DRAWINGS SHOWING LAYOUT, PROFILES, AND PRODUCT COMPONENTS, INCLUDING ANCHORAGE, ACCESSORIES, FINISH COLORS AND TEXTURES.
- 2.5. QUALITY ASSURANCE / CONTROL SUBMITTALS:
 - 2.5.1. TEST REPORTS: SUBMIT CERTIFIED TEST REPORTS SHOWING COMPLIANCE WITH SPECIFIED PERFORMANCE CHARACTERISTICS AND PHYSICAL PROPERTIES IF REQUIRED BY THE LOCAL PERMITTING AUTHORITY.
 - 2.6. CLOSEOUT SUBMITTALS:
 - 2.6.1. WARRANTY: SUBMIT WARRANTY DOCUMENTS SPECIFIED HEREIN.

QUALITY ASSURANCE

1. QUALIFICATIONS:

- 1.1. INSTALLER QUALIFICATIONS: INSTALLER EXPERIENCED (AS DETERMINED BY CONTRACTOR) TO PERFORM WORK OF THIS SECTION WHO HAS SPECIALIZED IN THE INSTALLATION OF WORK SIMILAR TO THAT REQUIRED FOR THIS PROJECT. IF REQUESTED BY OWNER, SUBMIT REFERENCE LIST OF COMPLETED PROJECTS.
- 2.2. PRE-INSTALLATION MEETINGS: CONDUCT PRE-INSTALLATION MEETING TO VERIFY PROJECT REQUIREMENTS, SUBSTRATE CONDITIONS, MANUFACTURER'S INSTALLATION INSTRUCTIONS, AND MANUFACTURER'S WARRANTY REQUIREMENTS.

3. PROJECT CONDITIONS / SITE CONDITIONS:

- 3.1. FIELD MEASUREMENTS: VERIFY ACTUAL MEASUREMENTS / OPENINGS BY FIELD MEASUREMENTS BEFORE FABRICATION; RETAIN RECORDED MEASUREMENTS ON SHOP DRAWINGS. COORDINATE FIELD MEASUREMENTS, FABRICATION SCHEDULE WITH CONSTRUCTION PROGRESS TO AVOID CONSTRUCTION DELAYS.

4. WARRANTY

- 4.1. PROJECT WARRANTY: REFER TO "CONDITIONS OF THE CONTRACT" FOR PROJECT WARRANTY PROVISIONS.
- 4.2. MANUFACTURER'S WARRANTY: SUBMIT, FOR OWNER'S ACCEPTANCE, MANUFACTURER'S STANDARD WARRANTY DOCUMENT EXECUTED BY AN AUTHORIZED COMPANY OFFICIAL.
- 4.3. WARRANTY PERIOD: MANUFACTURER'S ONE (1) YEAR STANDARD WARRANTY COMMENCING ON THE SUBSTANTIAL DATE OF COMPLETION FOR THE PROJECT PROVIDED THAT THE WARRANTY, IN NO EVENT, SHALL START LATER THAN SIX (6) MONTHS FROM THE DATE OF SHIPMENT BY YKK AP AMERICA INC. EDITOR NOTE: LONGER WARRANTY PERIODS ARE AVAILABLE AT ADDITIONAL COST.

5. PART 2 PRODUCTS

- 5.1. MANUFACTURERS (ACCEPTABLE MANUFACTURERS/PRODUCTS)
- 5.2. ACCEPTABLE MANUFACTURERS: YKK AP AMERICA INC.
- 5.3. STOREFRONT SYSTEM: YKK AP YES 45 FI STOREFRONT SYSTEM.
6. COLORS: AS SPECIFIED BY POPEYES LOUISIANA KITCHEN.

INSTALLATION

1. GENERAL: INSTALL MANUFACTURER'S SYSTEM IN ACCORDANCE WITH SHOP DRAWINGS, AND WITHIN SPECIFIED TOLERANCES.

- 1.1. PROTECT ALUMINUM MEMBERS IN CONTACT WITH MASONRY, STEEL, CONCRETE, OR DISSIMILAR MATERIALS USING NYLON PADS OR BITUMINOUS COATING.
2. SHIM AND BRACE ALUMINUM SYSTEM BEFORE ANCHORING TO STRUCTURE.

3. SHIM AND BRACE ALUMINUM SYSTEM BEFORE ANCHORING TO STRUCTURE.

4. PROVIDE SILL FLASHING AT EXTERIOR STOREFRONT SYSTEMS. EXTEND EXTRUDED FLASHING CONTINUOUS WITH SPLICE JOINTS; SET IN CONTINUOUS BEADS OF SEALANT.

5. VERIFY STOREFRONT SYSTEM ALLOWS WATER ENTERING SYSTEM TO BE COLLECTED IN GUTTERS AND WEPT TO EXTERIOR. VERIFY METAL JOINTS ARE SEALED IN ACCORDANCE WITH MANUFACTURER'S INSTALLATION INSTRUCTIONS.

SECTION 8D: GLAZING

GENERAL PROVISIONS

1. SCOPE: FURNISH AND INSTALL GLASS IN STOREFRONT AND DRIVE-THRU SERVICE WINDOW.

MATERIALS

1. TYPE: SOLAR CONTROL LOW-E CLEAR INSULATING GLASS "SOLARBAN 60 (2) CLEAR + CLEAR BY VITRO ARCHITECTURAL GLASS.
 - 1.1. OUTDOOR LITE: CLEAR GLASS, SPUTTER COATED ON SECOND SURFACE (2)
 - 1.2. INDOOR LITE: CLEAR (TRANSPARENT) FLOAT GLASS.
 - 1.3. LOW-E COATING: "SOLARBAN" 60 SOLAR CONTROL (SPUTTERED) BY VITRO ARCHITECTURAL GLASS
2. PERFORMANCE VALUES:
 - 2.1. VISIBLE LIGHT TRANSMITTANCE: 70%
 - 2.2. U-VALUE WINTER: 0.29
 - 2.3. U-VALUE SUMMER: 0.27
 - 2.4. SHGC: 0.39
 - 2.5. SHADING COEFFICIENT: 0.45
 - 2.6. OUTDOOR VISIBLE LIGHT REFLECTANCE: 11%

LOCATION: SECOND SURFACE (2)

APPROVED MANUFACTURERS:
VITRO CERTIFIED FABRICATOR REQUIRED
CERTIFICATION: BOTH LITES TO BE CRADLE TO CRADLE CERTIFIED™
MINIMUM BRONZE LEVEL. BY CRADLE TO CRADLE PRODUCT INNOVATION INSTITUTE (WWW.C2CCERTIFIED.ORG).

OUTDOOR APPEARANCE: CLEAR
INSULATING UNIT CONSTRUCTION: 1/4" (6MM) GLASS + 1/2" (13MM) AIR SPACE + 1/4" (6MM) GLASS

SPECIFICATIONS 1/4" = 1'-0" 4

DRIVE THRU SPECIFICATIONS

DRIVE THRU WINDOW - QUIKSERV
MODEL # BP-7241E - STANDARD INSTALLATION
MODEL # BP-7241E-IP - HIGH WIND ZONE AS DETERMINED BY THE LOCAL BUILDING CODE

GENERAL NOTES:
ACTUAL DIMENSIONS: 72" (W) x 27" (H)
ROUGH OPENING 72 1/2" (W) x 41 1/2" (H)

- 1) EYE SET TO BE MOUNTED DIRECTLY BELOW THE SERVICE OPENING. BAR TO BE MOUNTED ON THE WALL.
- 2) ANCHOR SCREWS TO BE SUPPLIED BY THE CONTRACTOR.
- 3) JUNCTION BOX TO BE SUPPLIED BY CONTRACTOR.

- INSTALLATION:**
- 1) QUIKSERV WINDOWS MUST BE INSTALLED LEVEL AND SQUARE TO WORK PROPERLY.
 - 2) ANCHOR ACCORDING TO THE LOCAL BUILDING CODE ANCHOR SCHEDULE.
 - 3) 115/15 AMP DEDICATED CIRCUIT FEATURE.
 - 4) RECOMMENDED HEIGHT FROM FLOOR TO SERVICE OPENING TO BE 36". (CHECK FOR ANY LOCAL CODES OR CITY CODES)
 - 5) SILICONE ALL EXTERIOR AND INTERIOR JOINTS.
 - 6) ALL OTHER TYPES OF ANCHORING TO BE APPROVED BY CERTIFIED ENGINEER.

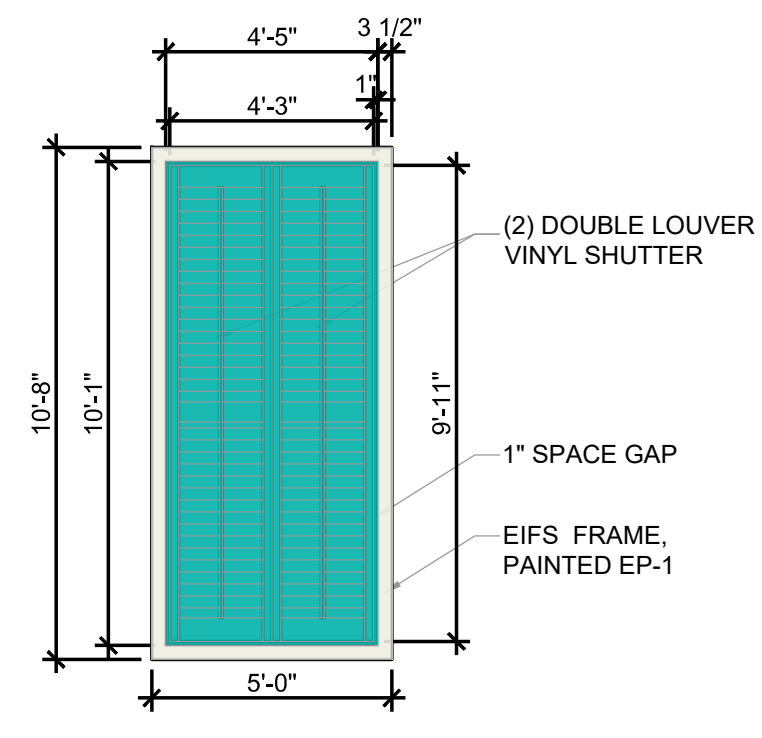
SHUTTER MANUFACTURER

SHUTTER CONTRACTOR
MODEL L-2 VINYL: PHONE: 1-800-734-8368
WWW.SHUTTERCONTRACTOR.COM

FINISH NOTES

THE FOLLOWING COMPONENTS CAN BE PURCHASED FROM THE APPROVED SIGN VENDORS:

• STANDING SEAM ROOF
• BALCONY RAILING
• CLEARANCE BAR
• MENU CANOPY
• GUARD RAIL
• AWNINGS
• SHUTTERS
• DUMPSTER GATES
• INTERIOR LADDER
• CANOPY



LARGE SHUTTER DIM 1/4" = 1'-0" 3

KEN MCCrackEN, ARCHITECT
Architecture • Program Management • Permitting

19120 SE 34TH STREET
SUITE 115
VANCOUVER, WA 98683
PROJECT CONTACT: ERICK PETERSON
PHONE: 360.980.8238
EMAIL: EPETERSON@PMDCINC.COM

KEN MCCrackEN, ARCHITECT
SEAL: REGISTERED ARCHITECT
KENNETH J. MCCrackEN
CAMAS, WA
License # 5599
STATE OF OREGON
08/30/21
KJM/xxxx

CONSULTANT:

LOUISIANA KITCHEN
SITE NO.: 00xxx
1771 SE Ensign Lane
Warrenton, Oregon 97146

LOUISIANA KITCHEN
19 72

REV	DATE	DESCRIPTION
△	07.29.21	PRE-APPLICATION

DRAWN BY: CDG
CHECKED BY: ...

ARCH. PROJECT NO.: **AMB21002.0**

SHEET NAME: **LEFT AND RIGHT EXTERIOR ELEVATIONS**

SHEET NUMBER: **A-5.1**

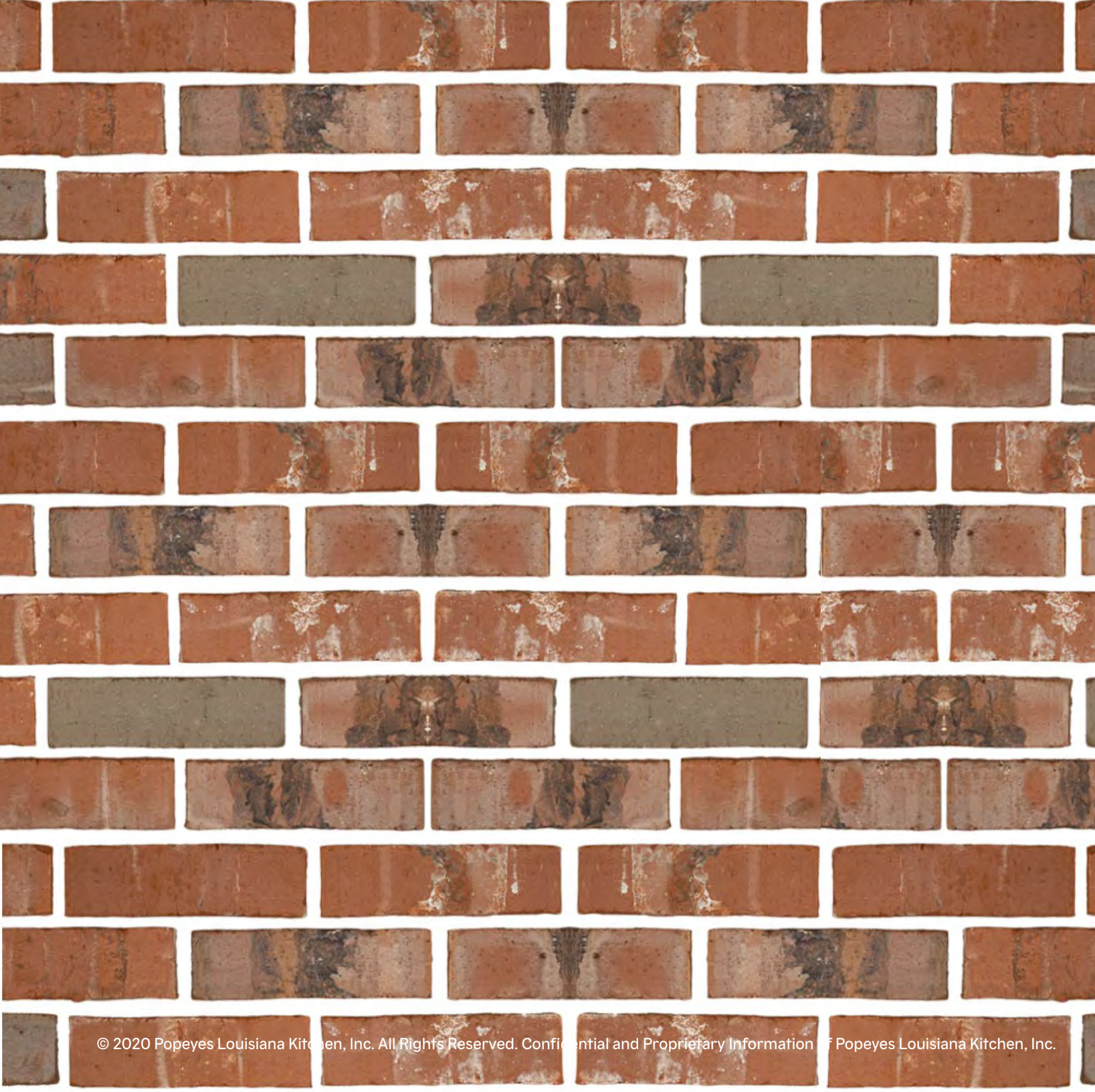


2021 POPEYE'S FINISH SCHEDULE

EXTERIOR MATERIALS & FINISH SCHEDULE **TO BE USED WITH FS-LEGACY (1846)

3/25/2021							
CODE	MATERIAL	LOCATION	SOURCE	PRODUCT	COLOR	DIMENSION	ADDITIONAL INFORMATION
WALL FINISHES AND PAINT							
EB-1	FACE BRICK	ACCENT TOWER BRICK	NICHIHA FIBER CEMENT	VINTAGE BRICK	ALEXANDRIA BUFF	7 3/8" x 2 1/2" x 3/4"	CONTACT: MATT STEPHENSON M: 770.789.8228 popeyes@nichiha.com
			ALTERNATE OPTION: CREATIVE MATERIALS CORPORATION	ALTERNATE OPTION: THIN BRICK	FLAGSTAFF	8-1/8" x 2-1/16" x 5/8"	CONTACT: popeyestile@creativematerialscorp.com 1.800.207.2967 Ext 7797
EF-1	EXTERIOR WOOD SIDING	FRONT FAÇADE EXTERIOR WALLS	NICHIHA FIBER CEMENT	VINTAGE WOOD AWP 3030	CEDAR	17-7/8" H x 119-5/16" L	CONTACT: MATT STEPHENSON M: 770.789.8228 popeyes@nichiha.com
EP-1	EXTERIOR PAINT	MAIN WALL SURFACE ABOVE WAINSCOT ACCENT TRIM	BENJAMIN MOORE	ULTRA SPEC 500 EGGSHELL	WHITE OC-125 MOONLIGHT WHITE		CONTACT: RODGER LIPPMAN C: 848.702.0239 rodger.lippman@benjaminmoore.com
			SHERWIN WILLIAMS		SW 7551 GREEK VILLA		CONTACT: GLENN REMLER C: 954.547.1217 glenn.j.remler@sherwin.com
EP-2	EXTERIOR PAINT	BRICK TOWER METAL COPING	BENJAMIN MOORE	ULTRA SPEC 500 EGGSHELL	ROOT BEER CANDY 2105-20		CONTACT: RODGER LIPPMAN C: 848.702.0239 rodger.lippman@benjaminmoore.com
			SHERWIN WILLIAMS		SW 6062 RUGGED BROWN		CONTACT: GLENN REMLER C: 954.547.1217 glenn.j.remler@sherwin.com
EP-3	EXTERIOR PAINT	EXTERIOR WAINSCOT, SLL PAINT, AND DUMSPETER WALLS	BENJAMIN MOORE	ULTRA SPEC 500 EGGSHELL	"GRAY" HC-170 STONINGTON GREY		CONTACT: RODGER LIPPMAN C: 848.702.0239 rodger.lippman@benjaminmoore.com
			SHERWIN WILLIAMS		SW 7657 TINSMITH		CONTACT: GLENN REMLER C: 954.547.1217 glenn.j.remler@sherwin.com
EP-4	EXTERIOR PAINT	BOLLARDS, PYLON POLE AND DIRECTIONAL SIGN POLES	BENJAMIN MOORE	SAFETY YELLOW			CONTACT: RODGER LIPPMAN C: 848.702.0239 rodger.lippman@benjaminmoore.com
EP-5	METAL/PAINT	DRIVE THRU WINDOW CANOPY	PROVIDED BY MANUFACTURER	PROVIDED BY MANUFACTURER	ORANGE. PANTONE #3564 C. PROVIDED BY MANUFACTURER		PROVIDED BY MANUFACTURER
EP-6	METAL/PAINT	BUILDING CANOPIES	PROVIDED BY MANUFACTURER	PROVIDED BY MANUFACTURER	TEAL. PANTONE #326 C. PROVIDED BY MANUFACTURER		PROVIDED BY MANUFACTURER
EP-7	EXTERIOR PAINT	DUMSPETER GATES	BENJAMIN MOORE	ULTRA SPEC 500 EGGSHELL	FACTORY FINISH BLACK		CONTACT: RODGER LIPPMAN C: 848.702.0239 rodger.lippman@benjaminmoore.com
			SHERWIN WILLIAMS		SW 6991 BLACK MAGIC		CONTACT: GLENN REMLER C: 954.547.1217 glenn.j.remler@sherwin.com
EP-8	EXTERIOR PAINT	ALL EXTERIOR WALLS	BENJAMIN MOORE	ALIPHATIC ACRYLIC URETHANE - GLOSS	ANTI-GRAFFITTI COAT V500-00		CONTACT: RODGER LIPPMAN C: 848.702.0239 rodger.lippman@benjaminmoore.com
DP-1	DOOR	EXTERIOR DOORS MAIN ENTRANCE AND SIDE ENTRANCE	PROVIDED BY MANUFACTURER	TO BE PURCHASED FROM MANUFACTURER IN ORANGE FINISH	ORANGE. PANTONE #3564 C. PROVIDED BY MANUFACTURER		
ES-1	SHUTTERS	EXTERIOR WALLS	PROVIDED BY MANUFACTURER	TO BE PURCHASED FROM MANUFACTURER IN TEAL FINISH	TEAL. PANTONE #326 C. PROVIDED BY MANUFACTURER		PROVIDED BY MANUFACTURER

7.0 Material Specifications



EB-1

Accent Tower Brick

*Refer to Finish Schedule for exact specification and alternatives



EF-1

Exterior Wood Siding

*Refer to Finish Schedule for exact specification and alternatives

EP-1

Exterior Upper Facade
Paint

*Refer to Finish Schedule for
exact specification and
alternatives

EP-2

Brick Tower Metal
Coping

*Refer to Finish Schedule for
exact specification and
alternatives

EP-3

Exterior Wainscot and
Sill Paint

*Refer to Finish Schedule for
exact specification and
alternatives

EP-4

Exterior **Safety Yellow**
Paint

*Refer to Finish Schedule for
exact specification and
alternatives

myperfectcolor.com

EP-5

Popeyes Orange

Pantone 3564 C

- Drive Thru Canopy
- Entry Doors

*Refer to Finish Schedule for exact specification and alternatives

EP-6

Popeyes Teal

Pantone 326 C

- Facade Canopies
- Exterior Shutters

*Refer to Finish Schedule for exact specification and alternatives

P-1

Interior Walls and Ceiling

*Refer to Finish Schedule for
exact specification and
alternatives

P-2

Door Frames

*Refer to Finish Schedule for exact specification and alternatives

P-3

Orange Brick Accent
Wall

*Refer to Finish Schedule for
exact specification and
alternatives

P-4

- Neon Sign Background
- Restroom Walls

*Refer to Finish Schedule for exact specification and alternatives



BR-1

Interior Walls

*Refer to Finish Schedule for exact specification and alternatives

F-1

Interior Walls

*Refer to Finish Schedule for exact specification and alternatives

TL-1, TL-2, TL-3

- Dining Room Floor
- Base Tile
- Restroom Floor
- Restroom Walls

*Refer to Finish Schedule for exact specification and alternatives



TL-4

Front Service Counter
Walls

*Refer to Finish Schedule for
exact specification and
alternatives



SS-1

Front Service
Countertop

*Refer to Finish Schedule for
exact specification and
alternatives



L-1

Interior Door Laminate

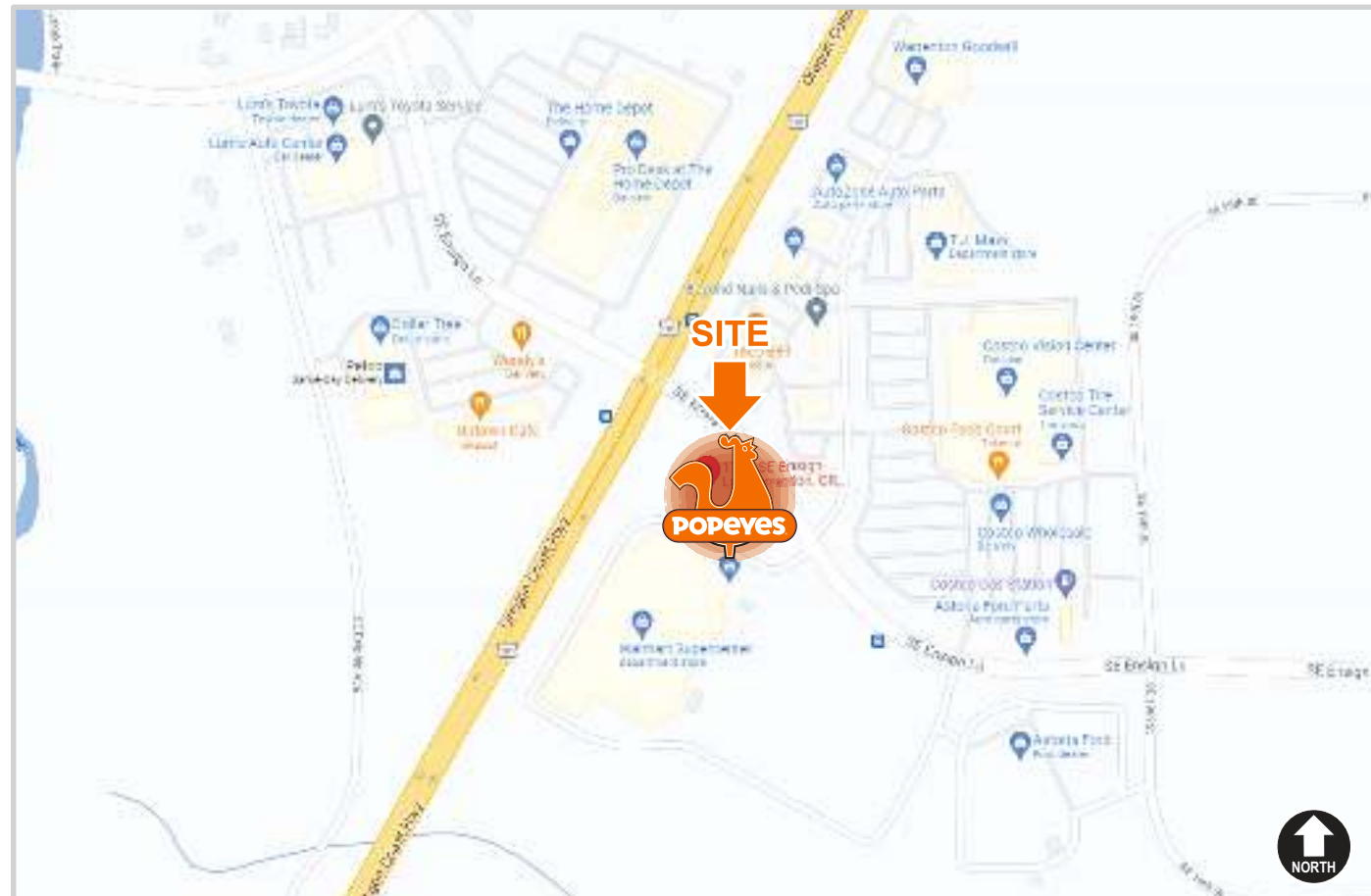
*Refer to Finish Schedule for exact specification and alternatives

SCHEDULE

- N1 - (1) 27" ILLUMINATED CHANNEL LETTERS WITH LK TAGLINE ILLUMINATED CHANNEL LETTERS ON WIREWAY
- N2 - (1) 60" ILLUMINATED BUILDING SEAL
- N3 - (1) 7'-0"X4'-0"X8" DT CANOPY (PAINTED PMS 3564C ORANGE)
- N4 - (1) 10'-0"X4'-0"X8" DT CANOPY (PAINTED PMS 3564C ORANGE)
- N5 - (1) 5'-0"X3'-0"X8" CANOPY (PAINTED PMS 326C TEAL)
- N6 - (1) 17'-0"X3'-0"X8" CANOPY (PAINTED PMS 326C TEAL)
- N7 - (1) 7'-0"X3'-0"X8" CANOPY (PAINTED PMS 326C TEAL)
- N8 - (1) 7'-0"X3'-0"X8" CANOPY (PAINTED PMS 3564C ORANGE)
- N9 - (2) NON-ILLUMINATED 8" TALL BUILDING FASCIA BAND
- N10 - (1) 3'-0" GUARD RAIL WITH PLATES
- N11 - (1) CLEARANCE BAR
- N12 - (1) DRIVE THRU MENU CANOPY
- N13 - (1) SPEAKER BOX
- N14 - (1) MONOLITHIC DIRECTIONAL SIGN (DRIVE-THRU LEFT ARROW /DRIVE-THRU RIGHT ARROW)
- N15 - (1) P-80 PYLON SIGN

LEGEND

- N1 NEW PROPOSED
- R1 EXISTING SIGN TO BE REPLACED
- X1 EXISTING SIGN TO BE REMOVED
- S1 EXISTING SIGN TO REMAIN



VICINITY MAP

NOT TO SCALE



POPEYES
 Ambrosia QSR Chicken Real Estate, LLC
 1771 SE Ensign Lane
 Warrenton, OR 97146

APPROVAL

X	Title	Date
---	-------	------

SIGN CODE

Building Signs
 Code Check: C-1 zone sign. 3 Signs Max on property. 1 sq. ft per linear foot of frontage. We have 240' of frontage on SE Ensign, plus 32sq.ft. for the little frontage on the entry lane (it counts as a "street"). = 272 sq.ft. total allowed (bldg & pylon).

Freestanding Signs
 Code Check: freestanding sign it could be up to 45' tall or extend no higher than 10' above the restaurant. Bldg is 22' tall + 10' = 32' max heith.

SQUARE FOOTAGE INFORMATION

ALLOWABLE 0.00 SQ. FT.

PROPOSED 0.00 SQ. FT.



AERIAL MAP

NOT TO SCALE



LOREN INDUSTRIES

12226 Coast Drive
 Whittier, CA 90601
 Tel: (562) 946-7545
 Fax: (562) 949-5707
 St. Lic.: 455415

Los Angeles, CA



DRAWING/REVISION NO.:

09-21-0008

PAGE NO.:

1 OF 20

CLIENT:

POPEYES

ADDRESS

1771 SE Ensign Lane
 Warrenton, OR 97146

Approval Signature

DATE:

09/16/2021

PROJECT MANAGER

Dave P.

DESIGNER:

EC

REVISION DATE:

REVISION BY:

ALL IDEAS, PLANS, AND ELECTRONIC ART INDICATED ON THIS DRAWING ARE COPYRIGHTED AND OWNED BY LOREN ELECTRIC SIGN CORPORATION AND SHALL NOT BE REPRODUCED, USED BY OR DISCLOSED TO ANY PERSONS, FIRM OR CORPORATION FOR ANY PURPOSE WHATSOEVER WITHOUT PERMISSION OF LOREN ELECTRIC SIGN CORPORATION.



LOREN

LOREN INDUSTRIES

12226 Coast Drive
Whittier, CA 90601

Tel: (562) 946-7545

Fax: (562) 949-5707

St. Lic.: 455415

Los Angeles, CA



DRAWING/REVISION NO.:

09-21-0008

PAGE NO.:

2 OF 20

CLIENT:

POPEYES

ADDRESS

1771 SE Ensign Lane
Warrenton, OR 97146

Approval Signature

DATE:

09/16/2021

PROJECT MANAGER

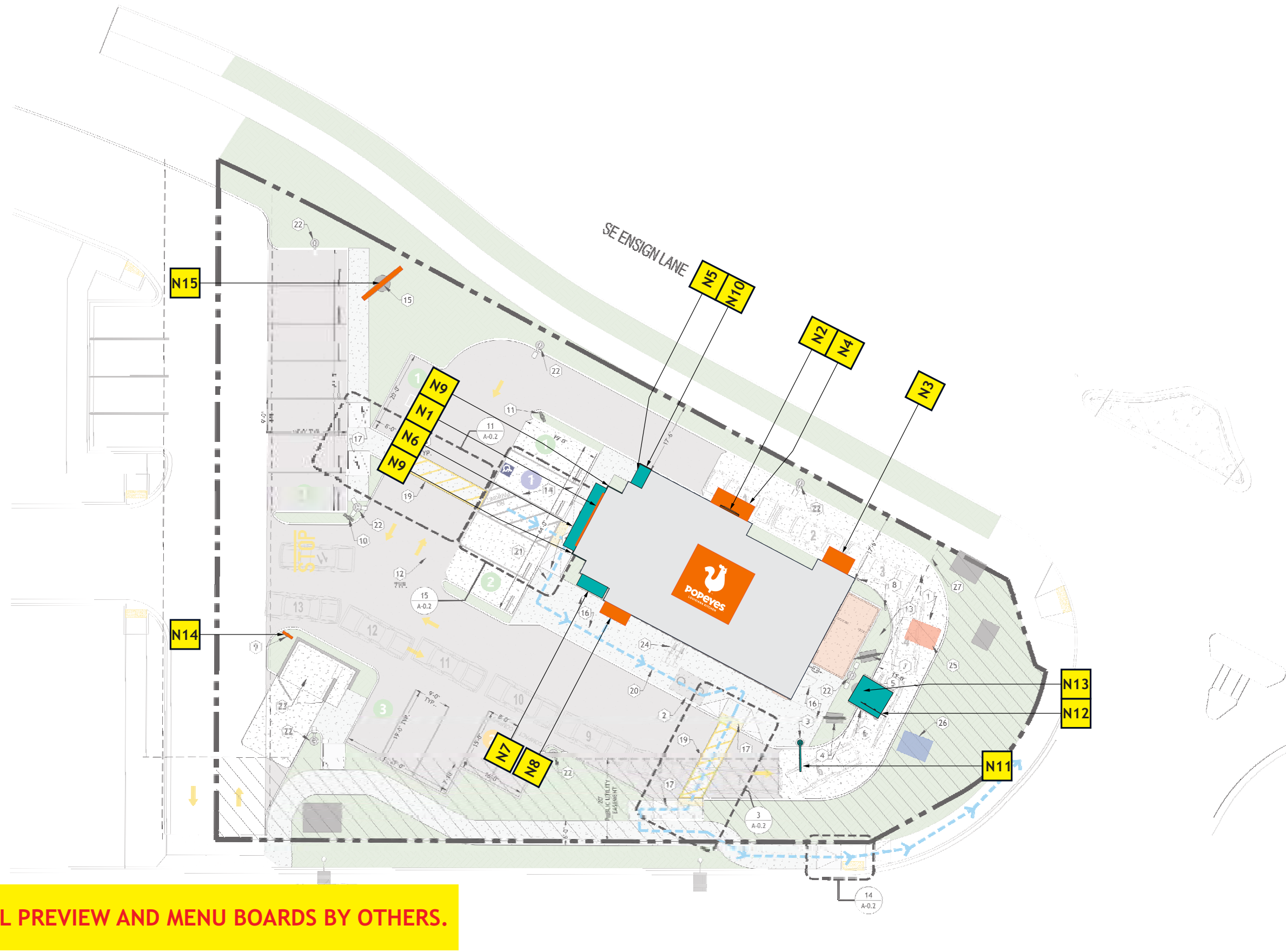
Dave P.

DESIGNER:

EC

REVISION DATE:

REVISION BY:



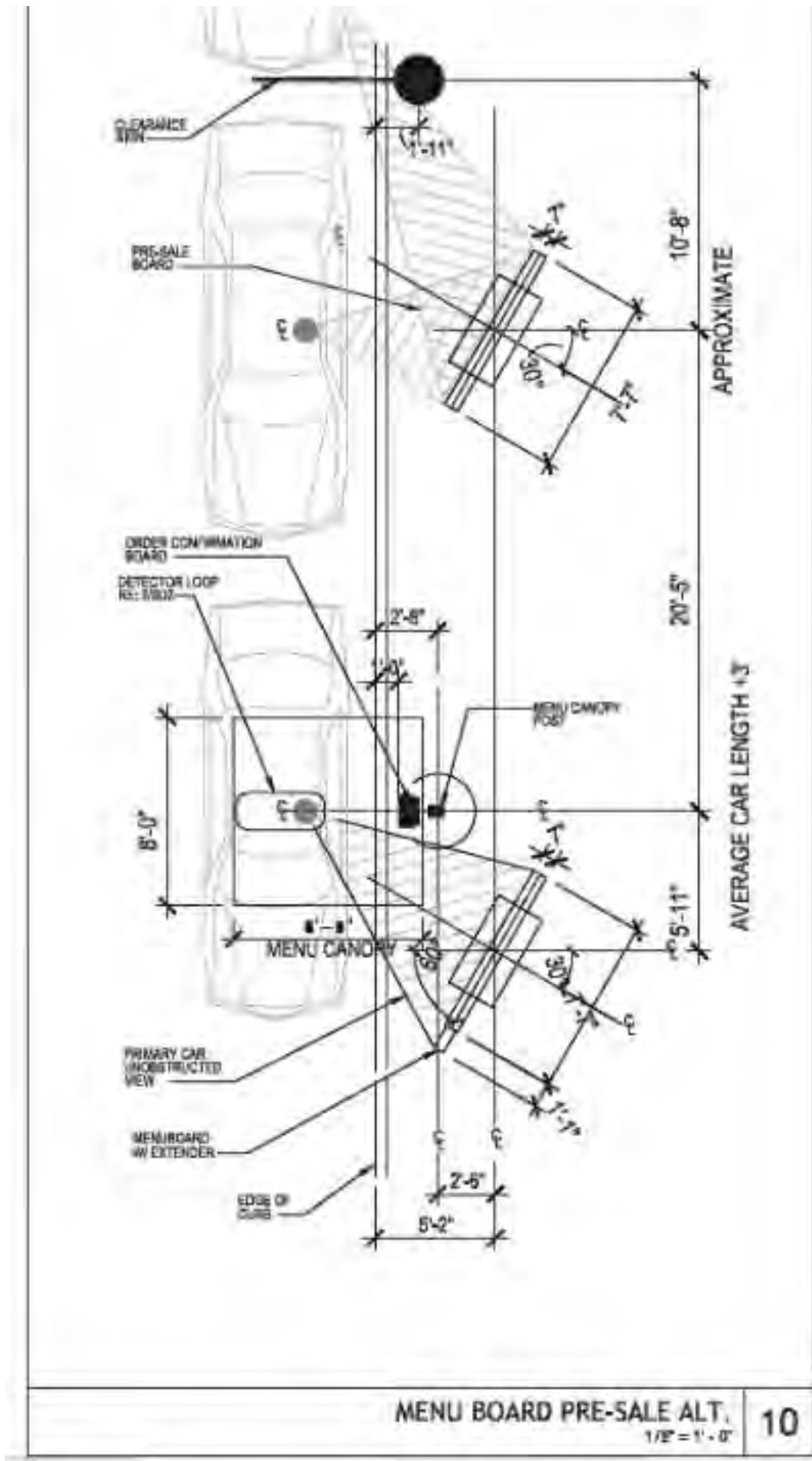
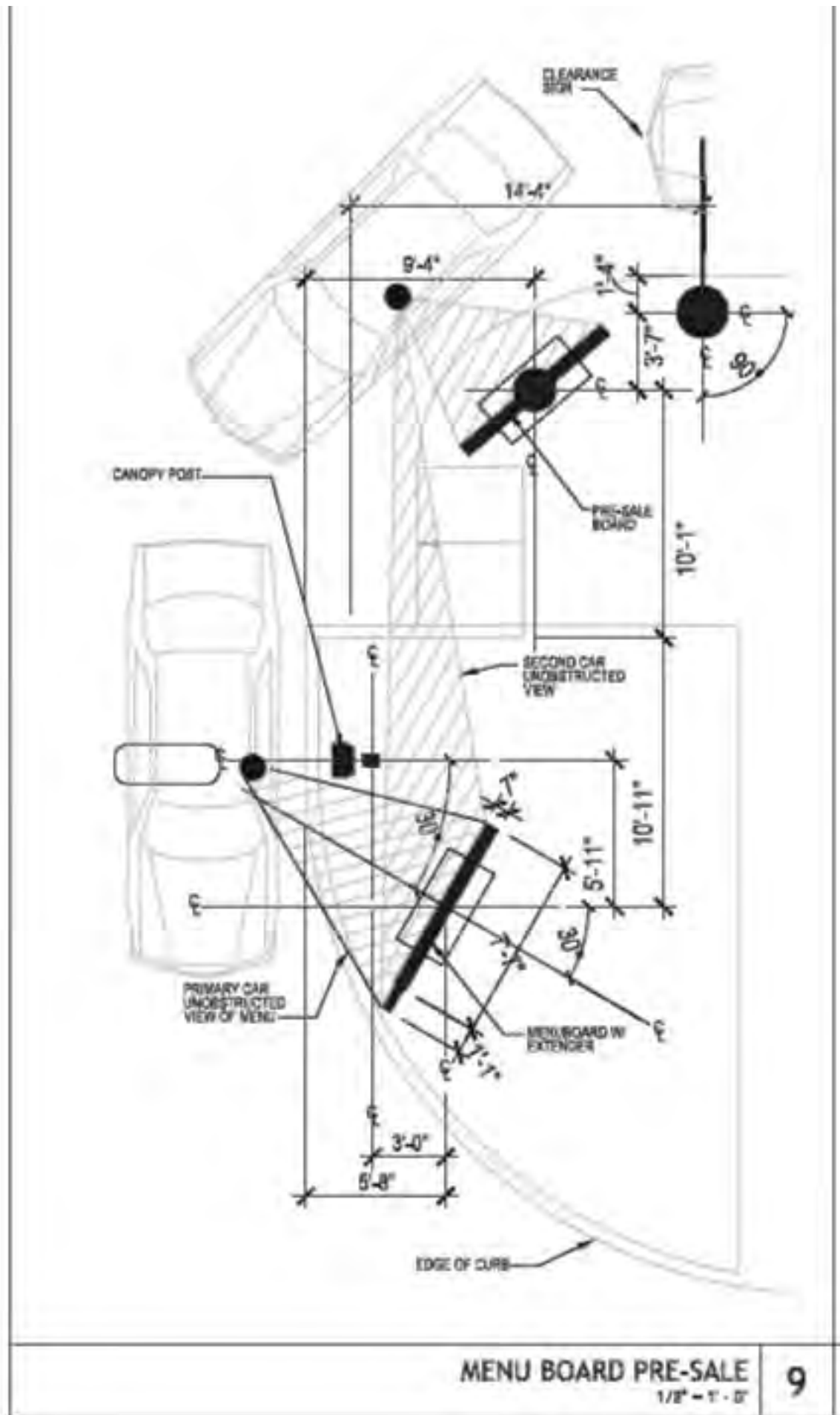
NOTE: DIGITAL PREVIEW AND MENU BOARDS BY OTHERS.

SITE PLAN



Scale: 1"=25'-0"

ALL IDEAS, PLANS, AND ELECTRONIC ART INDICATED ON THIS DRAWING ARE COPYRIGHTED AND OWNED BY LOREN ELECTRIC SIGN CORPORATION AND SHALL NOT BE REPRODUCED, USED BY OR DISCLOSED TO ANY PERSONS, FIRM OR CORPORATION FOR ANY PURPOSE WHATSOEVER WITHOUT PERMISSION OF LOREN ELECTRIC SIGN CORPORATION.



Speaker canopy menu board standard template location.



LOREN
LOREN INDUSTRIES

12226 Coast Drive
Whittier, CA 90601
Tel: (562) 946-7545
Fax: (562) 949-5707
St. Lic.: 455415

Los Angeles, CA



DRAWING/REVISION NO.:
09-21-0008

PAGE NO.:
3 OF 20

CLIENT:
POPEYES

ADDRESS
1771 SE Ensign Lane
Warrenton, OR 97146

Approval Signature

DATE:
09/16/2021

PROJECT MANAGER
Dave P.

DESIGNER:
EC

REVISION DATE:

REVISION BY:

ALL IDEAS, PLANS, AND ELECTRONIC ART INDICATED ON THIS DRAWING ARE COPYRIGHTED AND OWNED BY LOREN ELECTRIC SIGN CORPORATION AND SHALL NOT BE REPRODUCED, USED BY OR DISCLOSED TO ANY PERSONS, FIRM OR CORPORATION FOR ANY PURPOSE WHATSOEVER WITHOUT PERMISSION OF LOREN ELECTRIC SIGN CORPORATION.



LOREN

LOREN INDUSTRIES

12226 Coast Drive
Whittier, CA 90601

Tel: (562) 946-7545
Fax: (562) 949-5707

St. Lic.: 455415

Los Angeles, CA



POPEYES
LOUISIANA KITCHEN

DRAWING/REVISION NO.:

09-21-0008

PAGE NO.:

4 OF **20**

CLIENT:

POPEYES

ADDRESS

1771 SE Ensign Lane
Warrenton, OR 97146

Approval Signature

DATE:

09/16/2021

PROJECT MANAGER

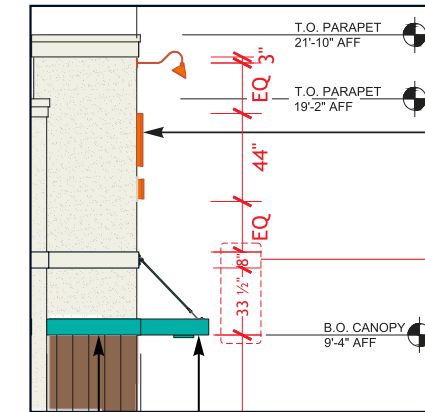
Dave P.

DESIGNER:

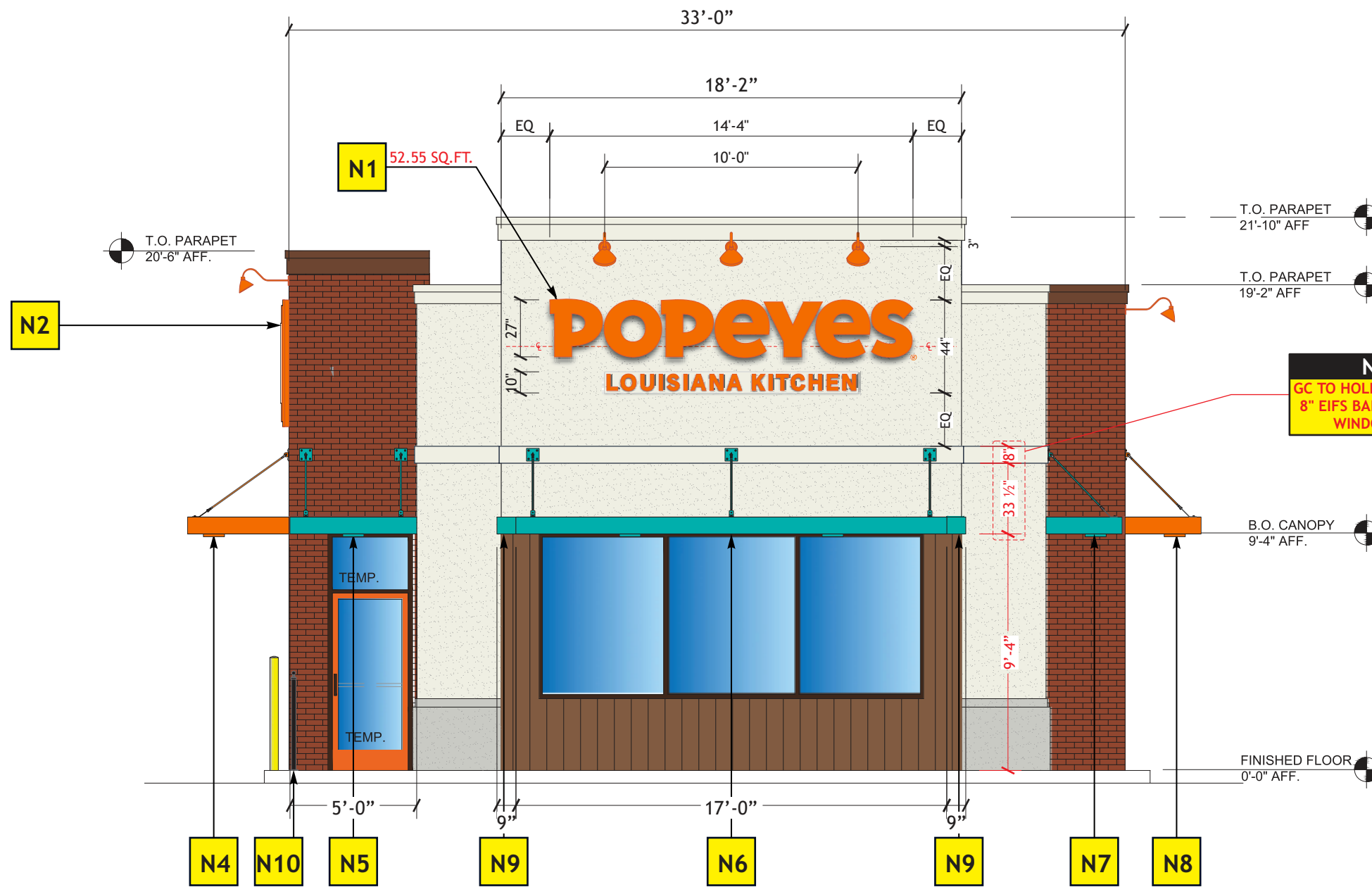
EC

REVISION DATE:

REVISION BY:



**GOOSENECK LIGHT PLACEMENT
SIDE ELEVATION VIEW**



NOTE:
GC TO HOLD DIMENSIONS OF
8" EIFS BAND 33 1/2" ABOVE
WINDOW FRAME.

NORTH ELEVATION (STOREFRONT)

PROPOSED: 52.55 SQ.FT.

Scale: 3/16" = 1'-0"

ALL IDEAS, PLANS, AND ELECTRONIC ART INDICATED ON THIS DRAWING ARE COPYRIGHTED AND OWNED BY LOREN ELECTRIC SIGN CORPORATION AND SHALL NOT BE REPRODUCED, USED BY OR DISCLOSED TO ANY PERSONS, FIRM OR CORPORATION FOR ANY PURPOSE WHATSOEVER WITHOUT PERMISSION OF LOREN ELECTRIC SIGN CORPORATION.



LOREN

LOREN INDUSTRIES

12226 Coast Drive
Whittier, CA 90601

Tel: (562) 946-7545

Fax: (562) 949-5707

St. Lic.: 455415

Los Angeles, CA



DRAWING/REVISION NO.:

09-21-0008

PAGE NO.:

5 OF 20

CLIENT:

POPEYES

ADDRESS

1771 SE Ensign Lane
Warrenton, OR 97146

Approval Signature

DATE:

09/16/2021

PROJECT MANAGER

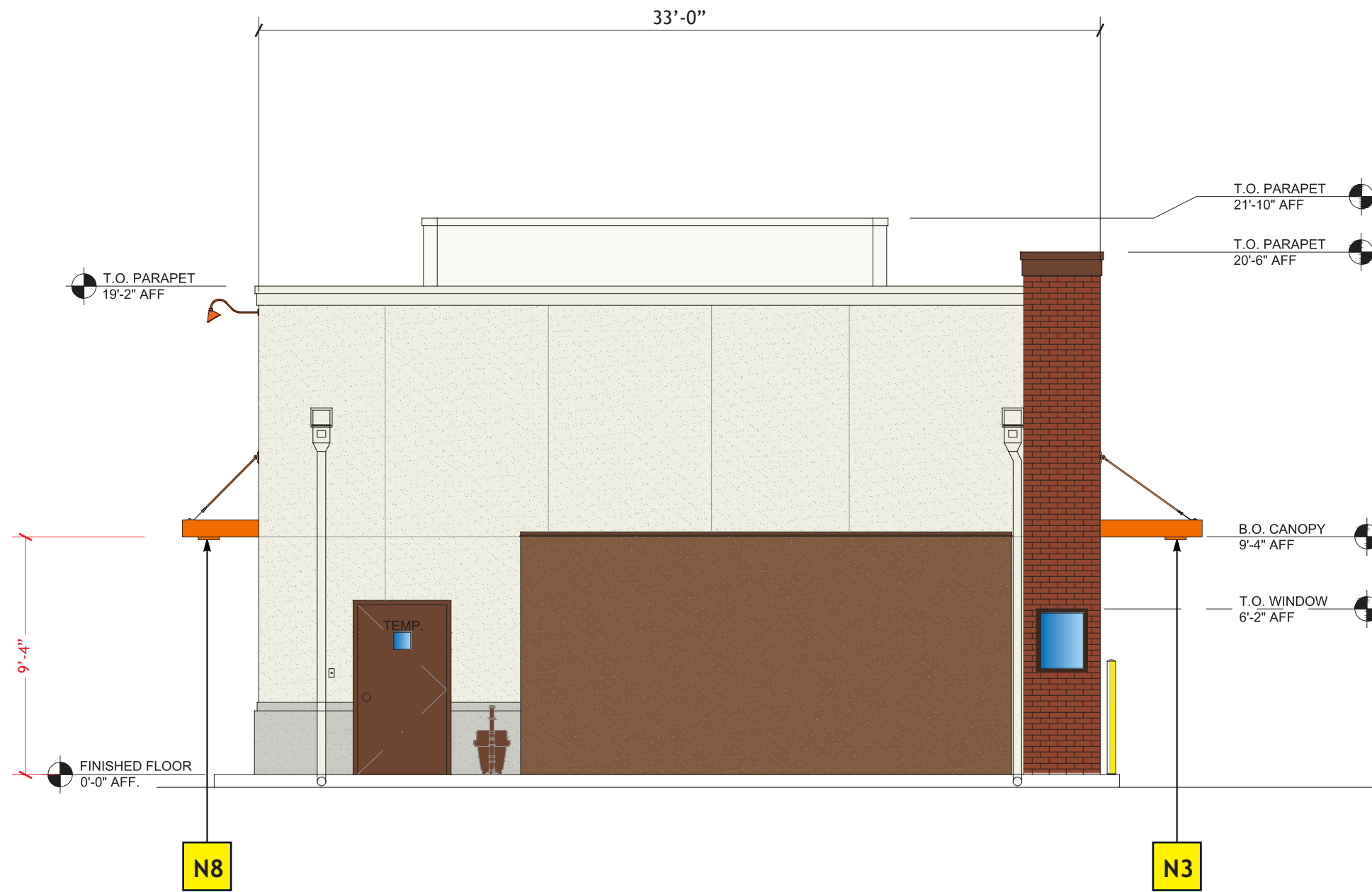
Dave P.

DESIGNER:

EC

REVISION DATE:

REVISION BY:



SOUTH ELEVATION (REAR)

Scale: 3/16" = 1'-0"

ALL IDEAS, PLANS, AND ELECTRONIC ART INDICATED ON THIS DRAWING ARE COPYRIGHTED AND OWNED BY LOREN ELECTRIC SIGN CORPORATION AND SHALL NOT BE REPRODUCED, USED BY OR DISCLOSED TO ANY PERSONS, FIRM OR CORPORATION FOR ANY PURPOSE WHATSOEVER WITHOUT PERMISSION OF LOREN ELECTRIC SIGN CORPORATION.



LOREN

LOREN INDUSTRIES

12226 Coast Drive
Whittier, CA 90601

Tel: (562) 946-7545
Fax: (562) 949-5707

St. Lic.: 455415

Los Angeles, CA



DRAWING/REVISION NO.:

09-21-0008

PAGE NO.:

6 OF 20

CLIENT:

POPEYES

ADDRESS

1771 SE Ensign Lane
Warrenton, OR 97146

Approval Signature

DATE:

09/16/2021

PROJECT MANAGER

Dave P.

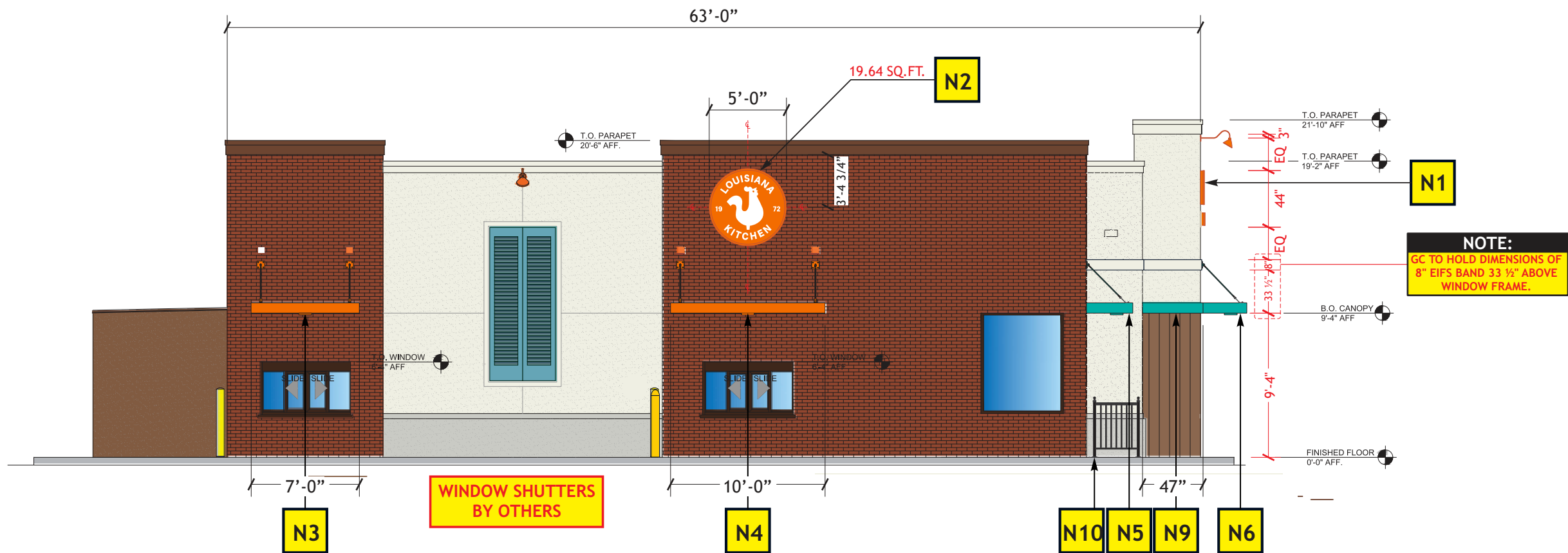
DESIGNER:

EC

REVISION DATE:

REVISION BY:

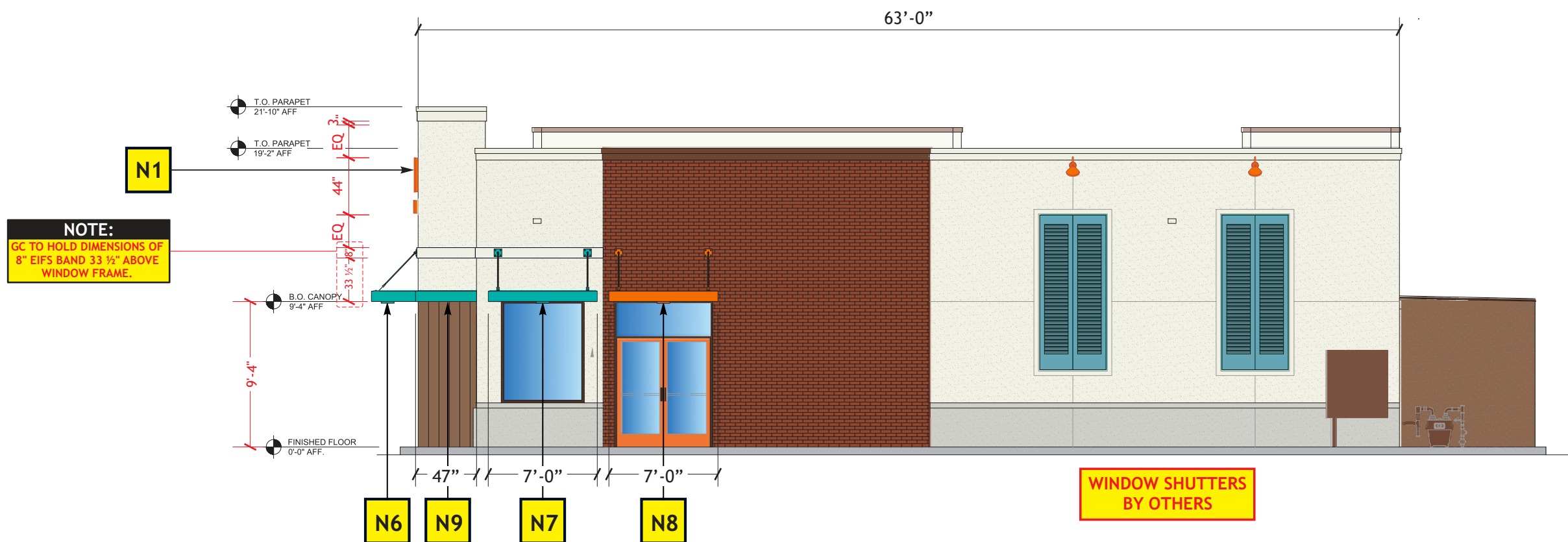
ALL IDEAS, PLANS, AND ELECTRONIC ART INDICATED ON THIS DRAWING ARE COPYRIGHTED AND OWNED BY LOREN ELECTRIC SIGN CORPORATION AND SHALL NOT BE REPRODUCED, USED BY OR DISCLOSED TO ANY PERSONS, FIRM OR CORPORATION FOR ANY PURPOSE WHATSOEVER WITHOUT PERMISSION OF LOREN ELECTRIC SIGN CORPORATION.



EAST ELEVATION (DRIVE-THRU)

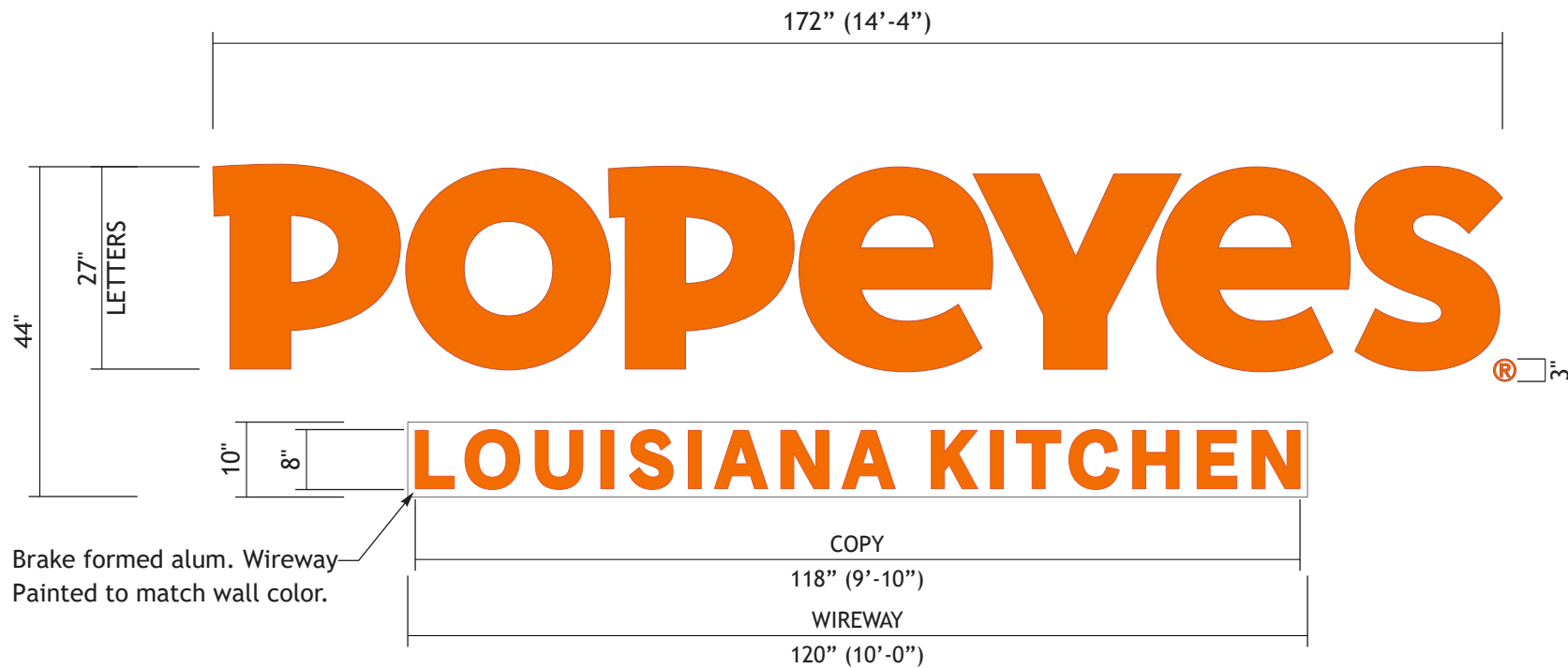
PROPOSED: 19.64 SQ.FT.

Scale: 1/8" = 1'-0"



WEST ELEVATION (MAIN ENTRANCE)

Scale: 1/8" = 1'-0"



Brake formed alum. Wireway
Painted to match wall color.

NOTE:
FACE COLOR, TRIM CAP AND
RETURN TO BE PMS 3564 C,
WIREWAY COLOR TO MATCH WALL

N1 ILLUMINATED CHANNEL LETTERS
FRONT VIEW

AREA: 52.55 SQ FT
SCALE: 1/2" = 1'-0"

SIDE VIEW
SCALE: 1/2" = 1'-0"

SPECIFICATIONS:

LETTERS: POPEYES

FACES:..... .177" #7328 WHITE ACRYLIC FACES
PAINTED PMS 3564 C

RETURNS:..... .040" X 3.0 ALUMINUM COIL PAINTED PMS 3564 C

BACKS:..... 1/8" WHITE ACM BACKS

TRIM CAP:..... 1" JEWELITE TRIMCAP PAINTED PMS 3564 C

ILLUMINATION:.. WHITE LED'S

SPECIFICATIONS:

LETTERS: LOUISIANA KITCHEN

FACES:..... .177" #7328 WHITE ACRYLIC FACES
PAINTED PMS 3564 C

RETURNS:..... .040" X 3.0 ALUMINUM COIL PAINTED PMS 3564 C

BACKS:..... 1/8" WHITE ACM BACKS

TRIM CAP:..... 1" JEWELITE TRIMCAP PAINTED PMS 3564 C

ILLUMINATION:.. WHITE LED'S

WIREWAY:..... .090" BRAKE FORMED ALUM.



LOREN INDUSTRIES

12226 Coast Drive
Whittier, CA 90601
Tel: (562) 946-7545
Fax: (562) 949-5707
St. Lic.: 455415

Los Angeles, CA



DRAWING/REVISION NO.:
09-21-0008

PAGE NO.:
7 OF 20

CLIENT:
POPEYES

ADDRESS
1771 SE Ensign Lane
Warrenton, OR 97146

Approval Signature

DATE:
09/16/2021

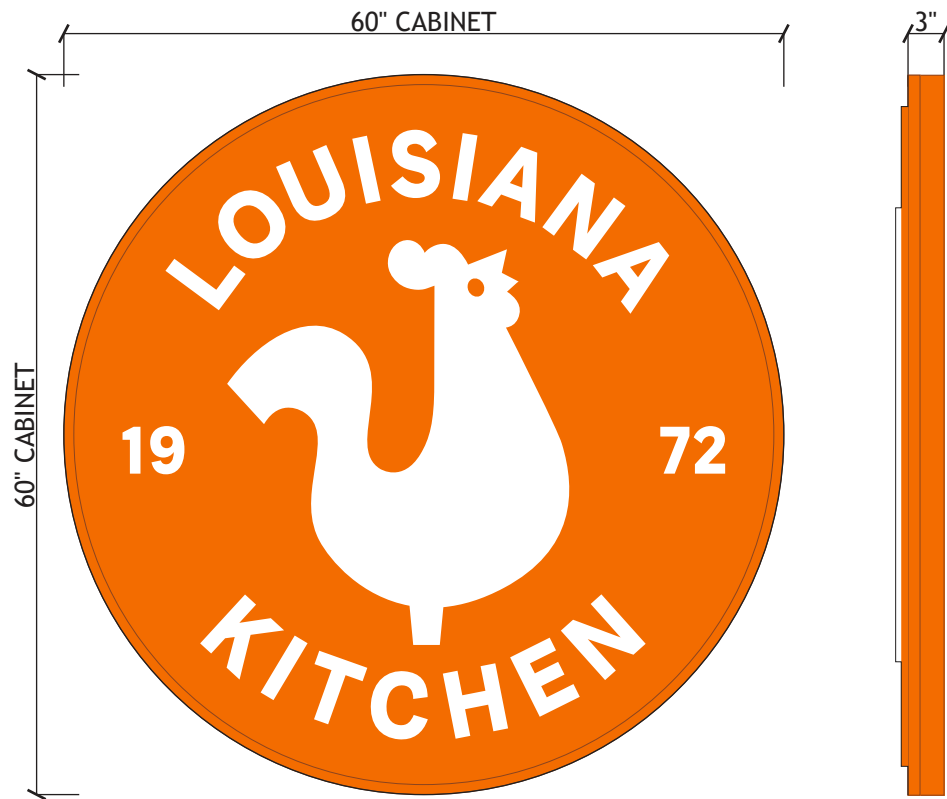
PROJECT MANAGER
Dave P.

DESIGNER:
EC

REVISION DATE:

REVISION BY:

ALL IDEAS, PLANS, AND ELECTRONIC ART INDICATED ON THIS DRAWING ARE COPYRIGHTED AND OWNED BY LOREN ELECTRIC SIGN CORPORATION AND SHALL NOT BE REPRODUCED, USED BY OR DISCLOSED TO ANY PERSONS, FIRM OR CORPORATION FOR ANY PURPOSE WHATSOEVER WITHOUT PERMISSION OF LOREN ELECTRIC SIGN CORPORATION.



N2 ILLUMINATED ROUND SEAL AREA: 19.64 SQ FT
 FRONT VIEW & SIDE VIEW SCALE: 3/4" = 1'-0"

SPECIFICATIONS:

1. RETURNS - 3" X .040" ALUMINUM PAINTED PMS 3564 C
2. FACES - .118" CLEAR IMPACTED MODIFIED ACRYLIC W/ 2ND SURFACE VINYL 3M 3630-20 WHITE, PMS 3564 C
3. TRIM CAP - 1" PMS 3564 C
4. BACKS - 5/8" ACM
5. LEDS- WHITE LED W/ 12V 60W POWER SUPPLY

NOTE:

FACE COLOR, TRIM CAP AND RETURN TO BE PMS 3564 C

NEW DIE TOOL REQUIRED



LOREN

LOREN INDUSTRIES

12226 Coast Drive
Whittier, CA 90601

Tel: (562) 946-7545

Fax: (562) 949-5707

St. Lic.: 455415

Los Angeles, CA



DRAWING/REVISION NO.:

09-21-0008

PAGE NO.:

8 OF 20

CLIENT:

POPEYES

ADDRESS

1771 SE Ensign Lane
Warrenton, OR 97146

Approval Signature

DATE:

09/16/2021

PROJECT MANAGER

Dave P.

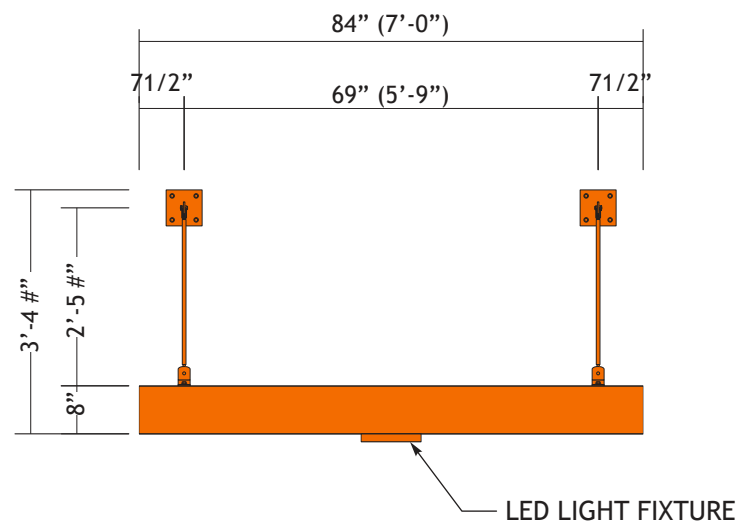
DESIGNER:

EC

REVISION DATE:

REVISION BY:

ALL IDEAS, PLANS, AND ELECTRONIC ART INDICATED ON THIS DRAWING ARE COPYRIGHTED AND OWNED BY LOREN ELECTRIC SIGN CORPORATION AND SHALL NOT BE REPRODUCED, USED BY OR DISCLOSED TO ANY PERSONS, FIRM OR CORPORATION FOR ANY PURPOSE WHATSOEVER WITHOUT PERMISSION OF LOREN ELECTRIC SIGN CORPORATION.



N3

DT WINDOW CANOPY
FRONT VIEW

QTY: 1

SCALE: 3/8" = 1'-0"

SPECIFICATIONS:

- 2" X 8" ALUMINUM RECT. TUBE CANOPY FRAME WITH .090" BRAKE FORMED STIFFENERS
- .090" ALUMINUM TOP & BOTTOM COVERS
- PAINT: - TOP, EDGE, BOTTOM AND SAG RODS PAINTED TO MATCH - PMS 3564 C
- LIGHTING - LED LIGHT FIXTURE

NOTES:

ELECTRICAL COMPONENTS TO BE UL LISTED AND INSTALLED IN ACCORDANCE WITH THE NATIONAL ELECTRIC CODE (NEC)

ALL WIRING SHALL BE 12 GA

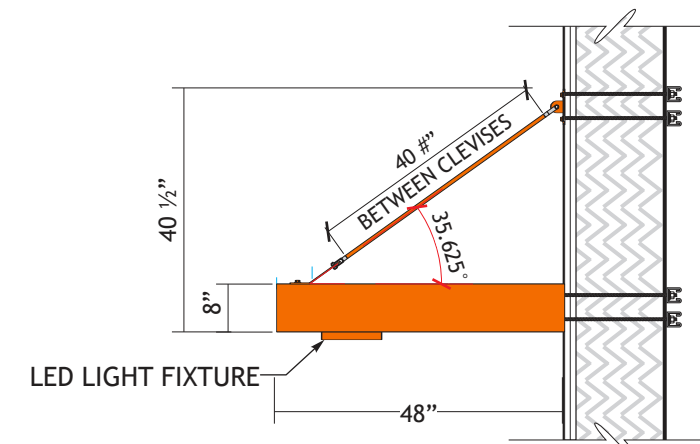
(1) 20 AMP-120V CIRCUIT TO BE BROUGHT TO JOB SITE BY LICENCED ELECTRICIAN.

ALL SIGNAGE WILL BE UL2161 COMPLIANT AND CARRY UL LABELS.

COLOR

 **PMS 3564 C TANGERINE**

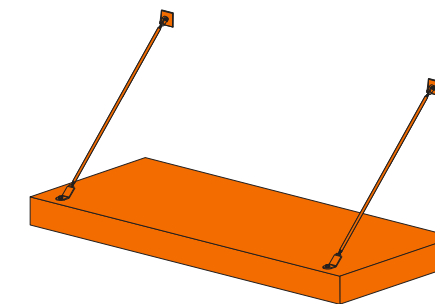
(1) LEFT SIDE-BRICK



2

CANOPY
END VIEW

SCALE: 3/8" = 1'-0"



3

CANOPY
ISOMETRIC VIEW

SCALE: NTS

UL LISTED
ELECTRIC SIGN SECTION

THIS SIGN IS INTENDED TO BE INSTALLED IN ACCORDANCE WITH THE REQUIREMENTS OF ARTICLE 600 OF THE NATIONAL ELECTRICAL CODE AND/OR OTHER APPLICABLE LOCAL CODES.

THIS INCLUDES PROPER GROUNDING AND BONDING OF THE SIGN.

NOTE: ADD SILICONE SEALANT TO ALL SEAMS ON TOP OF THE CANOPIES.



LOREN

LOREN INDUSTRIES

12226 Coast Drive
Whittier, CA 90601

Tel: (562) 946-7545
Fax: (562) 949-5707

St. Lic.: 455415

Los Angeles, CA



DRAWING/REVISION NO.:

09-21-0008

PAGE NO.:

9 OF 20

CLIENT:

POPEYES

ADDRESS

1771 SE Ensign Lane
Warrenton, OR 97146

Approval Signature

DATE:

09/16/2021

PROJECT MANAGER

Dave P.

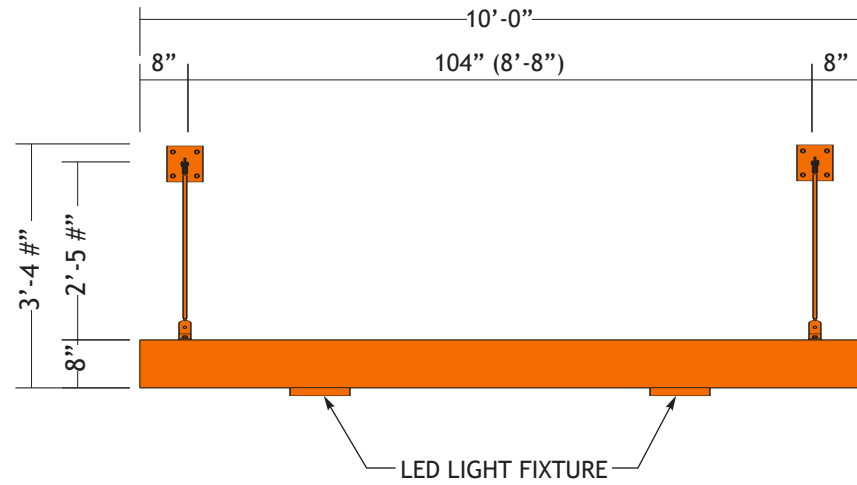
DESIGNER:

EC

REVISION DATE:

REVISION BY:

ALL IDEAS, PLANS, AND ELECTRONIC ART INDICATED ON THIS DRAWING ARE COPYRIGHTED AND OWNED BY LOREN ELECTRIC SIGN CORPORATION AND SHALL NOT BE REPRODUCED, USED BY OR DISCLOSED TO ANY PERSONS, FIRM OR CORPORATION FOR ANY PURPOSE WHATSOEVER WITHOUT PERMISSION OF LOREN ELECTRIC SIGN CORPORATION.



N4

DT WINDOW CANOPY
FRONT VIEW

QTY: 1

SCALE: 3/8" = 1'-0"

SPECIFICATIONS:

2. 2" X 8" ALUMINUM RECT. TUBE CANOPY FRAME WITH .090" BRAKE FORMED STIFFENERS
3. .090" ALUMINUM TOP & BOTTOM COVERS
4. PAINT: - TOP, EDGE, BOTTOM AND SAG RODS PAINTED TO MATCH - PMS 3564 C
5. LIGHTING - LED LIGHT FIXTURE

NOTES:

ELECTRICAL COMPONENTS TO BE UL LISTED AND INSTALLED IN ACCORDANCE WITH THE NATIONAL ELECTRIC CODE (NEC)

ALL WIRING SHALL BE 12 GA

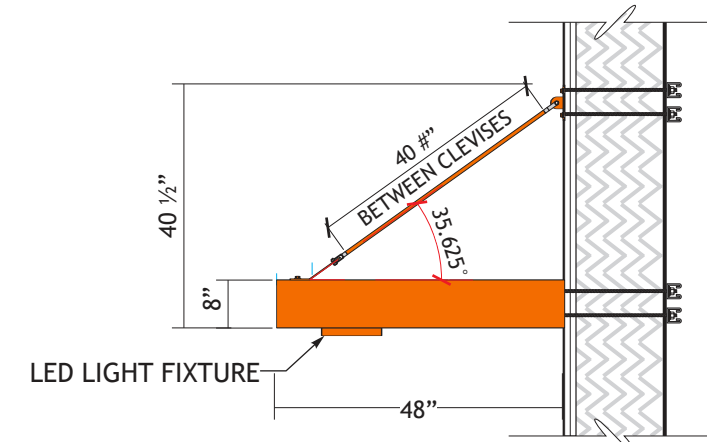
(1) 20 AMP-120V CIRCUIT TO BE BROUGHT TO JOB SITE BY LICENCED ELECTRICIAN.

ALL SIGNAGE WILL BE UL2161 COMPLIANT AND CARRY UL LABELS.

COLOR

 **PMS 3564 C TANGERINE**

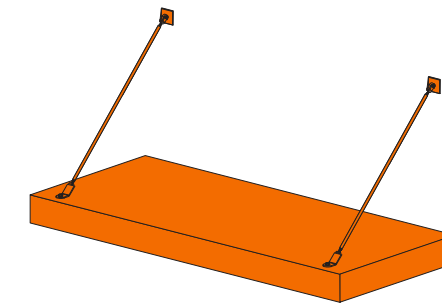
(1) LEFT SIDE-BRICK



2

CANOPY
END VIEW


SCALE: 3/8" = 1'-0"



3

CANOPY
ISOMETRIC VIEW

SCALE: NTS

 ELECTRIC SIGN SECTION	<p>THIS SIGN IS INTENDED TO BE INSTALLED IN ACCORDANCE WITH THE REQUIREMENTS OF ARTICLE 600 OF THE NATIONAL ELECTRICAL CODE AND/OR OTHER APPLICABLE LOCAL CODES.</p>
	<p>THIS INCLUDES PROPER GROUNDING AND BONDING OF THE SIGN.</p>

NOTE: ADD SILICONE SEALANT TO ALL SEAMS ON TOP OF THE CANOPIES.



LOREN
LOREN INDUSTRIES

12226 Coast Drive
Whittier, CA 90601
Tel: (562) 946-7545
Fax: (562) 949-5707
St. Lic.: 455415
Los Angeles, CA



DRAWING/REVISION NO.:
09-21-0008

PAGE NO.:
10 OF 20

CLIENT:
POPEYES

ADDRESS
1771 SE Ensign Lane
Warrenton, OR 97146

Approval Signature

DATE:
09/16/2021

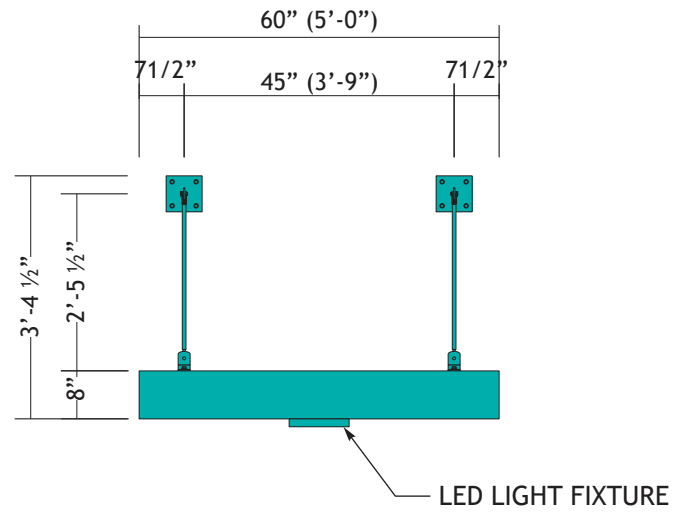
PROJECT MANAGER
Dave P.

DESIGNER:
EC

REVISION DATE:

REVISION BY:

ALL IDEAS, PLANS, AND ELECTRONIC ART INDICATED ON THIS DRAWING ARE COPYRIGHTED AND OWNED BY LOREN ELECTRIC SIGN CORPORATION AND SHALL NOT BE REPRODUCED, USED BY OR DISCLOSED TO ANY PERSONS, FIRM OR CORPORATION FOR ANY PURPOSE WHATSOEVER WITHOUT PERMISSION OF LOREN ELECTRIC SIGN CORPORATION.



N5 CANOPY FRONT VIEW **QTY: 1**
SCALE: 3/8" = 1'-0"

SPECIFICATIONS:

2. 2" X 8" ALUMINUM RECT. TUBE CANOPY FRAME WITH .090" BRAKE FORMED STIFFENERS
3. .090" ALUMINUM TOP & BOTTOM COVERS
4. PAINT: - TOP, EDGE, BOTTOM AND SAG RODS PAINTED TO MATCH - PMS 326 C TEAL
5. LIGHTING - LED LIGHT FIXTURE

NOTES:

ELECTRICAL COMPONENTS TO BE UL LISTED AND INSTALLED IN ACCORDANCE WITH THE NATIONAL ELECTRIC CODE (NEC)

ALL WIRING SHALL BE 12 GA

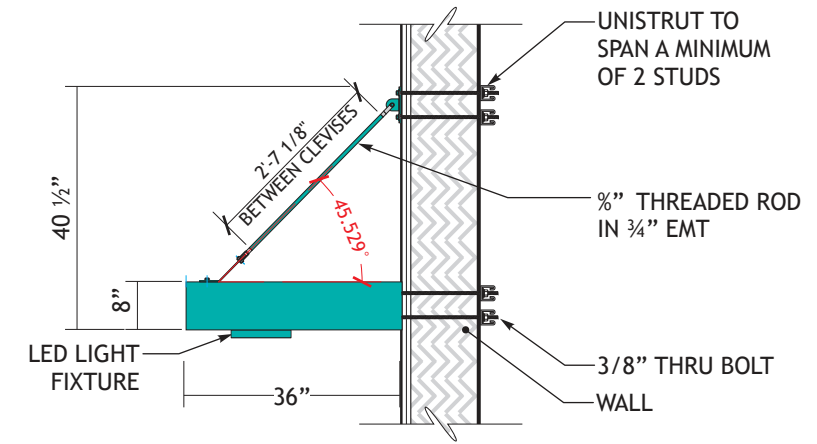
(1) 20 AMP-120V CIRCUIT TO BE BROUGHT TO JOB SITE BY LICENCED ELECTRICIAN.

ALL SIGNAGE WILL BE UL2161 COMPLIANT AND CARRY UL LABELS.

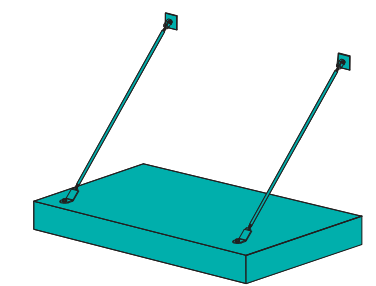
COLOR

PMS 326 C TEAL

(1) LEFT SIDE - CORNER



2 CANOPY END VIEW
SCALE: 3/8" = 1'-0"



3 CANOPY ISOMETRIC VIEW
SCALE: NTS

	<p>THIS SIGN IS INTENDED TO BE INSTALLED IN ACCORDANCE WITH THE REQUIREMENTS OF ARTICLE 600 OF THE NATIONAL ELECTRICAL CODE AND/OR OTHER APPLICABLE LOCAL CODES.</p> <p>THIS INCLUDES PROPER GROUNDING AND BONDING OF THE SIGN.</p>
<p>ELECTRIC SIGN SECTION</p>	

NOTE: ADD SILICONE SEALANT TO ALL SEAMS ON TOP OF THE CANOPIES.



LOREN INDUSTRIES

12226 Coast Drive
Whittier, CA 90601
Tel: (562) 946-7545
Fax: (562) 949-5707
St. Lic.: 455415

Los Angeles, CA



DRAWING/REVISION NO.:

09-21-0008

PAGE NO.:

11 OF 20

CLIENT:

POPEYES

ADDRESS

1771 SE Ensign Lane
Warrenton, OR 97146

Approval Signature

DATE:

09/16/2021

PROJECT MANAGER

Dave P.

DESIGNER:

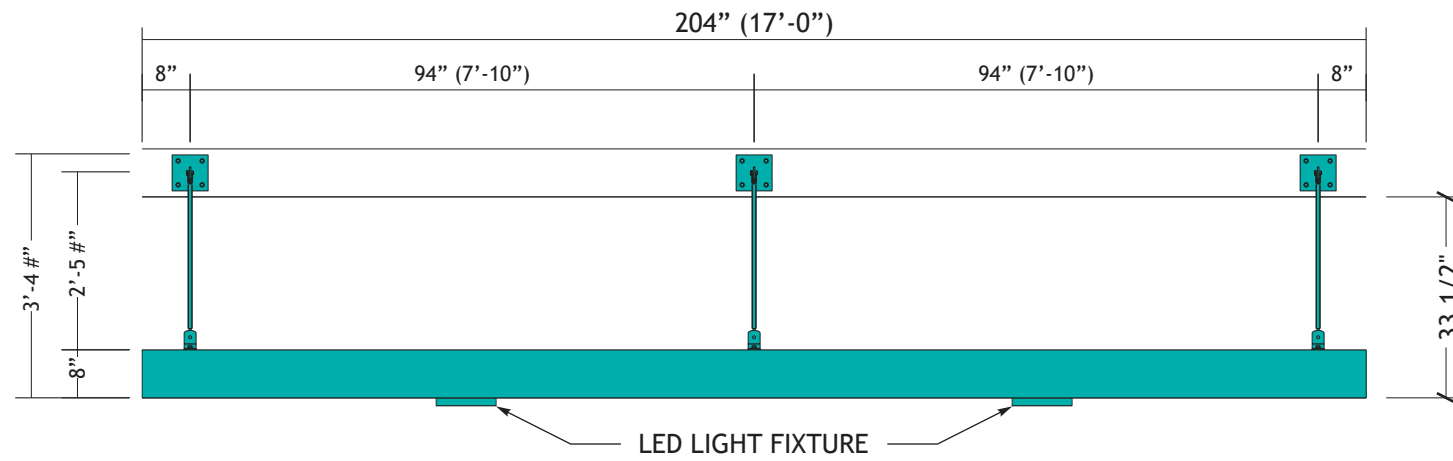
EC

REVISION DATE:

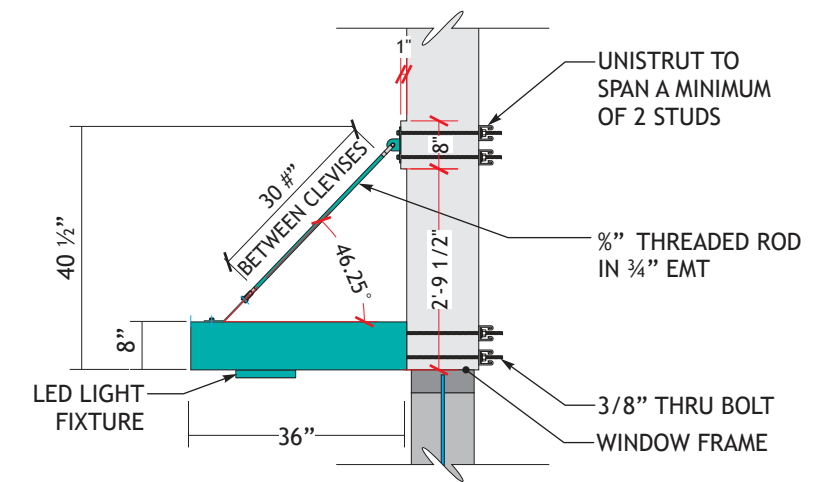
REVISION BY:

ALL IDEAS, PLANS, AND ELECTRONIC ART INDICATED ON THIS DRAWING ARE COPYRIGHTED AND OWNED BY LOREN ELECTRIC SIGN CORPORATION AND SHALL NOT BE REPRODUCED, USED BY OR DISCLOSED TO ANY PERSONS, FIRM OR CORPORATION FOR ANY PURPOSE WHATSOEVER WITHOUT PERMISSION OF LOREN ELECTRIC SIGN CORPORATION.

(1) FRONT



N6 CANOPY FRONT VIEW **QTY: 1**
SCALE: 3/8" = 1'-0"



2 CANOPY END VIEW
SCALE: 3/8" = 1'-0"

SPECIFICATIONS:

2. 2" X 8" ALUMINUM RECT. TUBE CANOPY FRAME WITH .090" BRAKE FORMED STIFFENERS
3. .090" ALUMINUM TOP & BOTTOM COVERS
4. PAINT: - TOP, EDGE, BOTTOM AND SAG RODS PAINTED TO MATCH - PMS 326 C TEAL
5. LIGHTING - LED LIGHT FIXTURE

NOTES:

ELECTRICAL COMPONENTS TO BE UL LISTED AND INSTALLED IN ACCORDANCE WITH THE NATIONAL ELECTRIC CODE (NEC)

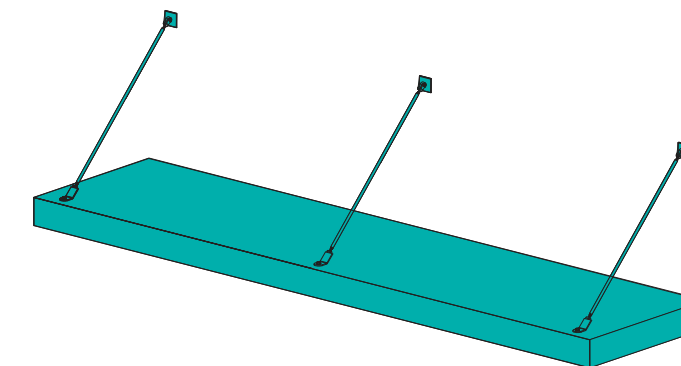
ALL WIRING SHALL BE 12 GA

(1) 20 AMP-120V CIRCUIT TO BE BROUGHT TO JOB SITE BY LICENCED ELECTRICIAN.

ALL SIGNAGE WILL BE UL2161 COMPLIANT AND CARRY UL LABELS.

COLOR

PMS 326 C TEAL



3 CANOPY ISOMETRIC VIEW
SCALE: NTS

	THIS SIGN IS INTENDED TO BE INSTALLED IN ACCORDANCE WITH THE REQUIREMENTS OF ARTICLE 600 OF THE NATIONAL ELECTRICAL CODE AND/OR OTHER APPLICABLE LOCAL CODES.
ELECTRIC SIGN SECTION	THIS INCLUDES PROPER GROUNDING AND BONDING OF THE SIGN.

NOTE: ADD SILICONE SEALANT TO ALL SEAMS ON TOP OF THE CANOPIES.



LOREN INDUSTRIES

12226 Coast Drive
Whittier, CA 90601
Tel: (562) 946-7545
Fax: (562) 949-5707
St. Lic.: 455415

Los Angeles, CA



DRAWING/REVISION NO.:

09-21-0008

PAGE NO.:

12 OF 20

CLIENT:

POPEYES

ADDRESS

1771 SE Ensign Lane
Warrenton, OR 97146

Approval Signature

DATE:

09/16/2021

PROJECT MANAGER

Dave P.

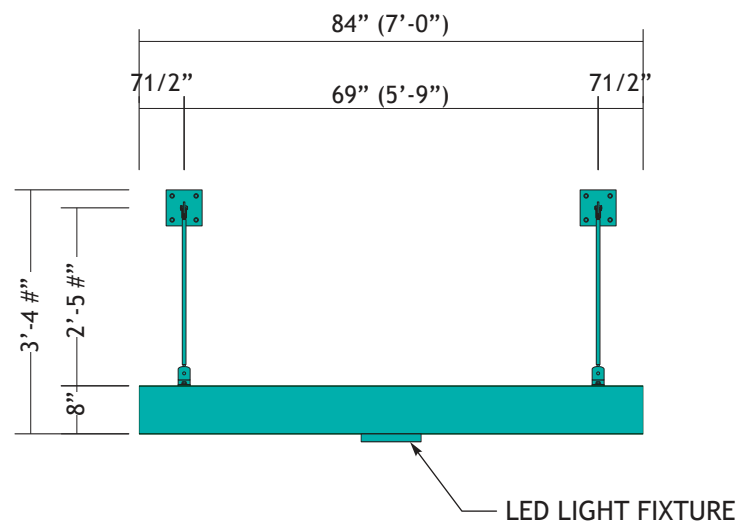
DESIGNER:

EC

REVISION DATE:

REVISION BY:

ALL IDEAS, PLANS, AND ELECTRONIC ART INDICATED ON THIS DRAWING ARE COPYRIGHTED AND OWNED BY LOREN ELECTRIC SIGN CORPORATION AND SHALL NOT BE REPRODUCED, USED BY OR DISCLOSED TO ANY PERSONS, FIRM OR CORPORATION FOR ANY PURPOSE WHATSOEVER WITHOUT PERMISSION OF LOREN ELECTRIC SIGN CORPORATION.



N7 CANOPY FRONT VIEW **QTY: 1**
SCALE: 3/8" = 1'-0"

SPECIFICATIONS:

- 2" X 8" ALUMINUM RECT. TUBE CANOPY FRAME WITH .090" BRAKE FORMED STIFFENERS
- .090" ALUMINUM TOP & BOTTOM COVERS
- PAINT: - TOP, EDGE, BOTTOM AND SAG RODS PAINTED TO MATCH - PMS 326 C TEAL
- LIGHTING - LED LIGHT FIXTURE

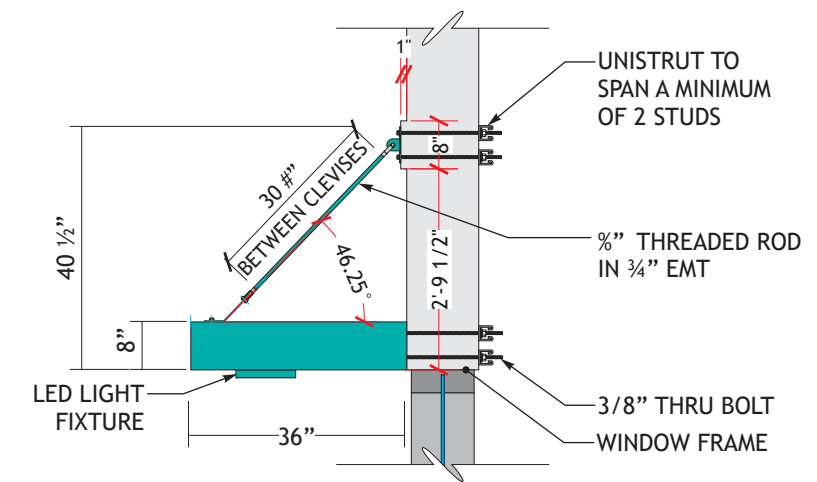
NOTES:

ELECTRICAL COMPONENTS TO BE UL LISTED AND INSTALLED IN ACCORDANCE WITH THE NATIONAL ELECTRIC CODE (NEC)
 ALL WIRING SHALL BE 12 GA
 (1) 20 AMP-120V CIRCUIT TO BE BROUGHT TO JOB SITE BY LICENCED ELECTRICIAN.
 ALL SIGNAGE WILL BE UL2161 COMPLIANT AND CARRY UL LABELS.

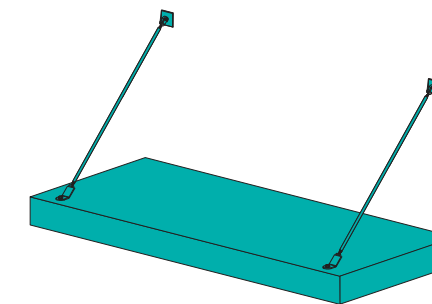
COLOR

PMS 326 C TEAL

(1) RIGHT SIDE



2 CANOPY END VIEW
SCALE: 3/8" = 1'-0"



3 CANOPY ISOMETRIC VIEW
SCALE: NTS

UL LISTED	THIS SIGN IS INTENDED TO BE INSTALLED IN ACCORDANCE WITH THE REQUIREMENTS OF ARTICLE 600 OF THE NATIONAL ELECTRICAL CODE AND/OR OTHER APPLICABLE LOCAL CODES. THIS INCLUDES PROPER GROUNDING AND BONDING OF THE SIGN.
------------------	--

NOTE: ADD SILICONE SEALANT TO ALL SEAMS ON TOP OF THE CANOPIES.



LOREN INDUSTRIES

12226 Coast Drive
Whittier, CA 90601
Tel: (562) 946-7545
Fax: (562) 949-5707
St. Lic.: 455415
Los Angeles, CA



DRAWING/REVISION NO.:
09-21-0008

PAGE NO.:
13 OF **20**

CLIENT:
POPEYES

ADDRESS
1771 SE Ensign Lane
Warrenton, OR 97146

Approval Signature

DATE:
09/16/2021

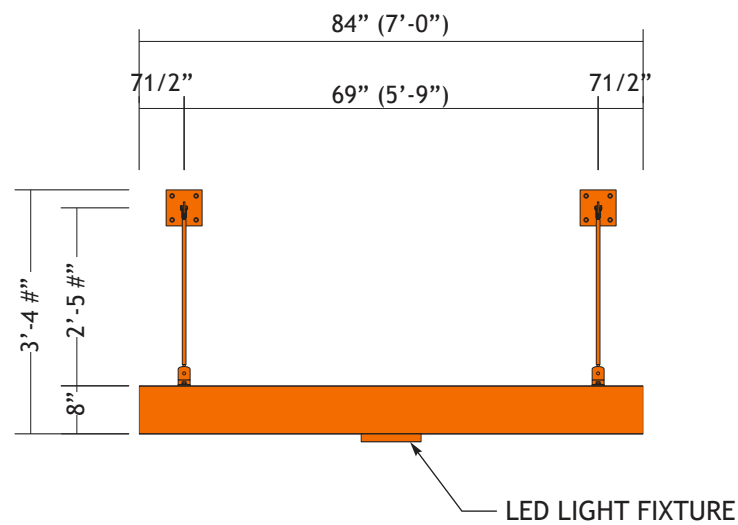
PROJECT MANAGER
Dave P.

DESIGNER:
EC

REVISION DATE:

REVISION BY:

ALL IDEAS, PLANS, AND ELECTRONIC ART INDICATED ON THIS DRAWING ARE COPYRIGHTED AND OWNED BY LOREN ELECTRIC SIGN CORPORATION AND SHALL NOT BE REPRODUCED, USED BY OR DISCLOSED TO ANY PERSONS, FIRM OR CORPORATION FOR ANY PURPOSE WHATSOEVER WITHOUT PERMISSION OF LOREN ELECTRIC SIGN CORPORATION.



N8 CANOPY (MAIN ENTRANCE) FRONT VIEW QTY: 1 SCALE: 3/8" = 1'-0"

SPECIFICATIONS:

- 2" X 8" ALUMINUM RECT. TUBE CANOPY FRAME WITH .090" BRAKE FORMED STIFFENERS
- .090" ALUMINUM TOP & BOTTOM COVERS
- PAINT: - TOP, EDGE, BOTTOM AND SAG RODS PAINTED TO MATCH - PMS 3564 C
- LIGHTING - LED LIGHT FIXTURE

NOTES:

ELECTRICAL COMPONENTS TO BE UL LISTED AND INSTALLED IN ACCORDANCE WITH THE NATIONAL ELECTRIC CODE (NEC)

ALL WIRING SHALL BE 12 GA

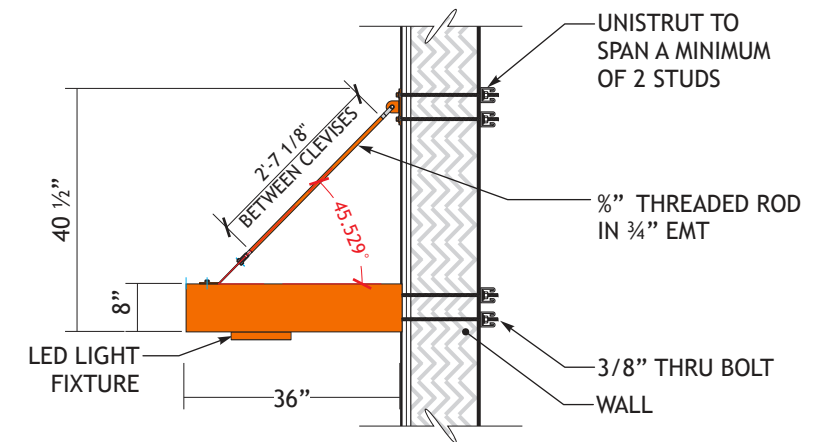
(1) 20 AMP-120V CIRCUIT TO BE BROUGHT TO JOB SITE BY LICENCED ELECTRICIAN.

ALL SIGNAGE WILL BE UL2161 COMPLIANT AND CARRY UL LABELS.

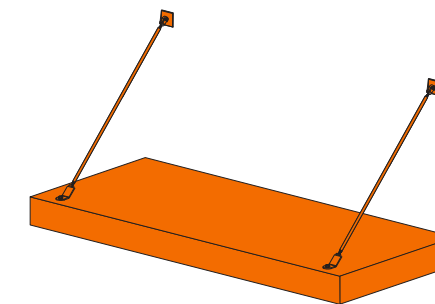
COLOR

 PMS 3564 C TANGERINE


(1) RIGHT SIDE-BRICK



2 CANOPY END VIEW SCALE: 3/8" = 1'-0"



3 CANOPY ISOMETRIC VIEW SCALE: NTS

 <p>UL LISTED</p> <p>ELECTRIC SIGN SECTION</p>	<p>THIS SIGN IS INTENDED TO BE INSTALLED IN ACCORDANCE WITH THE REQUIREMENTS OF ARTICLE 600 OF THE NATIONAL ELECTRICAL CODE AND/OR OTHER APPLICABLE LOCAL CODES.</p>
	<p>THIS INCLUDES PROPER GROUNDING AND BONDING OF THE SIGN.</p>

NOTE: ADD SILICONE SEALANT TO ALL SEAMS ON TOP OF THE CANOPIES.



LOREN INDUSTRIES

12226 Coast Drive
Whittier, CA 90601
Tel: (562) 946-7545
Fax: (562) 949-5707
St. Lic.: 455415
Los Angeles, CA



DRAWING/REVISION NO.:
09-21-0008

PAGE NO.:
14 OF **20**

CLIENT:
POPEYES

ADDRESS
1771 SE Ensign Lane
Warrenton, OR 97146

Approval Signature

DATE:
09/16/2021

PROJECT MANAGER
Dave P.

DESIGNER:
EC

REVISION DATE:

REVISION BY:

ALL IDEAS, PLANS, AND ELECTRONIC ART INDICATED ON THIS DRAWING ARE COPYRIGHTED AND OWNED BY LOREN ELECTRIC SIGN CORPORATION AND SHALL NOT BE REPRODUCED, USED BY OR DISCLOSED TO ANY PERSONS, FIRM OR CORPORATION FOR ANY PURPOSE WHATSOEVER WITHOUT PERMISSION OF LOREN ELECTRIC SIGN CORPORATION.



LOREN

LOREN INDUSTRIES

12226 Coast Drive
Whittier, CA 90601

Tel: (562) 946-7545

Fax: (562) 949-5707

St. Lic.: 455415

Los Angeles, CA



DRAWING/REVISION NO.:

09-21-0008

PAGE NO.:

15 OF 20

CLIENT:

POPEYES

ADDRESS:

1771 SE Ensign Lane
Warrenton, OR 97146

Approval Signature

DATE:

09/16/2021

PROJECT MANAGER

Dave P.

DESIGNER:

EC

REVISION DATE:

REVISION BY:

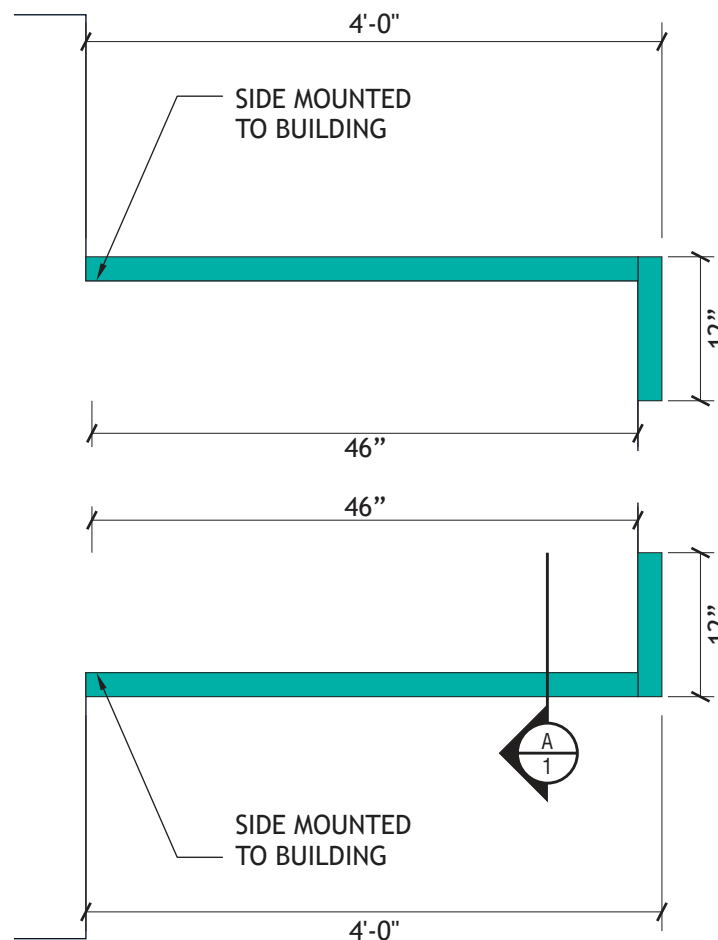
ALL IDEAS, PLANS, AND ELECTRONIC ART INDICATED ON THIS DRAWING ARE COPYRIGHTED AND OWNED BY LOREN ELECTRIC SIGN CORPORATION AND SHALL NOT BE REPRODUCED, USED BY OR DISCLOSED TO ANY PERSONS, FIRM OR CORPORATION FOR ANY PURPOSE WHATSOEVER WITHOUT PERMISSION OF LOREN ELECTRIC SIGN CORPORATION.

NOTE
SITE VERIFY WALL DIMENSION

NOTE
EXACT LENGTHS TO BE CUT IN THE FIELD BY INSTALLER

RIGHT ELEVATION

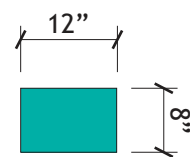
TOP VIEW



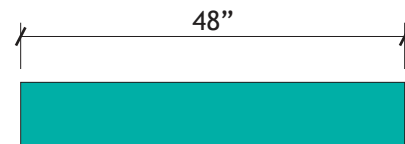
LEFT ELEVATION

TOP VIEW

Scale: 3/4"=1'-0"



(QTY: 2)
FRONT VIEW



(QTY: 2)
FRONT VIEW

.063 ALUMINUM
PAINTED TO MATCH
PMS 326 C TEAL

.063 ALUMINUM
PAINTED TO MATCH
PMS 326 C TEAL

24GA GALVALUM
PMS 326 C TEAL
BACK SUPPORT

3/8"X3 1/2"
LAG SCREWS
4' OC

EXT. WALL



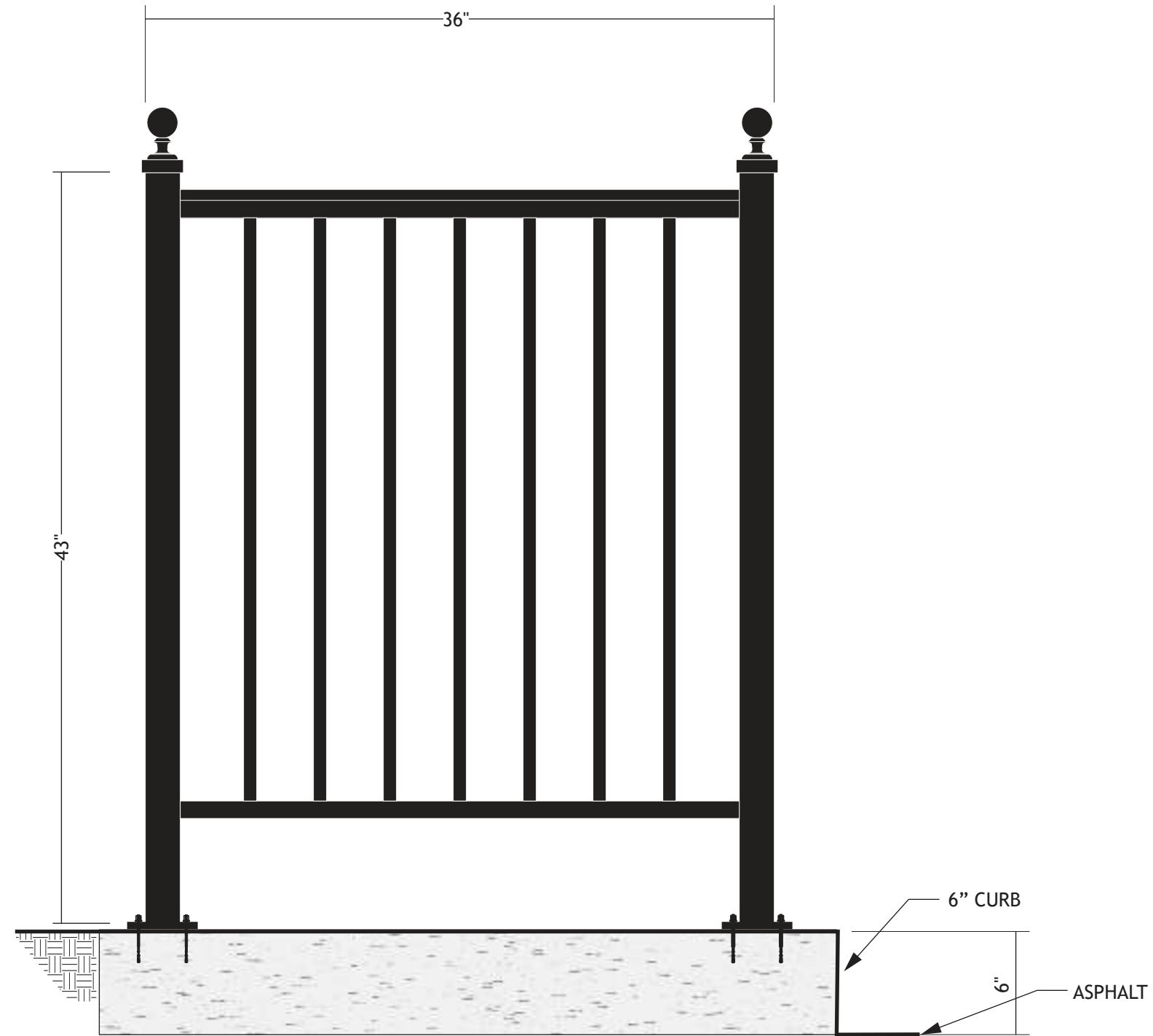
SECTION DETAIL

Scale: NTS

N9

NON-ILLUMINATED 8" TALL BUILDING FASCIA BAND (SIMPLE DESIGN)

Scale: 1/2"=1'-0"



SPECIFICATIONS:
 1. ALUMINUM SQUARE TUBE RAILINGS
 2. PAINT - BLACK

N10 GUARD RAIL WITH PLATE
 FRONT VIEW

SCALE: 1 1/2" = 1'-0"



LOREN

LOREN INDUSTRIES

12226 Coast Drive
 Whittier, CA 90601

Tel: (562) 946-7545

Fax: (562) 949-5707

St. Lic.: 455415

Los Angeles, CA



DRAWING/REVISION NO.:

09-21-0008

PAGE NO.:

16 OF 20

CLIENT:

POPEYES

ADDRESS

1771 SE Ensign Lane
 Warrenton, OR 97146

Approval Signature

DATE:

09/16/2021

PROJECT MANAGER

Dave P.

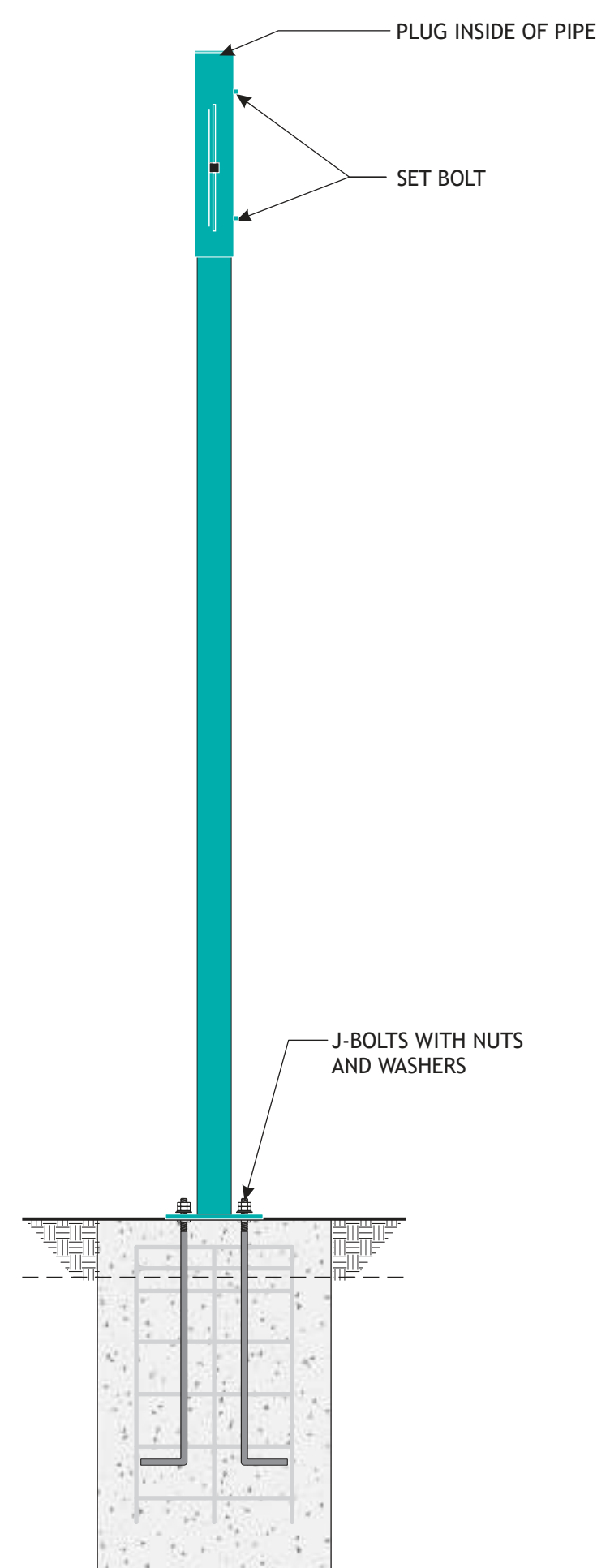
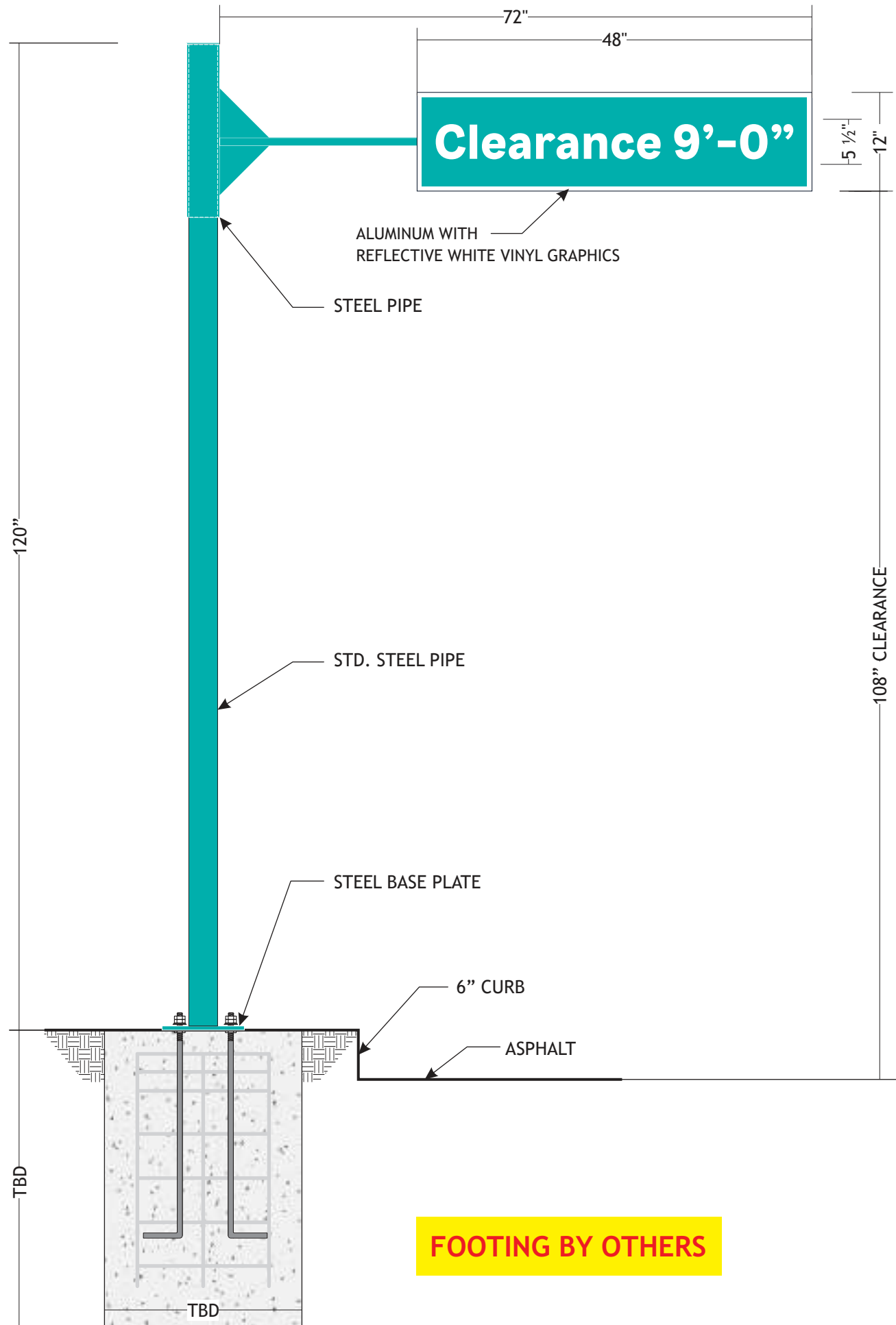
DESIGNER:

EC

REVISION DATE:

REVISION BY:

ALL IDEAS, PLANS, AND ELECTRONIC ART INDICATED ON THIS DRAWING ARE COPYRIGHTED AND OWNED BY LOREN ELECTRIC SIGN CORPORATION AND SHALL NOT BE REPRODUCED, USED BY OR DISCLOSED TO ANY PERSONS, FIRM OR CORPORATION FOR ANY PURPOSE WHATSOEVER WITHOUT PERMISSION OF LOREN ELECTRIC SIGN CORPORATION.



- SPECIFICATIONS:**
1. ALUMINUM PANEL WITH REFLECTIVE WHITE VINYL GRAPHICS
 2. TUBE STEEL HORIZONTAL ARM
 3. PIPE SLEEVE, WITH CAP PLATE
 4. PIPE MAIN SUPPORT
 5. STEEL BASE PLATE
 6. STEEL J-BOLTS
 7. PAINT: PMS 326 C - TEAL
- NOTE:**
TEMPLATE OF BASE PLATE & J-BOLTS TO SHIP AHEAD OF PIPE.



12226 Coast Drive
Whittier, CA 90601
Tel: (562) 946-7545
Fax: (562) 949-5707
St. Lic.: 455415
Los Angeles, CA



DRAWING/REVISION NO.:
09-21-0008

PAGE NO.:
17 OF 20

CLIENT:
POPEYES

ADDRESS
1771 SE Ensign Lane
Warrenton, OR 97146

Approval Signature

DATE:
09/16/2021

PROJECT MANAGER
Dave P.

DESIGNER:
EC

REVISION DATE:

REVISION BY:

ALL IDEAS, PLANS, AND ELECTRONIC ART INDICATED ON THIS DRAWING ARE COPYRIGHTED AND OWNED BY LOREN ELECTRIC SIGN CORPORATION AND SHALL NOT BE REPRODUCED, USED BY OR DISCLOSED TO ANY PERSONS, FIRM OR CORPORATION FOR ANY PURPOSE WHATSOEVER WITHOUT PERMISSION OF LOREN ELECTRIC SIGN CORPORATION.


N11 CLEARANCE BAR
FRONT VIEW


SCALE: 3/4" = 1'-0"

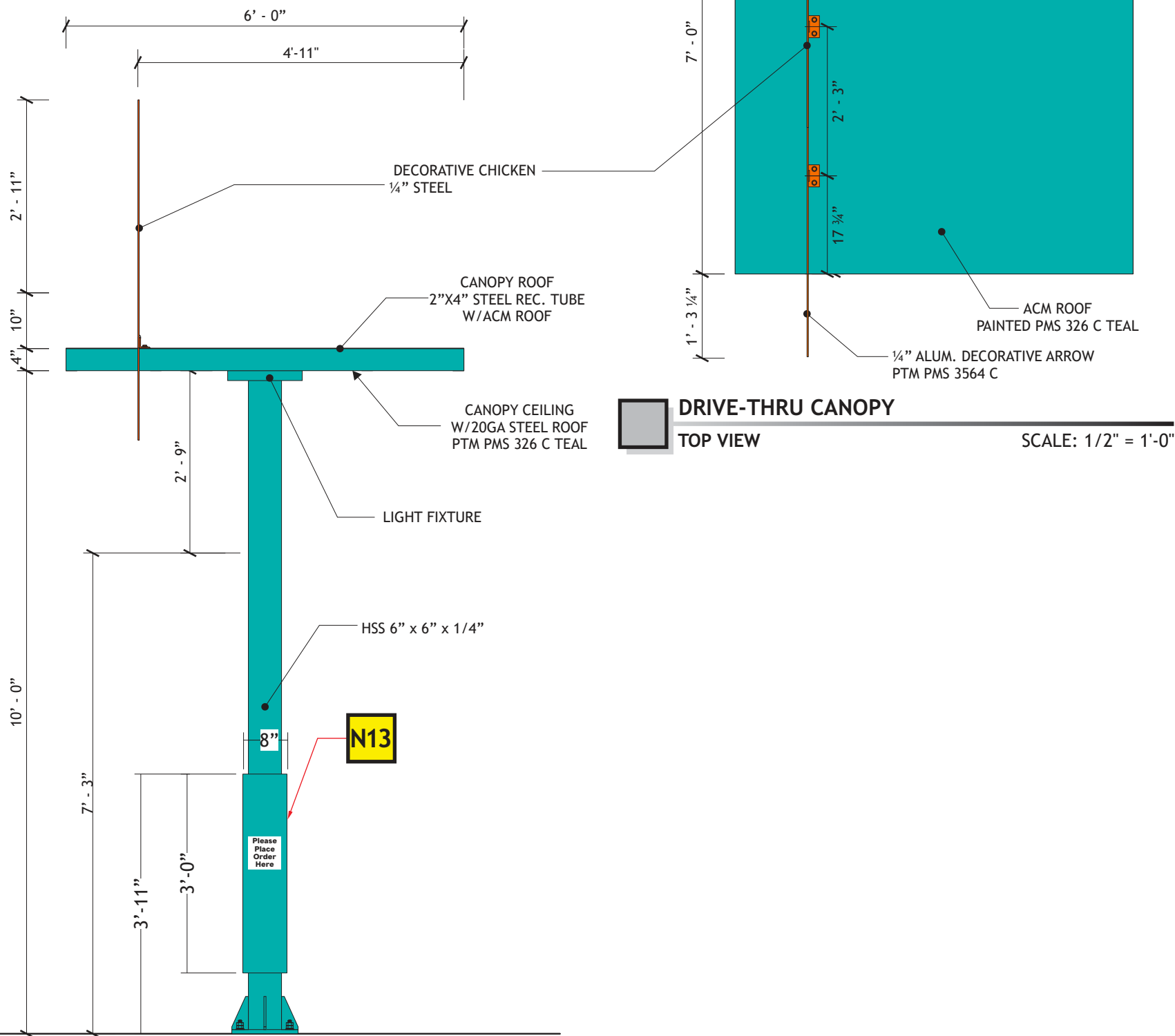
CLEARANCE BAR
SIDE VIEW

SCALE: 3/4" = 1'-0"

COLOR

 PMS 326 C TEAL

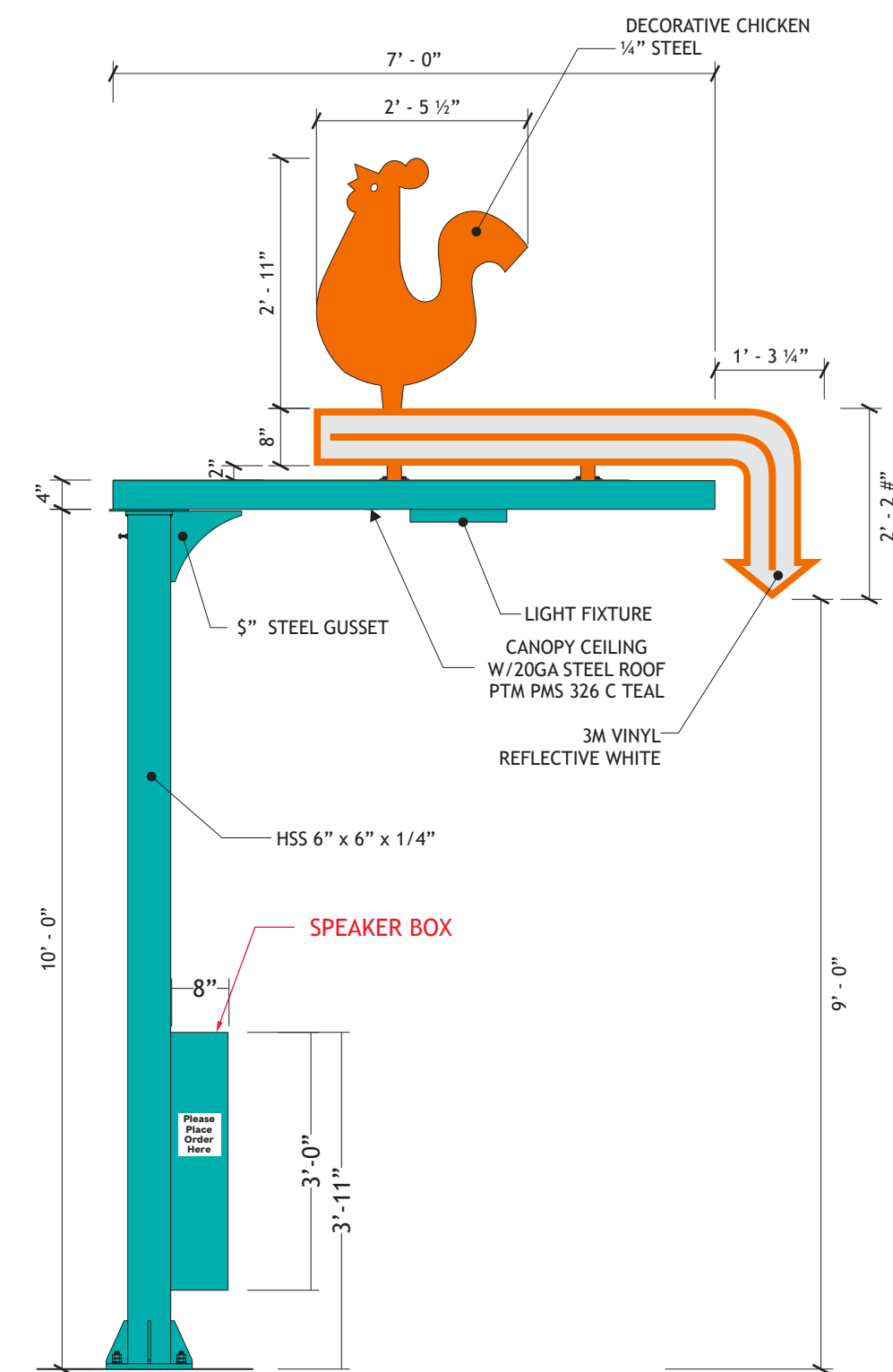
 PMS 3564 C ORANGE



 **DRIVE-THRU CANOPY**
TOP VIEW SCALE: 1/2" = 1'-0"

 **DRIVE-THRU CANOPY**
FRONT VIEW SCALE: 1/2" = 1'-0"

FOOTING BY OTHERS



 **DRIVE-THRU CANOPY**
SIDE VIEW SCALE: 1/2" = 1'-0"



LOREN INDUSTRIES

12226 Coast Drive
Whittier, CA 90601
Tel: (562) 946-7545
Fax: (562) 949-5707
St. Lic.: 455415
Los Angeles, CA



DRAWING/REVISION NO.:
09-21-0008

PAGE NO.:
18 OF 20

CLIENT:
POPEYES

ADDRESS
1771 SE Ensign Lane
Warrenton, OR 97146

Approval Signature

DATE:
09/16/2021

PROJECT MANAGER
Dave P.

DESIGNER:
EC

REVISION DATE:

REVISION BY:

ALL IDEAS, PLANS, AND ELECTRONIC ART INDICATED ON THIS DRAWING ARE COPYRIGHTED AND OWNED BY LOREN ELECTRIC SIGN CORPORATION AND SHALL NOT BE REPRODUCED, USED BY OR DISCLOSED TO ANY PERSONS, FIRM OR CORPORATION FOR ANY PURPOSE WHATSOEVER WITHOUT PERMISSION OF LOREN ELECTRIC SIGN CORPORATION.



LOREN

LOREN INDUSTRIES

12226 Coast Drive
Whittier, CA 90601

Tel: (562) 946-7545

Fax: (562) 949-5707

St. Lic.: 455415

Los Angeles, CA



DRAWING/REVISION NO.:

09-21-0008

PAGE NO.:

19 OF 20

CLIENT:

POPEYES

ADDRESS

1771 SE Ensign Lane
Warrenton, OR 97146

Approval Signature

DATE:

09/16/2021

PROJECT MANAGER

Dave P.

DESIGNER:

EC

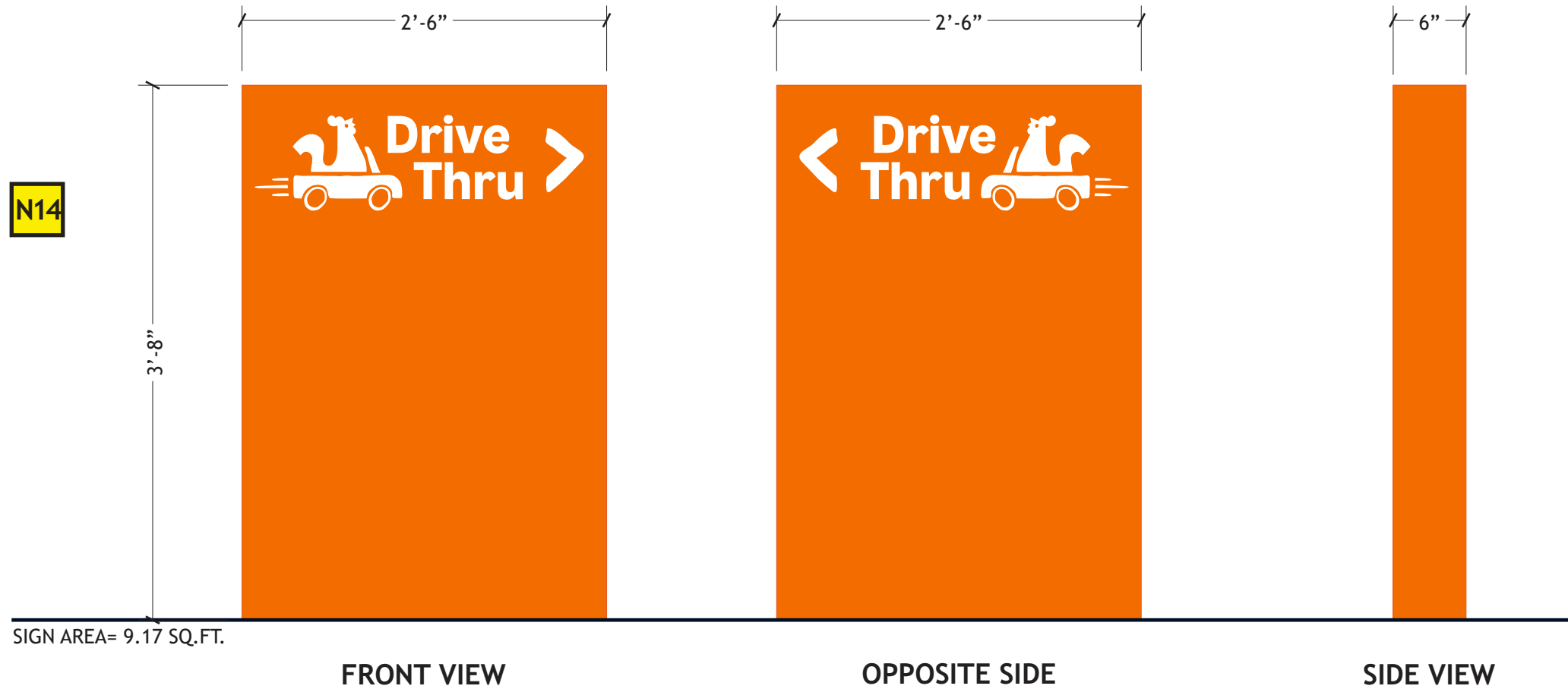
REVISION DATE:

REVISION BY:

COLOR SPECIFICATIONS:

 PMS 3564 C - 3M 3630-84 TANGERINE

 WHITE - BACK SPRAYED PAINT

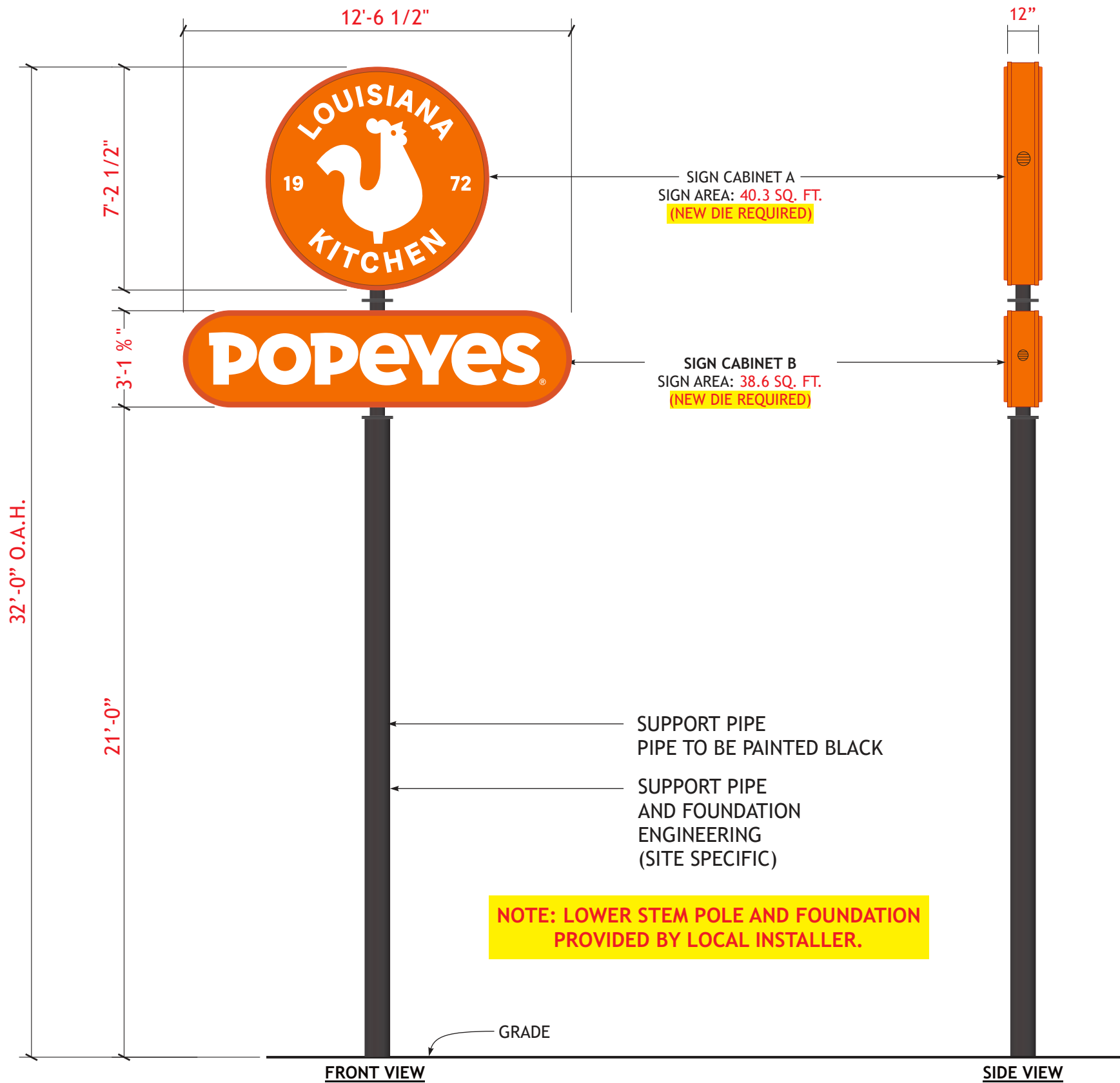


NOTE
STANDARD AND GRAPHICS LAYOUT
TO BE PROVIDED.
NOT READY FOR PRODUCTION

 MONOLITHIC DIRECTIONAL SIGN

SCALE: 1" = 1'-0"

ALL IDEAS, PLANS, AND ELECTRONIC ART INDICATED ON THIS DRAWING ARE COPYRIGHTED AND OWNED BY LOREN ELECTRIC SIGN CORPORATION AND SHALL NOT BE REPRODUCED, USED BY OR DISCLOSED TO ANY PERSONS, FIRM OR CORPORATION FOR ANY PURPOSE WHATSOEVER WITHOUT PERMISSION OF LOREN ELECTRIC SIGN CORPORATION.



CABINET A:
FORMED DOUBLE FACE CENTER POLE SIGN.
SECOND SURFACE APPLICATION.
ADDITIONAL NOTES , NOTED ON THE NEXT PAGE.
CABINET & RETURNS PAINTED BURNT ORANGE

CABINET B:
FORMED DOUBLE FACE CENTER POLE SIGN.
SECOND SURFACE APPLICATION.
ADDITIONAL NOTES , NOTED ON THE NEXT PAGE.
CABINET & RETURNS PAINTED BURNT ORANGE

Note:
Methods Of Attachment To Be Engineered.
Formal Stamped & Signed Engineering With
Calculations To Be Submitted With Permit
Drawings

UL LISTED

ELECTRIC SIGN SECTION

THIS SIGN IS INTENDED TO BE INSTALLED IN ACCORDANCE WITH THE REQUIREMENTS OF ARTICLE 600 OF THE NATIONAL ELECTRICAL CODE AND/OR OTHER APPLICABLE LOCAL CODES.

THIS INCLUDES PROPER GROUNDING AND BONDING OF THE SIGN.

NOTES:

ELECTRICAL COMPONENTS TO BE UL LISTED AND INSTALLED IN ACCORDANCE WITH THE NATIONAL ELECTRIC CODE (NEC)

ALL WIRING SHALL BE 12 GA

(1) 20 AMP-120V CIRCUIT TO BE BROUGHT TO JOB SITE BY LICENCED ELECTRICIAN.

ALL SIGNAGE WILL BE UL2161 COMPLIANT AND CARRY UL LABELS.

GENERAL NOTES:

- 1.) INSTALLATION OF THIS SIGN SHALL CONFORM TO THE REQUIREMENTS OF ARTICLE 600 OF NEC, UL48 AND/OR OTHER APPLICABLE LOCAL CODES.
- 2.) BRANCH CIRCUIT:
 - A.) ALL BRANCH CIRCUITS FOR SIGNS MUST BE TOTALLY DEDICATED TO SIGNS (INCLUDING DEDICATED GROUND AND DEDICATED NEUTRAL PER CIRCUIT).
 - B.) SIGN CIRCUITS MUST NOT BE SHARED WITH OTHER LOADS SUCH AS LIGHTING, AIR CONDITIONING AND OTHER EQUIPMENT.
 - C.) PROPERTY SIZED GROUND WIRE THAT CAN BE TRACKED TO THE BREAKER PANEL MUST BE PROVIDED.
- 3.) U.L. AND DATA LABELS REQUIRED
- 4.) SIGN SATISFIES ALL REQUIREMENTS OF TITLE 24.
- 5.) OUTDOOR SIGNAGE TO EMPLOY EITHER PHOTOCELL OR OUTDOOR ASTRONOMICAL TIMER

COLOR SPECIFICATIONS:

- PMS 3564C - 3M 3630-84 TANGERINE
- WHITE - 3M 3630-20 WHITE
- BLACK PAINT



LOREN INDUSTRIES

12226 Coast Drive
Whittier, CA 90601
Tel: (562) 946-7545
Fax: (562) 949-5707
St. Lic.: 455415

Los Angeles, CA



DRAWING/REVISION NO.:
09-21-0008

PAGE NO.:
20 OF 20

CLIENT:
POPEYES

ADDRESS
1771 SE Ensign Lane
Warrenton, OR 97146

Approval Signature

DATE:
09/16/2021

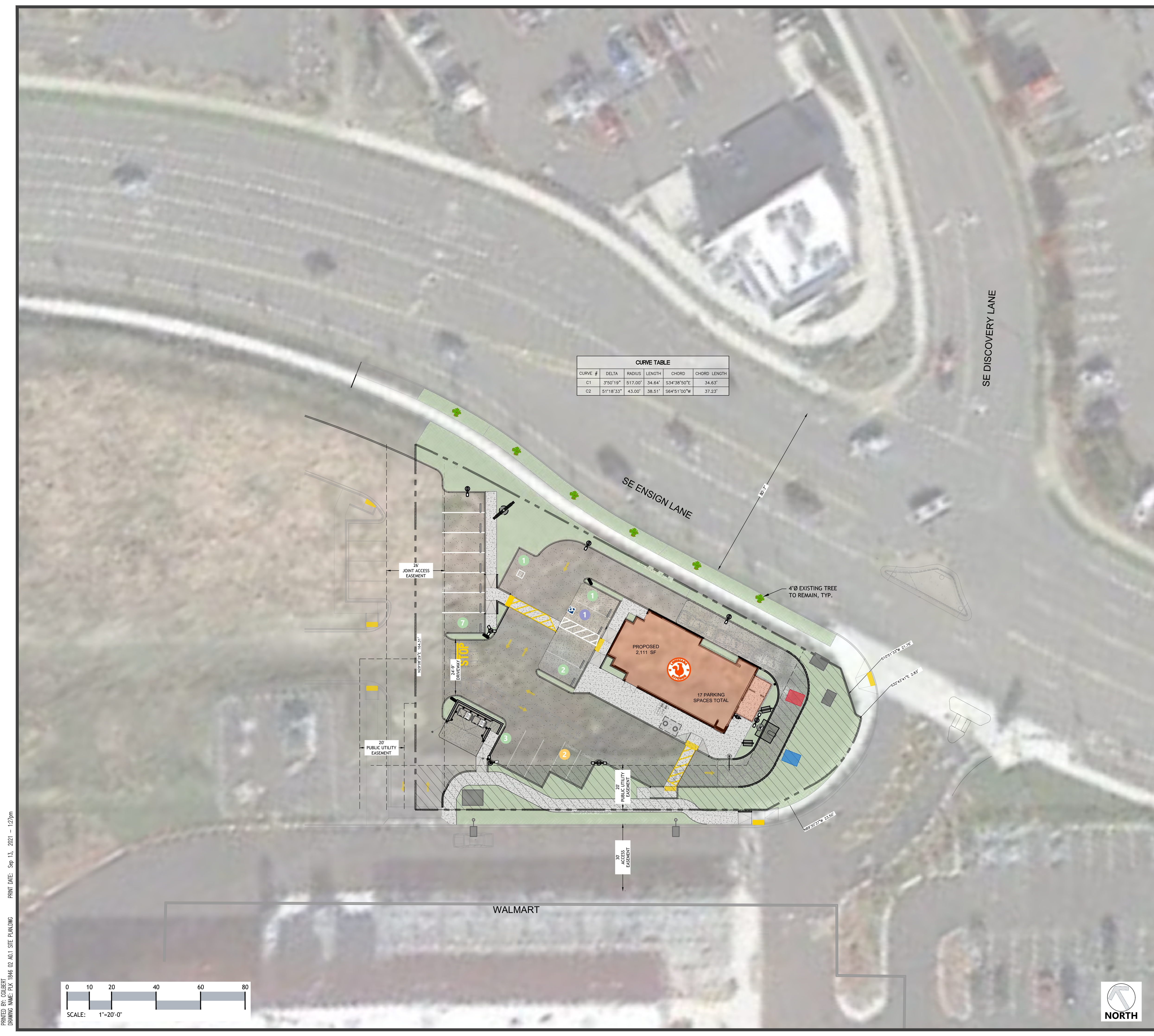
PROJECT MANAGER
Dave P.

DESIGNER:
EC

REVISION DATE:

REVISION BY:

ALL IDEAS, PLANS, AND ELECTRONIC ART INDICATED ON THIS DRAWING ARE COPYRIGHTED AND OWNED BY LOREN ELECTRIC SIGN CORPORATION AND SHALL NOT BE REPRODUCED, USED BY OR DISCLOSED TO ANY PERSONS, FIRM OR CORPORATION FOR ANY PURPOSE WHATSOEVER WITHOUT PERMISSION OF LOREN ELECTRIC SIGN CORPORATION.



CURVE TABLE					
CURVE #	DELTA	RADIUS	LENGTH	CHORD	CHORD LENGTH
C1	3°50'19"	517.00'	34.64'	S34°38'50"E	34.63'
C2	51°18'33"	43.00'	38.51'	S64°51'00"W	37.23'

SITE AND BUILDING DATA

ADMINISTRATIVE:
 TAX LOT: 81027CB01901
 ZONE: GENERAL COMMERCIAL (C-1)
SITE:
 EXISTING PROPERTY AREA: 21,756 SQ. FT. (0.50± ACRES)
BUILDING:
 TOTAL BUILDING AREA: 2,111 SQ. FT. BUILDING (NEW)
 % OF TOTAL SITE AREA: 9.7%
BUILDING USE:
 ASSEMBLY A-2 (RESTAURANT)
 USEABLE INTERIOR AREA: 1,798 SQ. FT.
 DINING AREA: 482 SQ. FT.
 WALK-IN AREA: 148 SQ. FT.
LEGAL DESCRIPTION:
 SW 1/4 OF SECTION 27, TOWNSHIP 8 NORTH, RANGE 10 WEST, WILLAMETTE MERIDIAN, CITY OF WARRENTON, COUNTY OF CLATSOP, STATE OF OREGON.

THE SUBJECT PROPERTY LIES WITHIN ZONE X, AN AREA WITH MADERATE TO LOW RISK, PER FEMA FLOOD INSURANCE MAP NUMBER 41007C0218F, EFFECTIVE DATE JUNE 20, 2018.

KEN MCCrackEN, ARCHITECT
 Architecture • Program Management • Permitting

19120 SE 34TH STREET
 SUITE 115
 VANCOUVER, WA 98683
 PROJECT CONTACT: ERICK PETERSON
 PHONE: 360.980.8258
 EMAIL: EPETERSON@PMOAGINC.COM

KEN MCCrackEN, ARCHITECT
SEAL:
 REGISTERED ARCHITECT
 KENNETH J. MCCrackEN
 CAMAS, WA
 License # 5599
 STATE OF OREGON
 08/30/21
 KJM/xxxx

REQUIRED TOTAL PARKING

	1 SPACE PER 4 SEATS	1 SPACE PER 100 SF	BUILDING SIZE (SF)	MIN. REQ.	MAX. REQ.	PROVIDED
TOTAL PARKING	24 SEATS/4	2,111/100	2111	6	21	17

*REQUIRED MAXIMUM PARKING IS BASED ON WARRENTON DEVELOPMENT CODE, TABLE 16.128.030.A 1 SPACE PER 100 SQ. FT. OF GROSS FLOOR AREA.
 MINIMUM PARKING IS BASED ON WARRENTON DEVELOPMENT CODE, TABLE 16.128.030.A 1 SPACE PER 4 SEATS.
 ** ACCESSIBLE PARKING REQUIREMENTS ARE BASED ON 2019 OSSC TABLE 1106.1

ACCESSIBLE PARKING

	REQUIRED	PROVIDED
ACCESSIBLE VAN PARKING (TABLE 1106.1)	1	1

PARKING CALCULATIONS

	NO SCALE	
		2

KEY	DESCRIPTION
(E) STRIPED SAFETY PATH, ACCESS PATH, OR CROSSWALK	
(N) CONCRETE PAVED SURFACE	
(N) ASPHALT PAVEMENT	
(E) EASEMENT, SEE ALTA SURVEY	
POLE MOUNTED SITE LIGHT	
ACCESSIBLE PATH OF TRAVEL	
PROPERTY LINE	
(E) FENCE LINE	
#	STANDARD PARKING
#	COMPACT PARKING
#	ACCESSIBLE PARKING
#	ACCESSIBLE VAN PARKING

NOTE: NOT ALL SYMBOLS MAY BE APPLICABLE TO, AND USED, IN THIS DRAWING.

SITE PLAN LEGEND

	NO SCALE	
		1

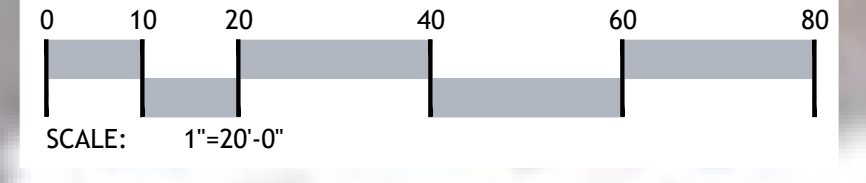


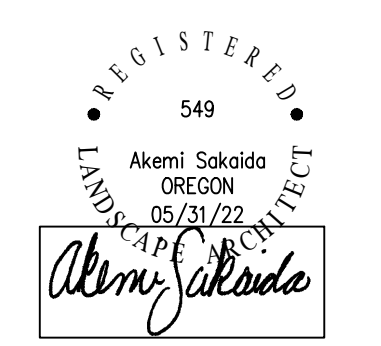
LOUISIANA KITCHEN
 SITE NO.: 00xxx
 1771 SE Ensign Lane
 Warrenton, Oregon 97146

REV	DATE	DESCRIPTION
△	07.29.21	PRE-APPLICATION

DRAWN BY: CDG
 CHECKED BY: ...
 ARCH. PROJECT NO.:
AMB21002.0
 SHEET NAME:
COMMERCIAL SITE DESIGN
 SHEET NUMBER:
A-0.0

PRINTED BY: CALDERET
 DRAWING NAME: POK_1846_02_A0.1 SITE PLAN.DWG
 PRINT DATE: Sep 13, 2021 - 1:27pm





LOUISIANA KITCHEN
SITE NO.: 00xxx
1771 SE Ensign Lane
Warrenton, Oregon 97146

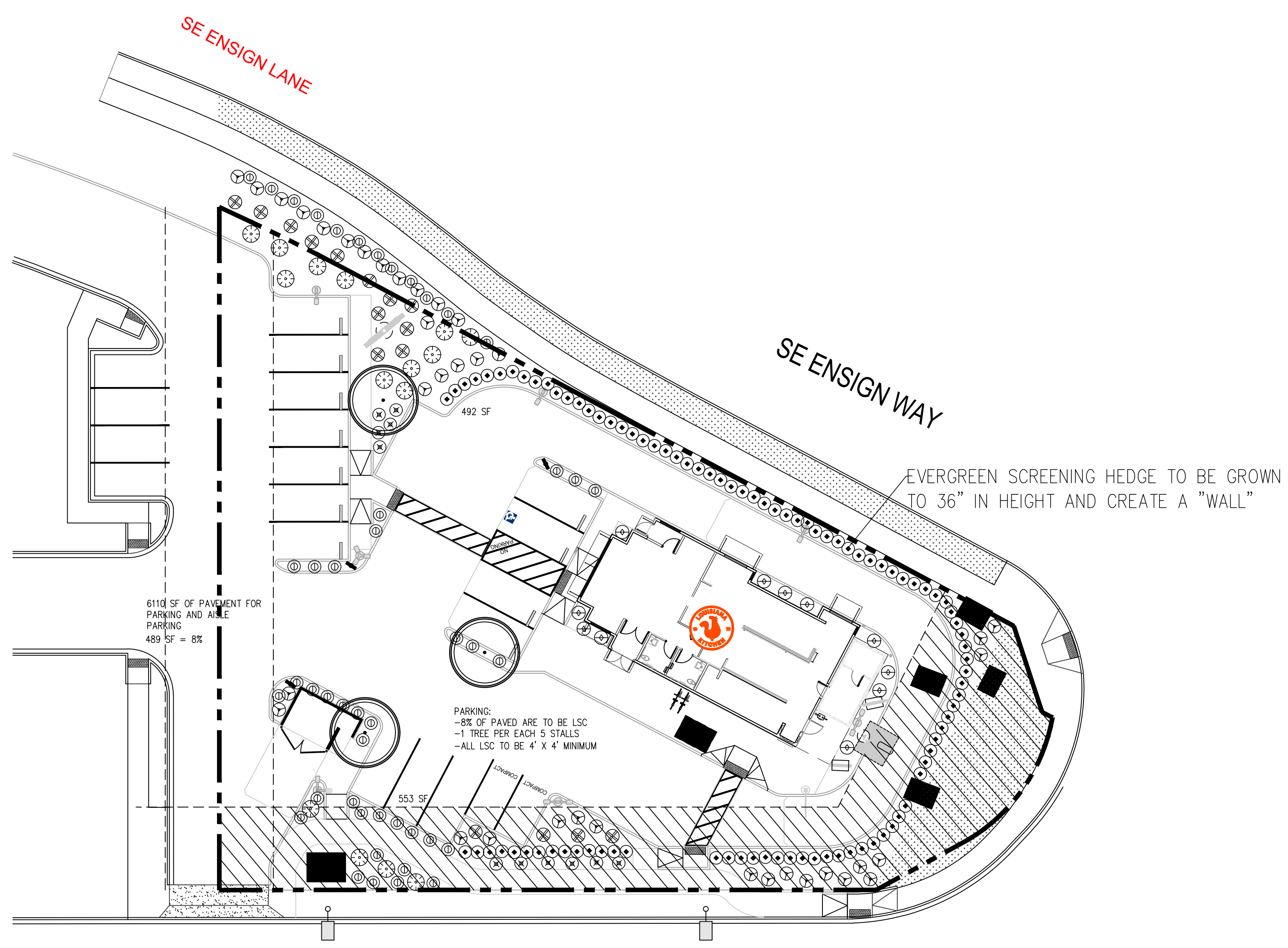
07-29-21	PRE-APPLICATION
09-03-21	SITE DEVELOPMENT REVIEW
9-10-21	PERMIT

DRAWN BY: CDG
CHECKED BY: ...

AMB21002.0

PLANTING PLAN

L1.1



GENERAL LANDSCAPE NOTES:
 -15% OF A COMMERCIAL SITE MUST BE LANDSCAPED
 -HARDSCAPE FEATURES MAY COVER UP TO 50% OF REQUIRED LSC AREA
 -NON PLANT GROUNDCOVERS: NO MORE THAN 50%
 -TREES: 15" CALIPER MINIMUM ON SITE
 - STREET TREES: 2" CALIPER MINIMUM
 - SHRUB: MULTI-GALLON CONTAINERS OR B&B
 - GROUNDCOVER: SIZED AND SPACED TO COVER A MINIMUM OF 30% WITHIN 2 YEARS

RETAINED OR SIGNIFICANT VEGETATION

PLANT SCHEDULE

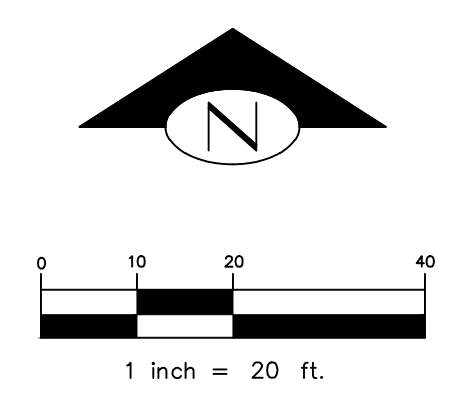
TREES

SYMBOL	SCIENTIFIC NAME COMMON NAME	QUANTITY	SIZE (MIN.)	SPACING (MAX.)	NOTES
	LAGERSTROEMIA 'MUSKOGEE' MUSKOGEE CREPE MYRTLE	3	1 1/2" CAL. (MIN.)	AS SHOWN	MATCHING FORM, SINGLE TRUNK, LOWER BRANCHES CLEAR AT 5' HEIGHT

SHRUBS

SYMBOL	SCIENTIFIC NAME COMMON NAME	QUANTITY	SIZE (MIN.)	SPACING (MAX.)	NOTES
	MAHONIA REPENS CREEPING BARBERRY	46	1 GALLON	AS SHOWN	FULL, WELL SHAPED
	MISCANTHUS ADIAGO ADIAGO COMPACT MAIDEN GRASS	16	1 GALLON	AS SHOWN	NICE FOLIAGE, MULTI STEMS
	NANDINA DOMESTICA 'HARBOUR DWARF' HARBOUR DWARF HEAVENLY BAMBOO	11	3 GALLON	AS SHOWN	FULL, WELL SHAPED
	PINUS MUGHO MUGHO PINE	15	2 GALLON	AS SHOWN	MULTI-STEM, FULL
	PRUNUS OTTO LUYKEN OTTO LUYKEN LAUREL	94	3 GALLON	AS SHOWN	FULL, HEALTHY FOLIAGE, STRONG
	ROSA RUGOSA PAVEMENT PAVEMENT ROSE	37	2 GALLON	AS SHOWN	NICE FOLIAGE, MULTI STEMS
	POTENTILLA 'PRIMROSE BEAUTY' PRIMROSE BEAUTY CINQUIFOL	11	2 GALLON	AS SHOWN	BUSHY, FULL

NOTE:
 - ALL PLANTS TO MEET AMERICAN ASSOCIATION OF NURSERYMEN STANDARDS. ALL TREES TO HAVE SINGLE TRUNK AND MATCHING FORM.





Recording Instrument #: 201406983
Recorded By: Clatsop County Clerk
of Pages: 27 Fee: 182.00
Transaction date: 10/30/2014 14:14:33
Deputy: nstethem

When recorded return to:
Laura Sever Blanco
Gust Rosenfeld P.L.C.
One East Washington, Suite 1600
Phoenix, AZ 85004-2553

Warrenton, OR #5861-01

**EASEMENTS WITH COVENANTS AND RESTRICTIONS
AFFECTING LAND ("ECR")**

THIS AGREEMENT is made as of the 30th day of October, 2014, by and among **WAL-MART REAL ESTATE BUSINESS TRUST**, a Delaware statutory trust ("Wal-Mart"), **PEAKS VIEW, LLC**, an Oregon limited liability company ("Developer") and **NORTH COAST RETAIL, LLC**, a Washington limited liability company ("NCR").

WITNESSETH:

WHEREAS, Wal-Mart is the owner of the Wal-Mart Tract as shown on the plan attached hereto as Exhibit A-1 hereof, said Tract being more particularly described in Exhibit B attached hereto;

WHEREAS, Developer is the owner of the Developer Tract shown on the plan attached hereto as Exhibit A-1 hereof, the same being more particularly described in Exhibit C hereof; and

WHEREAS, NCR is the owner of the NCR Tract shown on the plan attached hereto as Exhibit A-1 hereof, the same being more particularly described in Exhibit D hereof; and

WHEREAS, Wal-Mart, Developer and NCR desire that the Wal-Mart Tract, the Developer Tract and the NCR Tract be developed in conjunction with each other pursuant to a general plan of improvement to form a commercial Shopping Center (sometimes hereinafter referred to as the "Shopping Center"), and further desire that the Shopping Center be subject to the easements and the covenants, conditions and restrictions hereinafter set forth;

NOW, THEREFORE, for and in consideration of the premises, easements, covenants, conditions, restrictions, and encumbrances contained herein, the sufficiency of which is hereby acknowledged, Wal-Mart, Developer and NCR do hereby agree as follows:

17/32
Title Company

1. Building/Common Areas.

1.1 "Building Areas" as used herein shall mean those portions of the Shopping Center shown on Exhibit A-2 as "Building Area." Canopies may encroach from the Building Areas over the Common Areas provided the canopies do not interfere with the use of the Common Areas.

1.2 "Common Areas" shall be all of the Shopping Center except the Building Areas.

1.3 "Tracts" as used herein shall mean the Wal-Mart Tract, the Developer Tract and the NCR Tract.

1.4 Conversion to Common Areas: Those portions of the Building Areas which are not from time to time used or cannot, under the terms of this Agreement, be used for buildings shall become part of the Common Area for the uses permitted hereunder and shall be improved, kept and maintained as provided herein.

2. Use. Buildings in the Shopping Center shall be used for commercial purposes of the type normally found in a retail shopping center including, without limitation, financial institutions, service shops, offices, and retail stores. No cafeteria, theatre, bowling alley, billiard parlor, night club or other place of recreation or amusement, day care facility, child care facility, preschool or children's nursery, any business serving alcoholic beverages (except that restaurants serving alcohol as an incidental part of their business may do so), any business or facility used in growing, delivering, transferring, supplying, dispensing, dispersing, distributing or selling marijuana, whether by prescription, medical recommendation or otherwise, and whether consisting of live plants, seeds, seedlings or processed or harvested portions of the marijuana plant, shall occupy space within the Shopping Center without the prior written consent of Wal-Mart, which Wal-Mart may withhold in its sole and absolute discretion. Developer and NCR recognize that said businesses may inconvenience Wal-Mart's customers and adversely affect Wal-Mart's business. Notwithstanding anything to the contrary contained herein it is expressly agreed that nothing contained in this Agreement shall be construed to contain a covenant, either express or implied, to either commence the operation of a business or thereafter continuously operate a business by Wal-Mart on the Wal-Mart Tract. Developer and NCR recognize and agree that Wal-Mart may, at Wal-Mart's sole discretion and at any time during the term of this Agreement, cease the operation of its business on the Wal-Mart Tract; and Developer and NCR hereby waive any legal action for damages or for equitable relief which might be available to Developer or NCR because of such cessation of business activity by Wal-Mart.

3. Competing Business. Developer and NCR covenant that as long as Wal-Mart, or any affiliate of Wal-Mart, is the user of the Wal-Mart Tract, either as owner or lessee, no space in or portion of the Developer Tract or the NCR Tract shall be leased or occupied by or conveyed to any other party for use as (i) a facility dispensing gasoline or fuel from pumps, (ii) a membership warehouse club, (iii) a pharmacy, (iv) a discount department store or other discount store, as such terms are defined below, (v) a variety, general or "dollar" store, (vi) a grocery store or supermarket as such terms are defined below, or (vii) as any

combination of the foregoing uses. In the event of a breach of this covenant, Wal-Mart shall have the right to terminate this Agreement and to seek any and all remedies afforded by either law or equity, including, without limitation, the rights to injunctive relief. "Grocery store" and "supermarket," as those terms are used herein, shall mean a food store or a food department containing more than 3,000 square feet of building space used for the purpose of selling food for off premises consumption, which shall include but not be limited to the sale of dry, refrigerated or frozen groceries, meat, seafood, poultry, produce, delicatessen or bakery products, refrigerated or frozen dairy products, or any grocery products normally sold in such stores or departments. "Discount department store" and/or "discount store," as those terms are used herein, shall mean a discount department store or discount store containing more than 10,000 square feet of building space used for the purpose of selling a full line of hard goods and soft goods (e.g. clothing, cards, gifts, electronics, garden supplies, furniture, lawnmowers, toys, health and beauty aids, hardware items, bath accessories and auto accessories) at a discount in a retail operation similar to that of Wal-Mart.

4. Buildings.

4.1 Design and Construction. The Buildings constructed on the Shopping Center shall be designed so that the exterior elevation of each shall be architecturally and aesthetically compatible and so that building wall footings shall not encroach from one Tract onto another Tract except as provided for in Subsection 4.4 below. The design and construction shall be of high quality. No building constructed on the Wal-Mart Tract shall exceed 40' in height above finished grade. Incidental architectural embellishments and peaks shall not be considered in connection with determining compliance with said height restriction. No building constructed on the Developer Tract shall exceed 22' (including all mechanical improvements and architectural embellishments) in height, as measured from the mean finished elevation of the parking area of the Shopping Center. No building constructed on the NCR Tract shall exceed 35' (including all mechanical improvements and architectural embellishments) in height, as measured from the mean finished elevation of the parking area of the Shopping Center. No building shall have a metal exterior.

4.2 Location/Size. No building shall be constructed on the Shopping Center (as either immediate development or future expansion) except within the Building Areas. Any rooftop equipment constructed on the buildings located on the Developer Tract or the NCR Tract shall be screened so as not to be visible from the mean finished elevation of the parking area.

4.3 Fire Protection. Any building constructed in the Shopping Center shall be constructed and operated in such a manner which will preserve the sprinklered rate on the other buildings in the Shopping Center.

4.4 Easements. In the event building wall footings encroach from one Tract onto the other Tract, despite efforts to avoid that occurrence, the party onto whose Tract the footings encroach shall cooperate in granting an encroachment permit or easement to the party whose building wall footings encroach.

5. Common Areas.

5.1 Grant of Easements. Each party, as grantor, hereby grants to the other party, as grantee, and to the agents, customers, invitees, licensees, tenants and

employees of grantee, a nonexclusive easement over, through and around the Wal-Mart Tract, the Developer Tract and the NCR Tract for roadways, walkways, ingress and egress, parking of motor vehicles, loading and unloading of commercial and other vehicles, and the use of facilities installed for the comfort and convenience of customers, invitees, licensees, tenants and employees of all businesses and occupants of the buildings constructed on the Building Areas located on the Wal-Mart Tract, the Developer Tract and the NCR Tract; provided, however, in no event shall the owner, occupant, licensee or invitee of any Tract be permitted to use the other Tract for vehicular parking or for any other purpose other than as described above. The easement rights granted hereunder include, without limitation, (1) a perpetual easement for vehicular ingress and egress between the Developer Tract and SE Ensign Lane for the benefit of the Developer Tract over that portion of the Wal-Mart Tract shown in cross-hatching on Exhibit A-2 and identified as "Access Easement; and (2) a perpetual easement for vehicular ingress and egress between the Developer Tract and SE Ensign Lane for the benefit of the Developer Tract over the drive aisles established and maintained over the Common Areas of the Wal-Mart Tract from time to time to the access point to SE Ensign Lane at the northeast corner of the Wal-Mart Tract as shown on Exhibit A-2.

5.2 Limitations on Use.

(1) Customers. Each party shall use reasonable efforts to ensure that customers and invitees shall not be permitted to park on the Common Areas except while shopping or transacting business in the Shopping Center.

(2) Employees. Each party shall use reasonable efforts to ensure that employees park on the Common Areas of said party's Tract.

(3) General. Any activity within the Common Areas other than its primary purpose of the Common Areas, which is to provide for parking for the customers, invitees and employees of those businesses conducted within the Building Areas and for the servicing and supplying of such businesses, shall be permitted so long as such activity shall not unreasonably interfere with such primary purpose. The use by Wal-Mart of the Common Areas on the Wal-Mart Tract for the display, sale and storage of merchandise and for the use of seasonal sales structures is expressly permitted. Persons using the Common Areas in accordance with this Agreement shall not be charged any fee for such use. Notwithstanding the foregoing, each party covenants and agrees that, to the extent allowed by law, neither party will allow the Common Areas on its tract to be used for rallies, demonstrations, protests, picketing or handbilling to protest, publicize or allege improprieties regarding the acts, policies or operating practices of any business operating within the Shopping Center.

5.3 Utility and Service Easements. Each party hereby establishes and grants to the other party a nonexclusive easement for the benefit of the owner of each Tract, on, across and under the Common Areas, to install, use, maintain and repair public utility services and distribution systems (including storm drains, sewers, utilities and other proper services necessary for the orderly development and operation of the Shopping Center, now upon or hereafter installed on, across or under the Common Areas, to the extent necessary to service such Tract). All parties shall use their best efforts to cause the installation of such utility and service lines prior to paving of the Common Areas. No such lines, sewers, utilities or

services of one party shall be installed within the Building Areas on another party's parcel. The location of any utilities hereafter installed shall be determined by the owner of the Tract (the location of utilities on the Wal-Mart Tract shall be determined by Wal-Mart as long as it is the owner or lessee of the Wal-Mart Tract) upon which such utilities are to be installed. Any such installed utility services may be relocated by the owner of a Tract on such owner's Tract, subject to compliance with applicable laws, at the expense of the owner of that Tract, provided that such relocation shall not interfere with, increase the cost of, or diminish utility services to any other Tract and, further provided, that no utilities shall be relocated on the Wal-Mart Tract without the prior written consent of Wal-Mart as long as it is the owner of or lessee of the Wal-Mart Tract and no utilities shall be relocated on the Developer Tract without the prior written consent of Developer so long as Developer is the owner or lessee of the Developer Tract. No utilities shall be relocated on the NCR Tract without the prior written consent of NCR so long as NCR is the owner or lessee of the NCR Tract.

5.4 Water Flow. Each party hereby agrees that it shall contain all storm water flow on its own Tract, or to another offsite ditch or drainage area. No party shall allow storm water from its Tract to flow onto any other Tract. Notwithstanding the foregoing, Wal-Mart may drain its storm water flow onto other property owned by Developer (i.e., Lot 3 as shown on Exhibit A-1).

5.5 Illumination. Each of the parties hereby covenants and agrees to keep the Common Area located on its Tract fully illuminated each day from dusk until midnight, and further agrees to keep any exterior building security lights on from dusk until dawn (or such other times as Wal-Mart, Developer and NCR may mutually agree). The requirements of this Paragraph 5.5 shall apply to each Tract regardless of the type of use being made of such Tract. Each party may require the other party to illuminate its Tract beyond the required hours set forth above provided that the requesting party agrees to reimburse the other party for such Party's actual out-of-pocket utility costs for lighting beyond the hours required above. Such costs shall be reimbursed once each calendar quarter within 30 days after presentation of paid invoices to the party responsible to reimburse such costs evidencing costs incurred by the party requesting reimbursement along with calculations showing how utility costs were allocated to the hours for which reimbursement is due.

6. Development, Parking Ratios, Maintenance, and Taxes.

6.1 Development. The arrangement of the Common Areas shall not be changed in a manner inconsistent with the provisions of this Agreement.

6.2 Wal-Mart Tract "Parking Ratio." Wal-Mart agrees that at all times there shall be independently maintained on the Wal-Mart Tract parking area sufficient to accommodate not fewer than 4.0 car spaces for each 1,000 square feet of building or buildings on such Tract

6.3 Developer Tract and NCR Tract "Parking Ratio." Developer and NCR agree that at all times there shall be independently maintained on the Developer Tract and the NCR Tract parking area sufficient to accommodate not fewer than: (i) 15 spaces for every 1,000 square feet of building space for any restaurant or entertainment use in excess of 5,000

square feet, (the same ratio shall be provided for a McDonald's Restaurant, notwithstanding a building footprint of less than 5,000 square feet); or (ii) 10 spaces for every 1,000 square feet of building space for any restaurant or entertainment use less than 5,000 square feet (subject to the exception above); or (iii) 4.0 spaces per 1,000 square feet of building space for any other use.

6.4 Maintenance.

(1) Standards. The Tracts shall be kept neat, orderly, planted in grass and trimmed until improved and constructed. Each party shall be responsible for maintaining its own Tract. Following completion of the improvements on the Common Areas, the parties hereto shall maintain the Common Areas in good condition and repair. The maintenance is to include, without limitation, the following:

(a) Maintaining the surfaces in a level, smooth and evenly-covered condition with the type of surfacing material originally installed or such substitute as shall in all respects be equal in quality, use, and durability;

(b) Removing all papers, ice and snow, mud and sand, debris, filth and refuse and thoroughly sweeping the area to the extent reasonably necessary to keep the area in a clean and orderly condition;

(c) Placing, keeping in repair and replacing any necessary appropriate directional signs, markers and lines;

(d) Operating, keeping in repair and replacing, where necessary, such artificial lighting facilities as shall be reasonably required;

(e) Maintaining all perimeter and exterior building walls including but not limited to all retaining walls in a good condition and state of repair;

(f) Maintaining, mowing, weeding, trimming and watering all landscaped areas and making such replacements of shrubs and other landscaping as is necessary; and

(2) Expenses. The respective owners shall pay the maintenance expense of their Tracts.

(3) By Agent. Subject to the mutual agreement of the parties hereto, a third party may be appointed as an agent of the parties to maintain the Common Areas in the manner as above outlined. Said third party may receive for such agency a fee that is mutually acceptable to all parties to cover supervision, management, accounting and similar fees, which sums are to be included in the general maintenance expense paid by the respective owners of the Common Areas.

6.5 Taxes. Each of the parties hereto agrees to pay or cause to be paid, prior to delinquency, directly to the appropriate taxing authorities all real property taxes and assessments which are levied against that part of the Common Areas owned by it.

7. Signs. No rooftop sign shall be erected on the building constructed on the Developer Tract or the NCR Tract. Developer and NCR may erect one freestanding identification sign on the Developer Tract and the NCR Tract, respectively, that complies with City codes. In no event shall such freestanding identification sign block the visibility of the Wal-Mart Store or any of Wal-Mart's signage.

8. Indemnification/Insurance.

8.1 Indemnification. Each party hereby indemnifies and saves the other party harmless from any and all liability, damage, expense, causes of action, suits, claims, or judgments arising from personal injury, death, or property damage and occurring on or from its own Tract, except if caused by the act or negligence of the other party hereto.

8.2 Insurance.

(1) Each owner of any portion of the Shopping Center shall procure and maintain in full force and effect throughout the term of this Agreement general public liability insurance and property damage insurance against claims for personal injury, death or property damage occurring upon, in or about its property, each party's insurance to afford protection to the limit of not less than \$5,000,000.00 for injury or death of a single person, and to the limit of not less than \$5,000,000.00 for any one occurrence, and to the limit of not less than \$5,000,000.00 for property damage. Each party shall provide the other party with certificates of such insurance from time to time upon written request to evidence that such insurance is in force. Such insurance may be written by additional premises endorsement on any master policy of insurance carried by the party which may cover other property in addition to the property covered by this Agreement. Such insurance shall provide that the same may not be canceled without 10 days prior written notice to Wal-Mart, Developer and NCR.

(2) At all times during the term of this Agreement, each party shall keep improvements on its property insured against loss or damage by fire and other perils and events as may be insured against under the broad form of Uniform Extended Coverage Clause in effect from time to time in the state in which the parties' respective properties are located, with such insurance to be for the full replacement value of the insured improvements. The owner of a Tract shall pay for any increase in the cost of insuring the improvements on the other Tract if such increase is due to the use by such owner or its tenant(s).

(3) Policies of insurance provided for in this Section 8 shall name Wal-Mart, Developer and NCR as insureds.

(4) Each owner of any portion of the Shopping Center for itself and its property insurer hereby releases the other owners of portions of the Shopping Center from and against any and all claims, demands, liabilities or obligations whatsoever for damage to property or loss of rents or profits resulting from or in any way connected with any fire or other

casualty whether or not such fire or other casualty shall have been caused by the negligence or the contributory negligence of the party being released or by any agent, associate or employee of the party being released, this release being to the extent that such damage or loss is covered by the property insurance which the releasing party is obligated hereunder to carry, or, if the releasing party is not carrying that insurance, then to the extent such damage or loss would be covered if the releasing party were carrying that insurance.

(5) Notwithstanding anything to the contrary contained in this Section 8, so long as the net worth of Wal-Mart shall exceed \$100,000,000.00, and so long as Wal-Mart is owner or lessee of the Wal-Mart Tract, Wal-Mart shall have the right to retain (in whole or in part) the financial risk for any claim.

9. Eminent Domain.

9.1 Owner's Right To Award. Nothing herein shall be construed to give any party any interest in any award or payment made to any other party in connection with any exercise of eminent domain or transfer in lieu thereof affecting said other party's Tract giving the public or any government any rights in said Tract. In the event of any exercise of eminent domain or transfer in lieu thereof of any part of the Common Areas located in the Shopping Center, the award attributable to the land and improvements of such portion of the Common Areas shall be payable only to the owner thereof, and no claim thereon shall be made by the owners of any other portion of the Common Areas.

9.2 Collateral Claims. All other owners of the Common Areas may file collateral claims with the condemning authority for their losses which are separate and apart from the value of the land area and improvements taken from another owner.

9.3 Tenant's Claim. Nothing in this Section 9 shall prevent a tenant from making a claim against an owner pursuant to the provisions of any lease between tenant and owner for all or a portion of any such award or payment.

9.4 Restoration Of Common Areas. The owner of any portion of the Common Areas so condemned shall promptly repair and restore the remaining portion of the Common Areas within its respective Tract as nearly as practicable to the condition of the same immediately prior to such condemnation or transfer, to the extent that the proceeds of such award are sufficient to pay the cost of such restoration and repair and without contribution from any other owner.

10. Rights And Obligations Of Lenders. Any holder of a first lien on any portion of the Shopping Center, and any assignee or successor in interest of such first lienholder, shall be subject to the terms and conditions of this Agreement.

11. Release from Liability. Any person acquiring fee or leasehold title to any portion of the Shopping Center shall be bound by this Agreement only as to the Tract, or portion of the Tract acquired by such person. In addition, such person shall be bound by this Agreement only during the period such person is the fee or leasehold owner of such Tract, or portion of the Shopping Center, except as to obligations, liabilities or responsibilities that accrue

during said period. Although persons may be released under this Section, the easements, covenants and restrictions in this Agreement shall continue to be benefits to and servitudes upon said tracts running with the land.

12. Breach. In the event of breach or threatened breach of this Agreement, only all of the record owners of the Wal-Mart Tract as a group, all record owners of the Developer Tract as a group, or all record owners of the NCR Tract as a group, or Wal-Mart so long as it or any affiliate has an interest as owner or lessee of the Wal-Mart Tract, Developer so long as it or any affiliate has an interest as owner or lessee of the Developer Tract, or NCR so long as it or any affiliate has an interest as owner or lessee of the NCR Tract, shall be entitled to institute proceedings for full and adequate relief from the consequences of said breach or threatened breach.

13. Rights of Successors. The easements, restrictions, benefits and obligations hereunder shall create mutual benefits and servitudes running with the land. This Agreement shall bind and inure to the benefit of the parties hereto, their respective heirs, representatives, lessees, successors and assigns. The singular number includes the plural and the masculine gender includes the feminine and neuter.

14. Document Execution, Modification and Cancellation. It is understood and agreed that until this document is fully executed by Developer, NCR and Wal-Mart there is not and shall not be an agreement of any kind between the parties hereto upon which any commitment, undertaking or obligation can be founded. This Agreement (including exhibits) may be modified or canceled only by the mutual agreement of (a) Wal-Mart as long as it or its affiliate has any interest as either owner or lessee of the Wal-Mart Tract, or its successors in interest, (b) Developer, as long as it or its affiliate has any interest as either owner or lessor of the Developer Tract, or its successors in interest, and (c) NCR, as long as it or its affiliate has any interest as either owner or lessor of the NCR Tract, or its successors in interest.

15. Non-Merger. So long as Wal-Mart or its affiliate is owner or lessee of the Wal-Mart Tract, this Agreement shall not be subject to the doctrine of merger.

16. Duration. Unless otherwise canceled or terminated, all of the easements granted in this Agreement shall continue in perpetuity and all other rights and obligations hereof shall automatically terminate and be of no further force and effect after 99 years from the date hereof.

17. Headings. The headings herein are inserted only as a matter of convenience and for reference and in no way define, limit or describe the scope or intent of this document nor in any way affect the terms and provisions hereof.

18. Entire Agreement. This Agreement constitutes the entire agreement between the parties hereto. The parties do not rely upon any statement, promise or representation not herein expressed, and this Agreement once executed and delivered shall not be modified or altered in any respect except by a writing executed and delivered in the same manner as required by this document.

19. Transfer of Interests; Notices.

19.1 Transfer of Interests. In the event that any person or entity (the "Acquiring Party") shall acquire a fee or mortgage interest in any portion of the Shopping Center subject to this Agreement, or any portion thereof, the Acquiring Party shall execute and file in the land records of Clatsop County, Oregon, a statement setting forth the name of the Acquiring Party, the address of the Acquiring Party to which all notices for the purposes of this Agreement may be sent, the nature of the interest held by the Acquiring Party, and the date that such interest was acquired (the "Notice Statement"). Contemporaneously with such filing, the Acquiring Party shall also send by certified mail, return receipt requested, a copy of such Notice Statement to all other persons or entities then holding fee or mortgage interests in any tract subject to this Agreement, or any portion thereof, as reflected by the Notice Statements then of record in the land records of Clatsop County, Oregon (the "Existing Interest Holders"). Until such time as an Acquiring Party files and mails such Notice Statement in accordance with the terms of this Section 19.1, it shall not be entitled to receive any notice required or permitted to be given under this Agreement, and the Existing Interest Holders shall have no obligation to give any such notice to the Acquiring Party. Any change of address shall require the filing and mailing of a new Notice Statement. It is understood and agreed that the provisions of this Section 19.1 regarding the recordation of the Notice Statement are satisfied with respect to Developer and Wal-Mart.

19.2 Notices. All notices and other communications required or permitted to be given hereunder shall be in writing and shall be mailed by certified or registered mail, postage prepaid, or by Federal Express, Airborne Express, or similar overnight delivery service, addressed as follows:

Wal-Mart: Wal-Mart Stores, Inc. (Store #5861-01)
2001 S.E. 10th Street
Bentonville, AR 72716
Attention: President

With a copy to:
Wal-Mart Stores, Inc. (Store #5861-01)
Attention: Property Management, State of Oregon
2001 S.E. 10th Street
Bentonville, AR 72716-0550

Developer: Peaks View, LLC
915 South 12th Avenue
Cornelius, OR 97113
Attn: Ken Leahy

With a copy to:
Alston, Courtnage & Bassetti
1420 Fifth Avenue, Suite 3650
Seattle, WA 98101-4011

2240158.6 10/22/2014
Warrenton, OR Store # 5861-01
ECR

Attention: Parker Mason and Chuck Shigley

NCR:

North Coast Retail, LLC
Attention: Wes Giesbrecht
4259 East Mercer Way, Suite A
Mercer Island, WA 98040

With a copy to:
Alston, Courtnage & Bassetti
1420 Fifth Avenue, Suite 3650
Seattle, WA 98101-4011
Attention: Parker Mason

Notices shall be effective upon receipt or refusal. The telephone numbers listed above are for purposes of providing the same to overnight delivery services and are not to be otherwise used for notice purposes. In the event that any person acquires a fee interest in the Shopping Center said person shall be entitled to provide a request for notice to the addressees listed above, which request, in order to be effective, must also be recorded in the county recorder's office in the county in which the Shopping Center is located. Any party shall be entitled to change its address for notice by providing notice of such change and recording a copy of the notice of such change in the county recorder's office in the county in which the Shopping Center is located. Until such time as the notice of change is effective pursuant to the terms of this Section 19 and until such time as it is recorded as required above, the last address of said party shall be deemed to be the proper address of said party.

20. Consent. The owner of the Wal-Mart Tract agrees that for so long as a lease of all or a portion of the Wal-Mart Tract is in effect, whenever the consent of the owner of the Wal-Mart Tract is required under the Agreement, the owner of the Wal-Mart Tract will give such consent only after obtaining Wal-Mart's consent.

21. Obligations of the Owner of the Wal-Mart Tract. Wal-Mart hereby agrees that so long as a lease of all or a portion of the Wal-Mart Tract is in effect, it will satisfy the obligations of the owner of the Wal-Mart Tract hereunder, and will hold harmless and indemnify the owner of the Wal-Mart Tract from any and all loss, damage, expense, fees, claims, costs, and liabilities, including, but not limited to, attorneys' fees and costs of litigation, arising out of this Agreement, except for those arising out of the acts or omissions of the owner of the Wal-Mart Tract or its employees, agents, contractors or invitees.

22. Counterparts. This Agreement may be executed in one or more counterparts each of which in the aggregate shall constitute one and the same instrument.

[SIGNATURE PAGES FOLLOW]

2240158.6 10/22/2014
Warrenton, OR Store # 5861-01
ECR

IN WITNESS WHEREOF, the parties have executed this Agreement the day and year first written above.

WAL-MART REAL ESTATE BUSINESS TRUST, a Delaware statutory trust

By Mary Rottler
Mary Rottler
Its Vice President of Real Estate

"Wal-Mart"

State of Arkansas

County of Benton

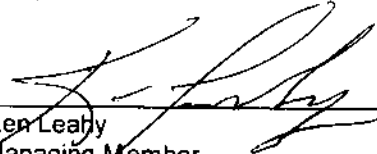
The foregoing instrument was acknowledged before me this 123 day of October, 2014, by Mary Rottler, a Vice President of Real Estate of Wal-Mart Real Estate Business Trust, a Delaware statutory trust, on behalf of the trust.

(Seal and Expiration Date)

REBECCA LEIGH SCHWARZBACH
BENTON COUNTY
NOTARY PUBLIC -- ARKANSAS
My Commission Expires Aug. 28, 2023
Commission No. 12396020

[Signature]
Notary Public

PEAKS VIEW, LLC, an Oregon limited liability company

By 
Ken Leahy
Its: Managing Member

Date _____

"Developer"

State of _____

County of _____

The foregoing instrument was acknowledged before me this _____ day of _____, 2014, by Ken Leahy, the Managing Member of Peaks View, LLC an Oregon limited liability company, on behalf of the company.

(Seal and Expiration Date)

See next page

Notary Public

COMMONWEALTH OF MASSACHUSETTS

Suffolk

, ss

On this 24th day of October, 2014, before me, the undersigned notary public, personally appeared [Ken Leahy], as managing member of Pears View LLC, proved to me through satisfactory evidence of identification, which was OR Driver's license, to be the person whose name is signed on the preceding or attached document, and acknowledged to me that he signed it voluntarily for its stated purpose.



[official signature and seal of notary]

My commission expires 11/5/15



NORTH COAST RETAIL, LLC, a Washington limited liability company

By [Signature]
Wes Giesbrecht
Its: Manager

Date 10-24-14

"NCR"

State of Washington)
County of King) ss.

The foregoing instrument was acknowledged before me this 24th day of October 2014, by Wes Giesbrecht, the Manager of North Coast Retail, LLC a Washington limited liability company, on behalf of the company.

(Seal and Expiration Date)

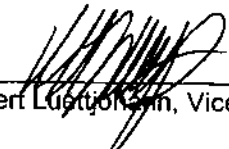
LAUREL BARTON
STATE OF WASHINGTON
NOTARY PUBLIC
MY COMMISSION EXPIRES
12-18-17

[Signature]
Notary Public

BENEFICIARY'S CONSENT

The undersigned beneficiary hereby consent to the placement of the easements, restrictions, and covenants contained in the foregoing instrument on the parcels of land described therein and further agree that the same shall not be terminated on any foreclosure on any parcel of land covered by the said instrument.

PREMIER COMMUNITY BANK, a state chartered community bank f/k/a **COLUMBIA COMMUNITY BANK**, a state chartered community bank

By: 
Robert Luettjohann, Vice President

State of Oregon
County of Washington

The foregoing instrument was acknowledged before me this 27th day of October, 2014, by Robert Luettjohann, a Vice President of Premier Community Bank, a state chartered bank, f/k/a Columbia Community Bank, a state chartered bank, on behalf of the bank.

(Seal and Expiration Date)




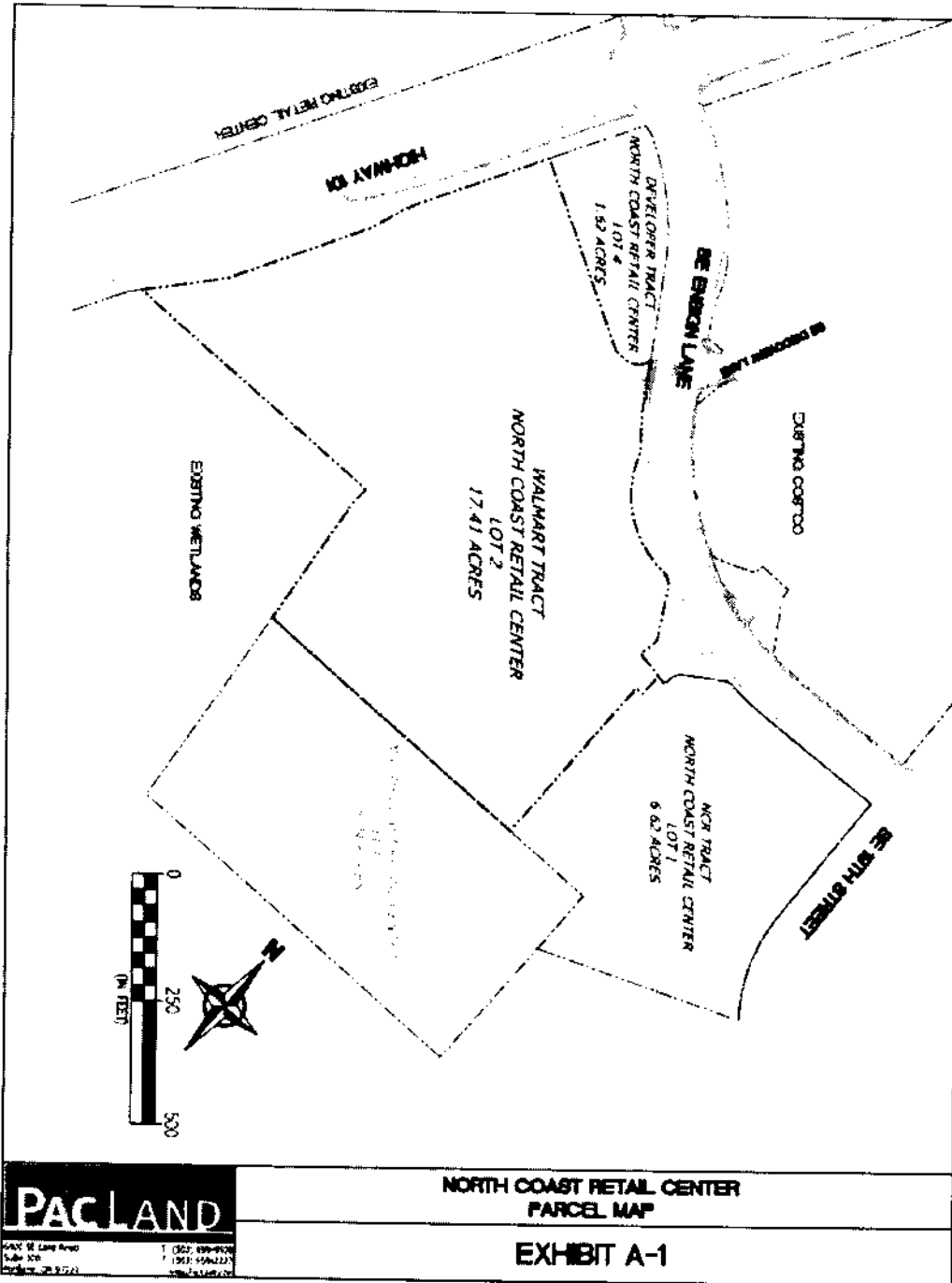

Notary Public

EXHIBIT A-1 TO ECR

Plot Plan

2240158.6 10/22/2014
Warrenton, OR Store # 5861-01
ECR

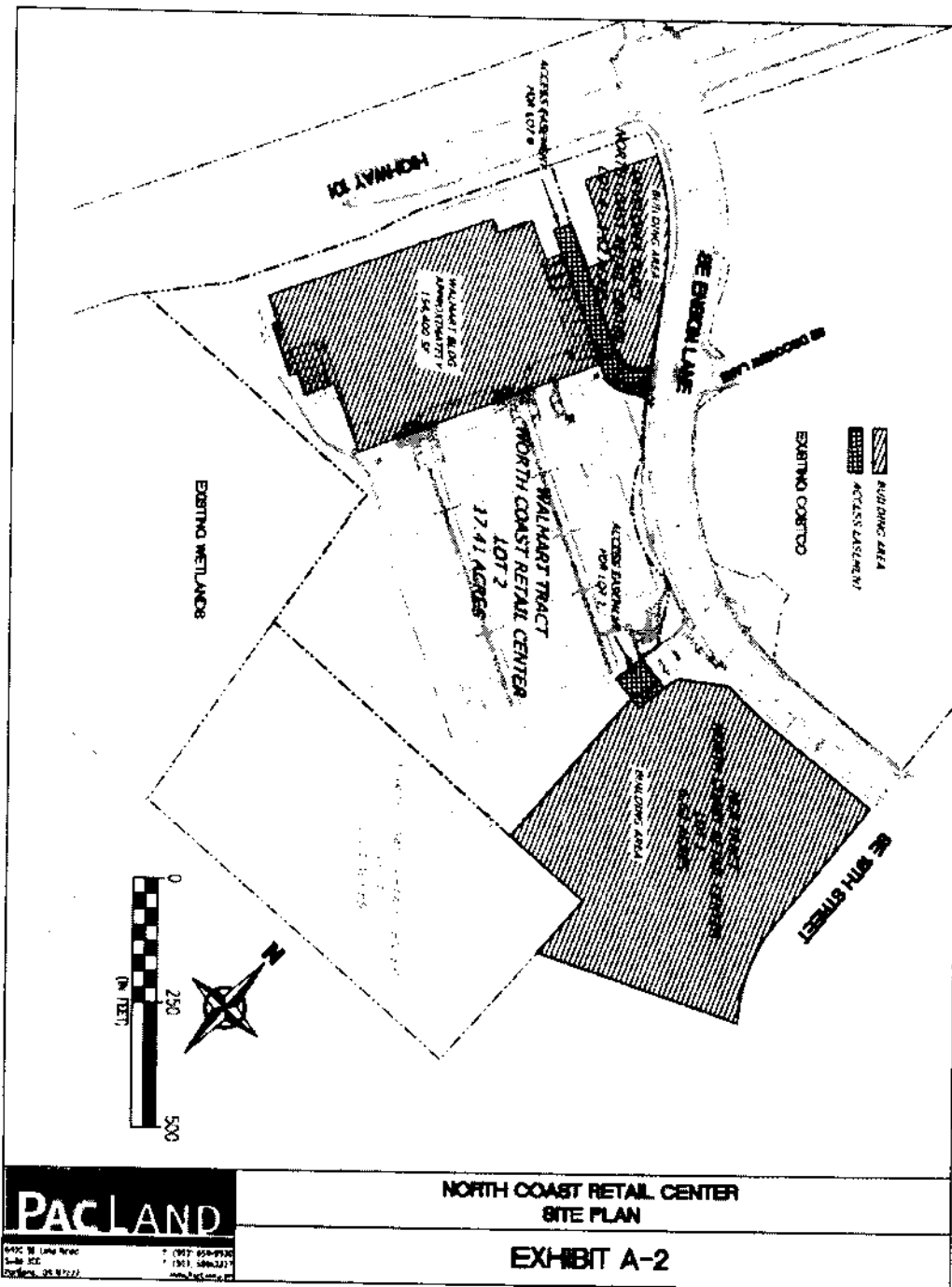


Warrenton, OR Store # 5861-01
 ECR

EXHIBIT A-2 TO ECR

Site Plan Showing Development Details

2240158.6 10/22/2014
Warrenton, OR Store # 5861-01
ECR



2290158.6 10/22/2014
 Warrenton, OR Store # 5861-01
 ECR

EXHIBIT B TO ECR

Legal Description of the Wal-Mart Tract

EXHIBIT B

A TRACT OF LAND BEING A PORTION OF LOT 2 AND LOT 4 AS SHOWN ON THE PLAT OF "NORTH COAST RETAIL", A DULY RECORDED PLAT IN THE RECORDS OF CLATSOP COUNTY, SAID TRACT BEING LOCATED IN THE SOUTHWEST ONE-QUARTER OF SECTION 27, TOWNSHIP 8 NORTH, RANGE 10 WEST OF THE WILLAMETTE MERIDIAN, CITY OF WARRENTON, CLATSOP COUNTY, OREGON AND BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

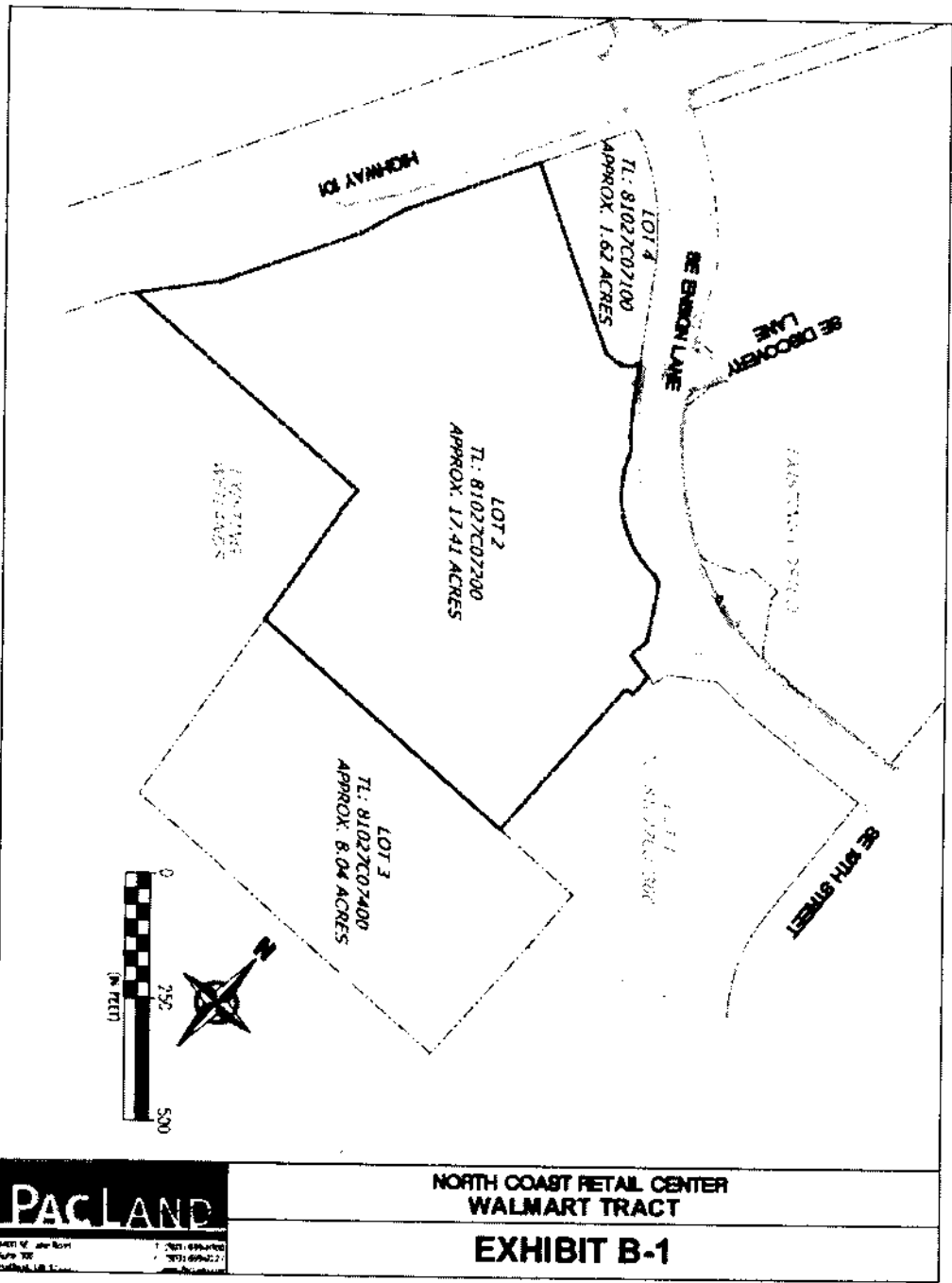
BEGINNING AT THE SOUTHWEST CORNER OF SAID LOT 4 BEING ON THE EAST RIGHT OF WAY LINE OF THE OREGON COAST HIGHWAY NUMBER 101, 86.01 FEET FROM THE CENTERLINE THEREOF; THENCE NORTH 40°27'04" EAST, 173.30 FEET TO AN ANGLE POINT ON SAID EAST LINE BEING 120.00 FEET FROM THE CENTERLINE THEREOF; THENCE NORTH 29°08'29" EAST, 300.00 FEET TO AN ANGLE POINT ON SAID EAST LINE BEING 120.00 FEET FROM THE CENTERLINE THEREOF; THENCE NORTH 20°36'38" EAST, 101.12 FEET TO AN ANGLE POINT ON SAID EAST LINE BEING 105.00 FEET FROM THE CENTERLINE THEREOF; THENCE NORTH 29°08'29" EAST, 290.89 FEET TO A POINT ON SAID EAST LINE BEING 105.00 FEET FROM THE CENTERLINE THEREOF; THENCE LEAVING SAID EAST LINE SOUTH 60°51'31" EAST, 405.52 FEET; THENCE SOUTH 89°30'37" EAST, 23.50 FEET TO THE POINT OF CURVATURE; THENCE 55.87 FEET ALONG THE ARC OF A 43.00 FOOT RADIUS CURVE THROUGH A CENTRAL ANGLE OF 74°26'41" (THE CHORD OF WHICH BEARS NORTH 53°15'16" EAST, 52.02 FEET) TO A POINT ON THE WEST RIGHT OF WAY LINE OF SOUTHEAST ENSIGN LANE, SAID POINT BEING 56.50 FEET FROM THE CENTERLINE THEREOF; THENCE ALONG SAID WEST RIGHT OF WAY LINE SOUTH 32°43'41" EAST, 74.17 FEET TO THE POINT OF CURVATURE; THENCE 20.58 FEET ALONG THE ARC OF A 706.50 FOOT RADIUS CURVE CONCAVE EAST THROUGH A CENTRAL ANGLE OF 1°40'08" (THE CHORD OF WHICH BEARS SOUTH 33°33'45" EAST, 20.58 FEET) TO THE POINT OF COMPOUND CURVATURE; THENCE 86.64 FEET ALONG THE ARC OF A 538.00 FOOT RADIUS CURVE CONCAVE EAST THROUGH A CENTRAL ANGLE OF 9°13'37" (THE CHORD OF WHICH BEARS SOUTH 39°00'38" EAST, 86.55 FEET) TO THE POINT OF CURVATURE OF A NON TANGENT 218.00 FOOT RADIUS CURVE, THE CENTER OF WHICH BEARS NORTH 72°57'10" EAST; THENCE 266.92 FEET ALONG THE ARC OF SAID CURVE THROUGH A CENTRAL ANGLE OF 70°09'09" (THE CHORD OF WHICH BEARS SOUTH 52°07'25" EAST, 250.56 FEET) TO THE POINT OF REVERSE CURVATURE; THENCE 8.58 FEET ALONG THE

2240158.6 10/22/2014
Warrenton, OR Store # 5861-01
ECR

ARC OF A 20.00 FOOT RADIUS CURVE CONCAVE SOUTH THROUGH A CENTRAL ANGLE OF 24°35'14" (THE CHORD OF WHICH BEARS SOUTH 74°54'24" EAST, 8.52 FEET) TO THE POINT OF REVERSE CURVATURE; THENCE 9.24 FEET ALONG THE ARC OF A 701.00 FOOT RADIUS CURVE CONCAVE NORTH THROUGH A CENTRAL ANGLE OF 0°45'18" (THE CHORD OF WHICH BEARS SOUTH 62°59'25" EAST, 9.24 FEET) TO THE POINT OF REVERSE CURVATURE; THENCE 11.88 FEET ALONG THE ARC OF A 20.00 FOOT RADIUS CURVE CONCAVE WEST THROUGH A CENTRAL ANGLE OF 34°01'38" (THE CHORD OF WHICH BEARS SOUTH 46°21'15" EAST, 11.70 FEET) TO THE POINT OF TANGENCY; THENCE SOUTH 29°20'26" EAST, 103.86 FEET TO THE POINT OF CURVATURE; THENCE 14.25 FEET ALONG THE ARC OF A 20.00 FOOT RADIUS CURVE CONCAVE WEST THROUGH A CENTRAL ANGLE OF 40°49'44" (THE CHORD OF WHICH BEARS SOUTH 08°55'34" EAST, 13.95 FEET) TO THE POINT OF TANGENCY; THENCE SOUTH 11°29'18" WEST 33.43 FEET; THENCE SOUTH 79°57'18" EAST, 57.12 FEET TO THE NORTHEAST CORNER OF SAID LOT 2; THENCE LEAVING SAID WEST RIGHT OF WAY LINE, ALONG THE EAST LINE OF SAID LOT 2, SOUTH, 47.24 FEET; THENCE WEST 17.20 FEET; THENCE SOUTH, 383.71 FEET TO THE NORTH LINE OF LOT 3, SAID PLAT OF "NORTH COAST RETAIL"; THENCE ALONG SAID NORTH LINE AND THE EXTENSION THEREOF, WEST 639.07 FEET TO A POINT ON THE WEST LINE OF SAID LOT 4. THENCE ALONG SAID WEST LINE NORTH 06°02'10" WEST, 264.52 FEET; THENCE NORTH 87°36'25" WEST, 2.13 FEET; THENCE NORTH 0°07'02" EAST, 54.73 FEET; THENCE SOUTH 89°50'07" WEST, 24.91 FEET; THENCE SOUTH 89°55'27" WEST, 575.44 FEET TO THE POINT OF BEGINNING.

CONTAINING 758,637 SQUARE FEET, 17.41 ACRES MORE OR LESS

PREPARED BY CESNW, INC.



2240158.6 10/22/2014
 Warrenton, OR Store # 5861-01
 ECR

EXHIBIT C TO ECR

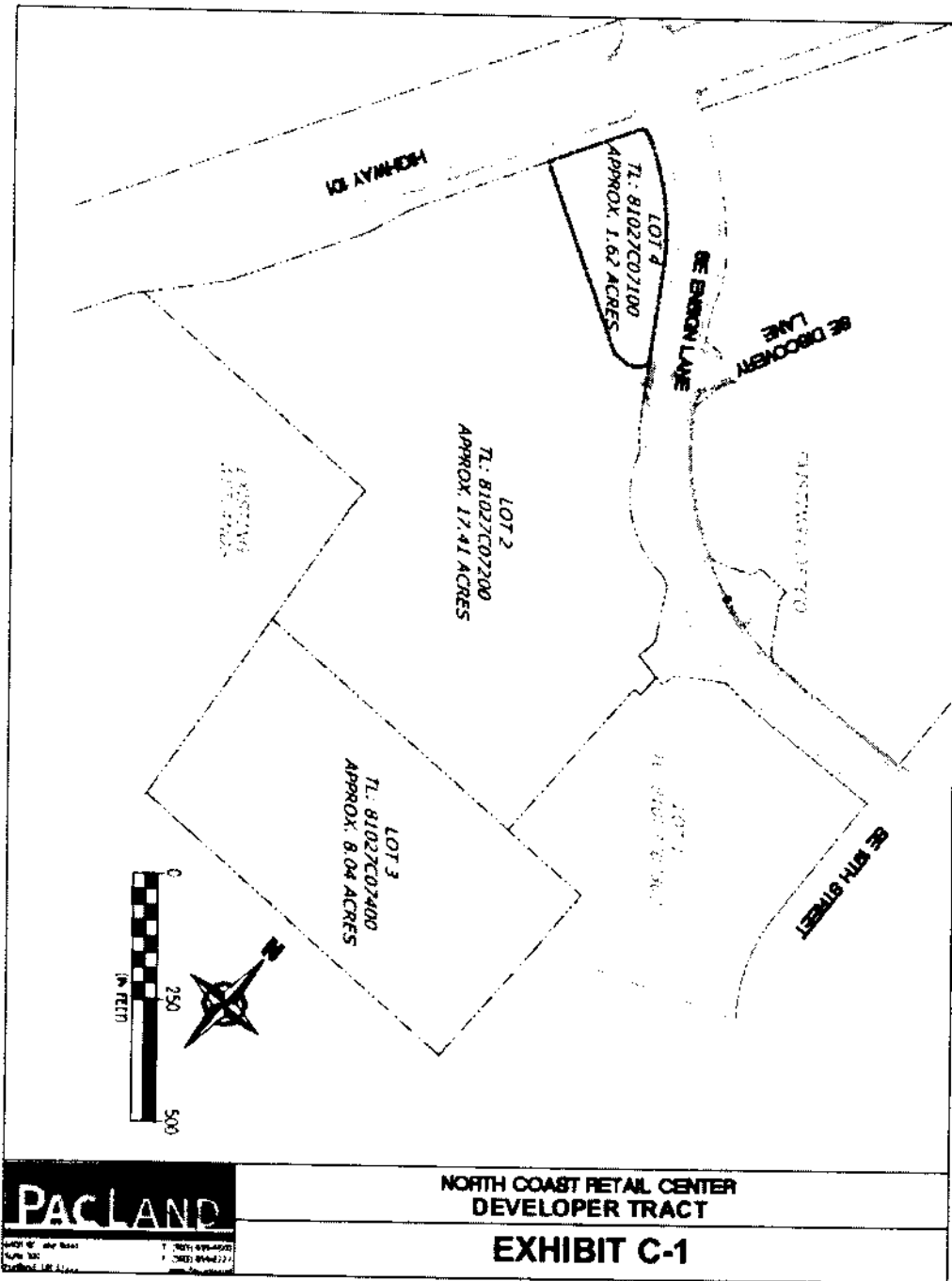
Legal Description of the Developer Tract

EXHIBIT C

A TRACT OF LAND BEING A PORTION OF LOT 4 AS SHOWN ON THE PLAT OF "NORTH COAST RETAIL", A DULY RECORDED PLAT IN THE RECORDS OF CLATSOP COUNTY, SAID TRACT BEING LOCATED IN THE SOUTHWEST ONE-QUARTER OF SECTION 27, TOWNSHIP 8 NORTH, RANGE 10 WEST OF THE WILLAMETTE MERIDIAN, CITY OF WARRENTON, CLATSOP COUNTY, OREGON AND BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

COMMENCING AT THE SOUTHWEST CORNER OF SAID LOT 4 BEING ON THE EAST RIGHT OF WAY LINE OF THE OREGON COAST HIGHWAY NUMBER 101, 86.01 FEET FROM THE CENTERLINE THEREOF; THENCE NORTH 40°27'04" EAST, 173.30 FEET TO AN ANGLE POINT ON SAID EAST LINE BEING 120.00 FEET FROM THE CENTERLINE THEREOF; THENCE NORTH 29°08'29" EAST, 300.00 FEET TO AN ANGLE POINT ON SAID EAST LINE BEING 120.00 FEET FROM THE CENTERLINE THEREOF; THENCE NORTH 20°36'38" EAST, 101.12 FEET TO AN ANGLE POINT ON SAID EAST LINE BEING 105.00 FEET FROM THE CENTERLINE THEREOF; THENCE NORTH 29°08'29" EAST, 290.89 FEET TO THE POINT OF BEGINNING ON SAID EAST LINE BEING 105.00 FEET FROM THE CENTERLINE THEREOF; THENCE CONTINUING ALONG SAID EAST LINE NORTH 29°08'29" EAST, 204.33 FEET TO A POINT ON THE SOUTH RIGHT OF WAY LINE OF SOUTHEAST ENSIGN LANE SAID POINT BEING THE POINT OF CURVATURE OF A NON TANGENT 95.00 FOOT RADIUS CURVE, THE CENTER OF WHICH BEARS SOUTH 08°54'47" WEST; THENCE 34.08 FEET ALONG SAID CURVE CONCAVE SOUTH THROUGH A CENTRAL ANGLE OF 20°33'18" (THE CHORD OF WHICH BEARS SOUTH 70°48'34" EAST, 33.90 FEET) TO THE POINT OF COMPOUNDING CURVATURE; THENCE 27.07 FEET ALONG THE ARC OF A 617.00 FOOT RADIUS CURVE CONCAVE SOUTH THROUGH A CENTRAL ANGLE OF 2°30'51" (THE CHORD OF WHICH BEARS SOUTH 59°16'29" EAST, 27.07 FEET) TO THE POINT OF COMPOUNDING CURVATURE; THENCE 228.20 FEET ALONG THE ARC OF A 517.00 FOOT RADIUS CURVE CONCAVE SOUTH THROUGH A CENTRAL ANGLE OF 25°17'23" (THE CHORD OF WHICH BEARS SOUTH 45°22'22" EAST, 226.35 FEET) TO THE POINT OF TANGENCY; THENCE SOUTH 32°43'41" EAST, 191.43 FEET TO THE POINT OF CURVATURE OF A NON TANGENT 43.00 FOOT RADIUS CURVE CONCAVE WEST; THENCE LEAVING SAID SOUTH RIGHT OF WAY LINE 55.87 FEET ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 74°26'41" (THE CHORD OF WHICH BEARS SOUTH 53°15'16" WEST, 52.02 FEET; THENCE NORTH 89°30'37" WEST, 23.50 FEET; THENCE NORTH 60°51'31" WEST, 405.52 FEET TO THE POINT OF BEGINNING.

CONTAINING 70,613 SQUARE FEET, 1.62 ACRES MORE OR LESS
PREPARED BY CESNW, INC.

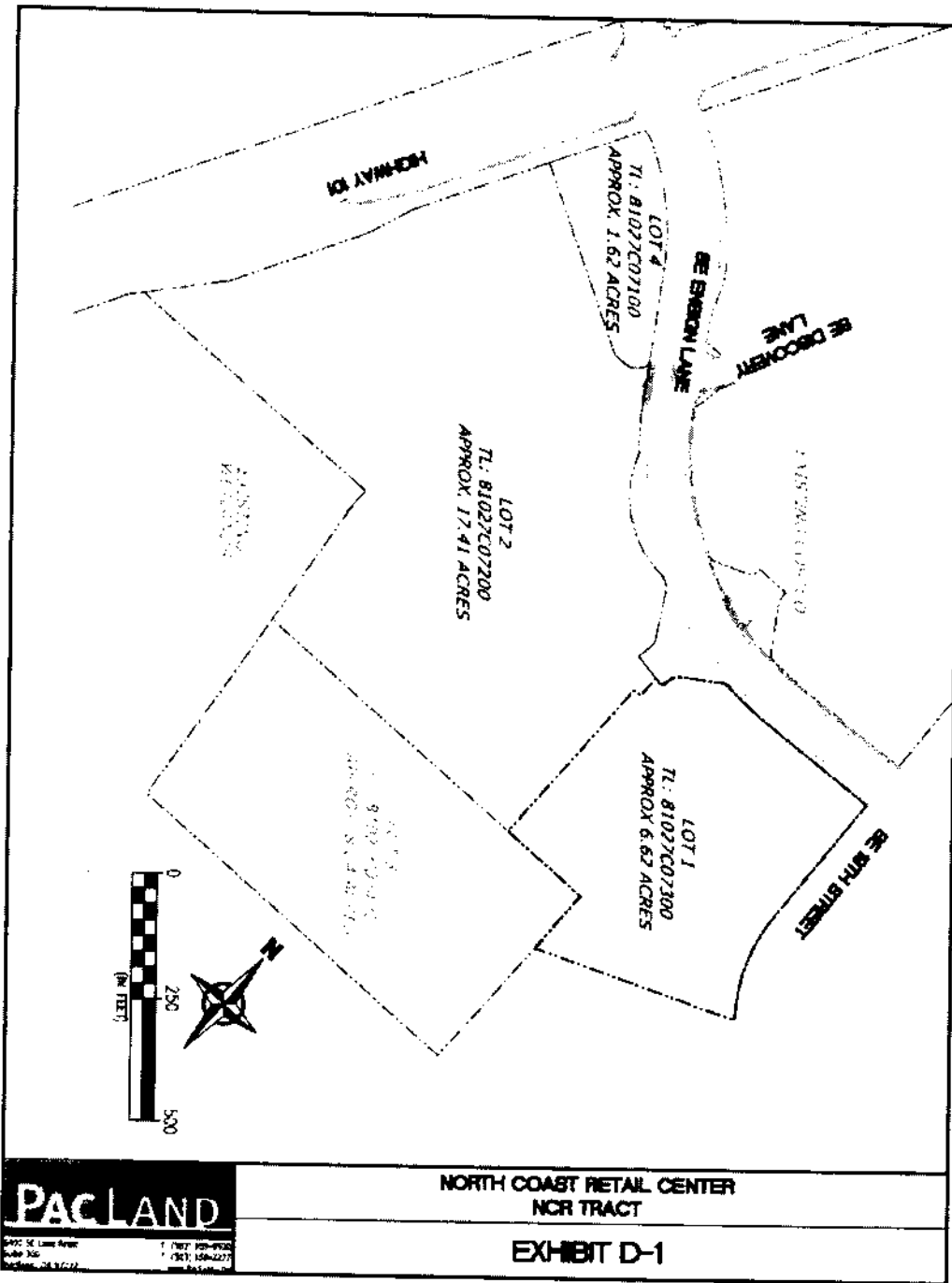


2240158.6 10/22/2014
 Warrenton, OR Store # 5861-01
 ECR

EXHIBIT D TO ECR

Legal Description of NCR Tract

LOT 1 AS SHOWN ON THE PLAT OF "NORTH COAST RETAIL," A DULY RECORDED PLAT IN THE RECORDS OF CLATSOP COUNTY, SAID TRACT BEING LOCATED IN THE SOUTHWEST ONE-QUARTER OF SECTION 27, TOWNSHIP 8 NORTH, RANGE 10 WEST OF THE WILLAMETTE MERIDIAN, CITY OF WARRENTON, CLATSOP COUNTY, OREGON.
CONTAINING 288,204 SQUARE FEET, 6.62 ACRES MORE OR LESS.



Warrenton, OR Store # 5861-01
 ECR

City of Warrenton
Planning and Building Department
PO Box 250
Warrenton, Oregon 97146
503-861-0920

<i>OFFICE USE ONLY</i>	
FILE # _____	FEE \$ <u>1,000</u>
ZONING DISTRICT _____	
RECEIPT # _____	
DATE RECEIVED _____	

Conditional Use Application

I. Property

Address: 1771 SE Ensign Lane, Warrenton, OR 97146 Cross Street SE Discovery Lane

Assessor's Parcel No.: Twp 8N, Rng10W, Section SW 1/4 of 27 Tax Lot 81027CB01901

II. Applicant

Applicant: PM Design (Craig Gilbert) Phone: 360-719-1953

Mailing Address: 19120 SE 34th Street, Suite 115, Vancouver, WA 98683 E-

Mail Address: cgilbert@pmdginc.com

Applicant's Signature: *Craig D Gilbert* Date 9/08/2021

III. Owner

Owner: North Coast Retail LLC (Wes Giesbrecht) Phone: 206-749-9600

Mailing Address: P.O. Box 791, Mercer Island WA 98040

E-mail Address: atlin@qwestoffice.net

Owner's Signature: *Wes Giesbrecht*

IV. Describe the Proposed Use

The proposal is to develop the site for a Popeye's Louisiana Kitchen restaurant with parking, driveways, landscaping and a drive-through. The proposed Popeye's development will provide a 2,111-sf one-story building with a drive-through. The restaurant will provide dine-in and take-out service and will provide 24 indoor seats. The restaurant will operate during the hours of 10:30 a.m. to 11 p.m. Deliveries will occur in the morning or afternoon.

Six Conditional Use Criteria-Provide Written Responses to Each One-Section 16.220

1. The proposed use is in conformance with the Comprehensive Plan.

This project is in conformance with the Comprehensive Plan as the site is zoned C-1 (General Commercial), and as per **16.0.020 A-7**, eating and drinking establishments are an allowed use in a C-1 zone. The drive-thru portion of this project is a Conditionally allowed use in the C-1 zone per **16.40.030 A-6**. All parts of the development are more than 50 ft from the Highway 101 right-of-way line as per **16.40.050 B**. This project complies with all the parking, signage, wetland, and other Development Code requirements as listed in **16.40.060 C-E** in the following manner:

C. Parking: 16.116 and 16.128—six parking spots are required, 17 are provided

D. Signage 16.144-No sign will be over 45 feet in height. As the site has two street frontages, and will comply with the following signage limits: 32 sf cumulative of signage is allowed, and an addition sign not larger than 32 sq ft. is allowed.

E. Wetland and Riparian Protection 16.156—Site is not located in a wetland or riparian corridor..

2. The location, size and design, and operating characteristics of the proposed use are such that the development will be compatible with, and have a minimal impact on surrounding properties.

This project is compatible with the surrounding area in the following ways:

Location: Site is currently unused, vacant land in proximity to an existing Walmart and Costco. This is one of the few undeveloped or under-developed parcels in an otherwise developed commercial area. This project will bring another food choice to the customers already using the above mentioned retailers.

Size and Design: The characteristics of the site are suitable for the proposed use in both size and design. This is an approximately .5 acre lot with a proposed 2,111 ft quick serve restaurant with a drive thru. There are other restaurants on the east side of SE Ensign Lane, but this will be the first food offering on the west side of SE Ensign lane.

Operating Characteristics: The majority of the activity will be contained in the building or in the drive-through lanes. There are no nearby residential developments, so menu board and speaker noise is not expected to be an issue. Restaurant intends to be open 10:30am to 11:00 pm 7 days a week.

Minimal Impact: new project will have minimal impact on the surrounding properties. All parking and drive-through queuing is on-site. The hours of operation are within the hours of operation for the big box stores in the vicinity,

3. The use will not generate excessive traffic, when compared to traffic generated by uses permitted outright, and adjacent streets have the capacity to accommodate the traffic generated.

The additional traffic generated by the project will be negligible. In a traffic study conducted on 11-22-2019, the adjacent streets and intersections have the capacity to accommodate any extra trips. This project is an ancillary use to the large retailers in the area, and much of the traffic will be cross-over from the retailers. The maximum need for queuing for a quick serve restaurant is estimated to be eight cars. The site as designed provides queuing for thirteen cars.

4. Public facilities and services are adequate to accommodate the proposed use.

The site and proposed use will have adequate public facilities and services to accommodate the proposed use. Please see the attached preliminary utility plan.

5. The site's physical characteristics, in term of topography, soils and other pertinent considerations are, are appropriate for the use.

The new parking lot will be paved. The site's topography is appropriate for the use. The site is generally flat with a slight slope to the southwest, and will not require excessive grading or additional disturbance. The underlying soils are sandy and mixed, and are stable to provide an adequate foundation for the restaurant and associated structures, and provide adequate constructability for the project.

6. The site has an adequate area to accommodate the proposed use. The site layout has been designed to provide for the building, parking, landscaping, driveway, on-site circulation, public areas, loading areas, storage facilities, setbacks, buffers, and utilities which are required by City ordinances.

The site has adequate area to accommodate the project. The queuing of the drive-through will all be on-site. The setback of 25' for front yard structures, 35 feet from the line of non-aquatic vegetation, and is at least 50 feet from adjacent resource zones. An adequate number of parking spaces is provided. Landscape islands and planter bays with a minimum of 25 sq ft and min 5 ft in width will be provided.

This application will not be officially accepted until department staff has determined that the application is completely filled out and the site plan requirements have been completed and a copy of the deed.

RECORDED BY TICOR
TITLE INS. CO. 300414010327A

Recording Instrument #201710089
Recorded By: Clatsop County Clerk
of Pages: 3 Fee: 57.00
Transaction date: 12/11/2017 14:54:11
Deputy: Stethem-Norris

AFTER RECORDING RETURN TO:
Peaks View LLC
PO Box 489
Cornellus, OR 97113

EXHIBIT B

DECLARATION OF RECIPROCAL ACCESS
EASEMENT AND MAINTENANCE AGREEMENT

Peaks View LLC, an Oregon limited liability company ("Declarant"), is the owner of Parcels 1 and 2 of Partition Plat 2017-006, a duly recorded plat in Clatsop County, Oregon (the "Partition Plat"). Declarant hereby declares and agrees as follows:

1. Declaration of Easements. Declarant declares and grants a nonexclusive and perpetual access easement over the Easement Area (as defined in Section 2 below) for the following purposes: (i) pedestrian and vehicular ingress and egress to the Benefited Lots (defined below); and (ii) the installation, use, maintenance, repair and replacement of a road system and related improvements for the Benefited Lots.
2. Legal Description; Depiction.
 - a. As used herein, "Easement Area" shall mean and refer to those portions of Parcels 1 and 2 of the Partition Plat located in Clatsop County, Oregon depicted on the Partition Plat and labeled thereon as "26' wide joint access easement."
 - b. The parcels of real property which will benefit from the right to jointly utilize the Easement Area for the purposes described herein (each a "Benefited Lot" and collectively the "Benefited Lots") are Parcels 1 and 2 of the Partition Plat.
3. Duration and Nature of Declaration. This Declaration shall run with the land and be binding upon and inure to the benefit of Declarant's successors and assigns.
4. Maintenance of Road Improvements. As of the date of this Declaration, Declarant has not yet constructed the road improvements in the Easement Area. The owners of Parcel 1 and Parcel 2 will be responsible for constructing road improvements in the Easement Area on each such owner's parcel, but only when and as required by government authorities for development of the owner's parcel. If, however, government authorities require the first owner to develop its lot to construct the entire roadway in conjunction with such development, such owner shall have a construction easement over the other lot for purposes of construction of the initial roadway Improvements. In such event, the owner constructing all the initial roadway improvements shall be reimbursed by the other owner for one-half (1/2) of all design, construction, and permit fees and costs for the initial roadway improvements in the Easement

Area within thirty (30) days after receipt of written demand, together with supporting documentation (e.g., copies of Invoices). If the other owner fails to pay all such costs in full within thirty (30) days after receipt of the written demand, all outstanding amounts shall bear interest at the rate of twelve percent (12%) per annum from the date of demand until paid in full. In the event an owner of a Benefited Lot fails to pay such owner's share of construction costs within thirty (30) days after written demand for reimbursement by the other owner who built the initial roadway improvements, then such reimbursement amount shall become an automatic charge and lien against the non-paying owner's property, which may be foreclosed in the manner provided in Chapter 88 of the Oregon Revised Statutes (or its successor statutes) for the foreclosure of liens generally.

The owners of the Benefited Lots shall each be responsible for the maintenance, repair and replacement of road improvements on the portion of the Easement Area located on such owner's lot (if any). For purposes of this Declaration, maintenance and repair costs shall include the third-party cost of any work required to keep the road improvements in good condition and in compliance as required by any applicable governmental regulations and the terms of this Declaration. If, however, the act or omission of any such owner or any guest, invitee, licensee, contractor, or agent of an owner of a Benefited Lot causes the damage to the road improvements located on another owner's Benefited Lot, such owner shall be responsible for repair of the damage to the road improvements on the other owner's Benefited Lot at his or her sole expense.

5. Insurance. Each Benefited Lot owner shall ensure that all workers undertaking construction or maintenance work on the roadway located on, in or under the Easement Area within such owner's Benefited Lot have standard liability insurance in a reasonable amount from a reputable insurance company which protects each owner of the Benefited Lots.
6. Notices. Any notice, demand, or report required under this Declaration shall be sent to each owner of a Benefited Lot in care of the street address of such owner's Benefited Lot; provided, however, that an owner can change the owner's notification address by written notice to the other owner(s). Any required notice or demand shall be made by hand delivery or certified mail, and shall be deemed received on actual receipt or two (2) days after being sent by certified mail, whichever first occurs.
7. Termination. Except as provided otherwise in Section 3 above, the owners of the Easement Area may not amend, withdraw from or dissolve this Agreement without the written approval of all owners of the Benefited Lots.

[Signature page follows]

IN WITNESS WHEREOF, Declarant has executed this Agreement as of 8TH OF DECEMBER, 2017.

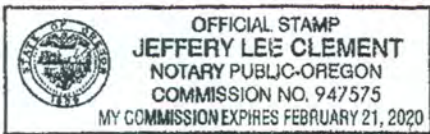
Peaks View LLC, an Oregon limited liability company

By: [Signature]
Its: KENNETH C. LEAHY SR.

STATE OF OREGON)
) ss.
County of WASHINGTON)

The above instrument was subscribed and sworn to before me this 8TH day of DECEMBER, 2017 by KENNETH C. LEAHY SR. as MEMBER of Peaks View LLC, an Oregon limited liability company.

[Signature]
Notary Public - State of OREGON
My commission expires: 2/21/20



Memorandum

To: Charlie Patton
Ambrosia QSR

From: Daniel Stumpf, PE

Date: September 29, 2021

Subject: Louisiana Kitchen Popeyes
Transportation Study Addendum



Introduction

This memorandum serves as an addendum to the *Warrenton Burger King – Transportation Study*, dated November 22, 2019, where City of Warrenton staff have raised concerns regarding to the applicability of this transportation study to a proposed Popeyes restaurant, which will be constructed in place of the prior proposed Burger King restaurant. Of specific concern City staff have requested input with regard to the following:

- Compare the Burger King and Popeyes restaurant projects, specifically comparing building size (i.e. trip generation) and the drive-thru lane (i.e. the prior double lane compared to the proposed single lane).
- Demonstrate there will be little or no impact to the traffic report with the change in restaurant franchise.
- If impacts associated with a nearby credit union were considered in the traffic count data.

Restaurant Comparison

Trip Generation

The transportation study reviewed the impacts associated with a 1,980 square foot Burger King restaurant with a drive-thru window. To estimate the number of trips generated by the Burger King, trip rates from the *Trip Generation Manual*¹ were used; specifically, data from land-use code 934, *Fast-Food Restaurant with a Drive-Through Window*, based on the square-footage of the gross building floor area.

Given the close proximity of the retail land use (i.e. Walmart) adjacent to the project site, some trips generated from the proposed development were assumed to be shared/internally captured by the store and therefore would not impact public area intersections or adjoining roadways. To determine an appropriate morning and evening internal capture rate, internalization was referenced from the *Intersection Traffic Control Study Warrenton Retail* traffic study, which was prepared for the Walmart shopping center on June 14, 2012. Based on

¹ Institute of Transportation Engineers (ITE), *Trip Generation Manual*, 10th Edition, 2017.

this prior study, the following internal capture rates were used for the Burger King restaurant during the morning and evening peak hours:

- Approximately 12 percent of total morning peak hour site trips were assumed to be internally captured; and
- Approximately 6 percent of total evening peak hour site trips were assumed to be internally captured.
- It is assumed the average weekday internal capture rate would approximately match the evening peak hour rate.

Additionally, the Burger King restaurant was expected to attract pass-by trips to the site. Pass-by trips are trips that leave the adjacent roadway to patronize a land use and then continue in their original direction of travel. Therefore, these trips do not add additional vehicles to the surrounding transportation system; however, they do add additional turning movements at site access intersections. To estimate pass-by trip generation, data from land use code 934 of the *Trip Generation Handbook, 3rd Edition*² was referenced. The following rates were utilized for determining pass-by trip generation of the Burger King restaurant during the morning and evening peak hours:

- Approximately 49 percent of morning peak hour site trips were assumed as pass-by trips;
- Approximately 50 percent of evening peak hour site trips were assumed as pass-by trips; and
- It is assumed that the average weekday pass-by trip rate would approximately match the evening peak hour rate.

Similar to the Burger King restaurant, the proposed 2,133 square foot Popeyes restaurant is considered a fast-food restaurant with a drive-thru lane. As such, data from land use code 934 in the *Trip Generation Manual* would be applicable to the proposed development. Additionally, the internal capture and pass-by trip rates would also be applicable to the proposed development. Therefore, the only difference between the prior studied Burger King and the proposed Popeyes restaurant, with respect to trip generation, would be the size of each building.

The trip generation estimates of both the Burger King and the proposed Popeyes restaurants are compared and summarized in Table 1. Detailed trip generation calculations are included as an attachment to this memorandum.

² Institute of Transportation Engineers (ITE), *Trip Generation Handbook, 3rd Edition*, 2014.



Table 1: Restaurant Trip Generation Summary

	ITE Code	Size/Rate	Morning Peak Hour			Evening Peak Hour			Weekday Total
			Enter	Exit	Total	Enter	Exit	Total	
Burger King Restaurant									
Fast-Food Restaurant with Drive-Thru	934	1,980 SF	41	39	80	34	31	65	932
Internalization within Shopping Center		12% (6%)	5	5	10	2	2	4	56
External Trips			36	34	70	32	29	61	876
Pass-by Trips		49% (50%)	17	17	34	15	15	30	438
Net New Primary Trips			19	17	36	17	14	31	438
Popeyes Restaurant									
Fast-Food Restaurant with Drive-Thru	934	2,133 SF	44	42	86	36	34	70	1,004
Internalization within Shopping Center		12% (6%)	5	5	10	2	2	4	60
External Trips			39	37	76	34	32	66	944
Pass-by Trips		49% (50%)	19	19	38	17	17	34	472
Net New Primary Trips			20	18	38	17	15	32	472
Net Change									
Pass-by Trips			2	2	4	2	2	4	34
Net New Primary Trips			1	1	2	0	1	1	34

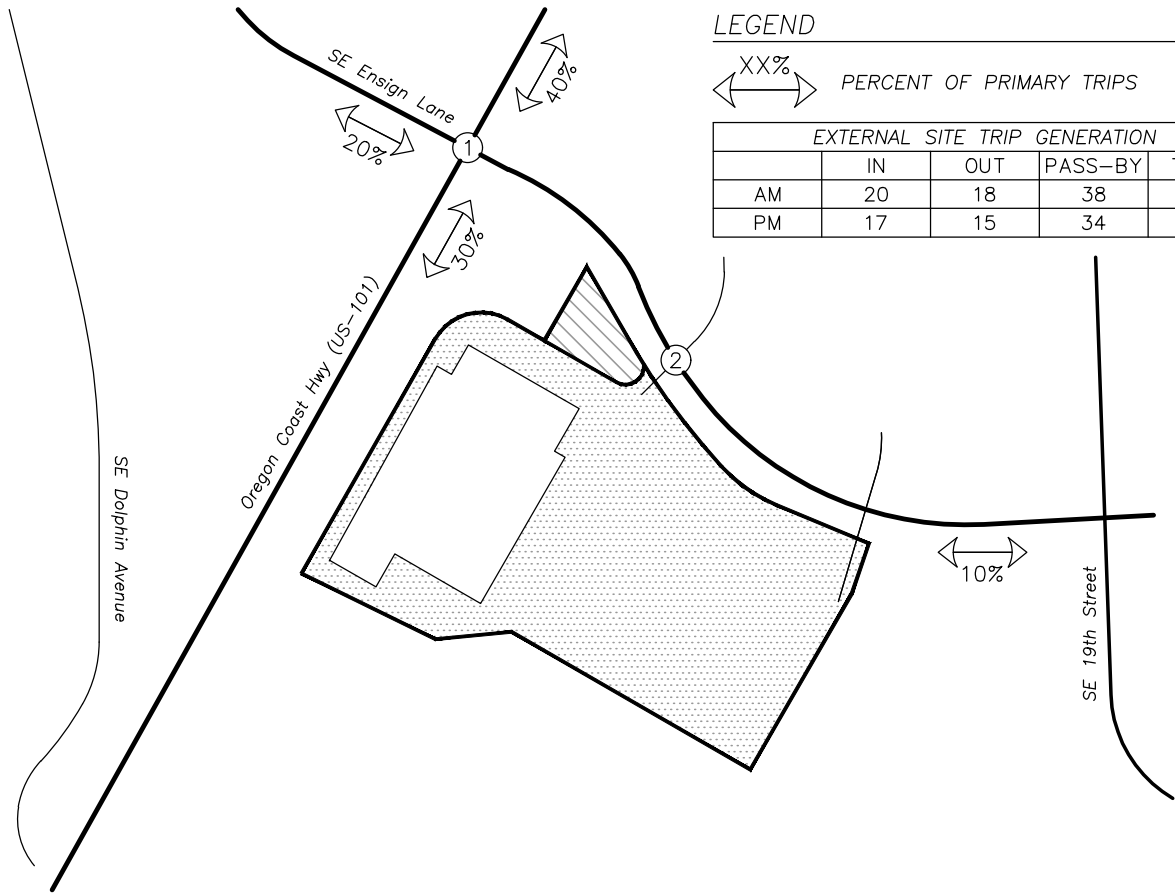
As depicted in Table 1, the proposed Popeyes is projected to generate approximately 2 more net new morning peak hour trips, 1 more net new evening peak hour trip, and 34 more net new daily trips than the prior studied Burger King restaurant. Referencing the *Intersection Capacity* section of the transportation study, the intersections of US-101 at SE Ensign Lane and the right-in/right-out (RIRO) shopping center access at SE Ensign Lane operate well within acceptable jurisdictional standards. The addition of 1 to 2 peak hour trips to these intersections will not significantly impact these intersections or degrade intersection operation to levels below acceptable standards. Therefore, with respect to trip generation and the building size the proposed Popeyes restaurant will not create significantly more impacts than the Burger King restaurant.



Note that since the transportation impacts of a recently constructed credit union near the site weren't captured in the traffic count data or incorporated in the transportation study, an updated analysis was conducted which includes these credit union trips and the Popeyes restaurant's trip generation (see the *Credit Union Impacts* section).

The trip distribution and assignment for the trips generated by the Popeyes restaurant during the morning and evening peak hours is shown in Figure 1.





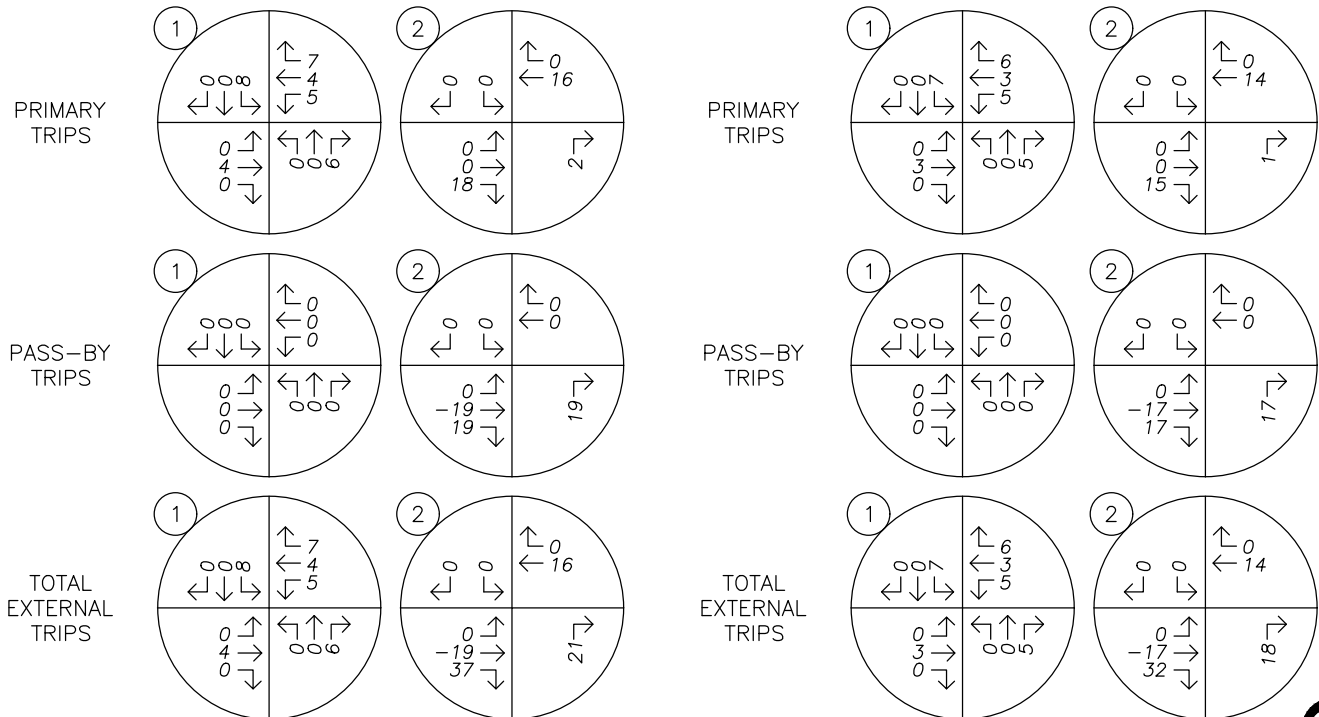
LEGEND

XX% PERCENT OF PRIMARY TRIPS

EXTERNAL SITE TRIP GENERATION				
	IN	OUT	PASS-BY	TOTAL
AM	20	18	38	76
PM	17	15	34	66

AM PEAK HOUR

PM PEAK HOUR



no scale

Drive-Thru Analysis

According to the applicant, Ambrosia QSR who operates multiple Burger King and Popeyes restaurants, the proposed Popeyes restaurant is expected to generate a similar number of trips and drive-thru queuing as a typical Burger King restaurant. The transportation study for the prior studied Burger King reviewed queuing at two similar Burger King restaurants on Friday, November 22, 2019, between the following three time periods to capture the expected morning, mid-day, and evening peak periods of the restaurants:

- Morning peak period: 7:30 AM through 9:00 AM;
- Mid-day peak period: 11:30 AM through 1:00 PM; and
- Evening peak period: 5:30 PM through 7:00 PM.

The largest observed queue at the two observed Burger King restaurants was 8 vehicles which occurred during the mid-day peak period. Note that the two observed Burger Kings had a single drive-thru lane at their respective order boards which will be similar to the proposed Popeyes and dissimilar to the prior proposed Burger King which had two drive-thru lanes at the order board.

Upon reviewing the site plan of the proposed Popeyes restaurant, the planned drive-thru provides approximately 160 feet of vehicle queue storage, while taking into consideration an additional 10 feet in front of a vehicle's driver side window/the drive-thru window. Assuming each vehicle occupies approximately 20 feet of space within the queue (an average 16 feet per vehicle with a 4-foot buffer between vehicles), the proposed drive-thru will be able to accommodate up to 8 vehicles. Note that an additional 70 feet of queue storage space through the proposed Popeyes parking lot could accommodate approximately 3 more vehicles without having queues extend back to the drive aisle along the northeast side of the Walmart building. Therefore, sufficient queue storage is expected to be available to adequately serve the proposed Popeyes restaurant.



Credit Union Impacts

Traffic counts utilized in the *Warrenton Burger King – Transportation Study* were collected on November 13/14, 2019. The TLC Credit Union, located northwest of the project site, had its grand opening in January of 2020 whereby impacts associated with the credit union were not incorporated in the traffic counts or transportation study. Therefore, the following methodology was used to determine whether the study intersections of US-101 at SE Ensign Lane and the RIRO shopping center access at SE Ensign Lane are able to accommodate future impacts of the proposed restaurant in conjunction with the impacts from the credit union.

Credit Union Trip Generation

According to a February 22, 2019 article from Clatsop News³, the credit union includes a 3,250 square-foot building with a two-lane remote view drive-up window and a drive-up ATM. To estimate the number of trips that are generated by the credit union, trip rates of the *Trip Generation Manual* were used. Data from land use code 912, *Drive-In Bank*, based on the square footage of the gross building floor area was used. The morning and evening peak hour internalization rates used for the credit union were referenced from the *Intersection Traffic Control Study Warrenton Retail* traffic study, which is consistent with the internalization that was applied to the proposed Popeyes restaurant. For pass-by trip generation, the following rates were referenced from land use code 912 in the *Trip Generation Handbook*.

- Approximately 29 percent of morning peak hour trips were assumed as pass-by trips;
- Approximately 35 percent of evening peak hour site trips were assumed as pass-by trips; and
- It is assumed that the average weekday pass-by trip rate would approximately match the evening peak hour rate.

The trip generation estimates of the credit union are summarized in Table 2. Detailed trip generation calculations are included as an attachment to this memorandum.

³ [TLC Credit Union to Expand into Warrenton Business Hub - ClatsopNews](#)



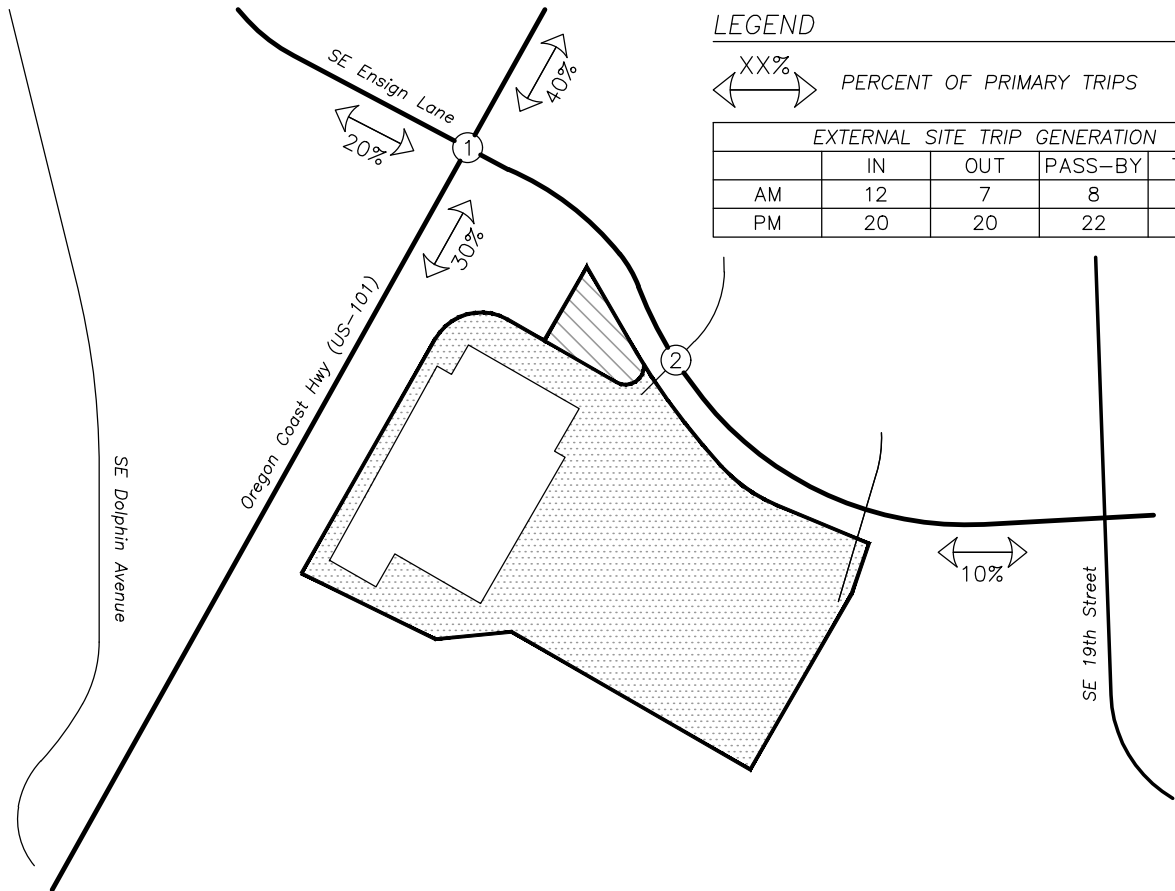
Table 2: Credit Union Trip Generation Summary

	ITE Code	Size/Rate	Morning Peak Hour			Evening Peak Hour			Weekday Total
			Enter	Exit	Total	Enter	Exit	Total	
<i>TLC Credit Union</i>									
Drive-In Bank	912	3,250 SF	18	13	31	33	33	66	326
<i>Internalization within Shopping Center</i>		12% (6%)	2	2	4	2	2	4	20
External Trips			16	11	27	31	31	62	306
<i>Pass-by Trips</i>		29% (35%)	4	4	8	11	11	22	108
Net New Primary Trips			12	7	19	20	20	40	198

With regard to trip distribution, it is assumed the trips generated by the credit union would travel to/from nearby transportation facilities in a manner consistent with what was estimated and presented in the *Warrenton Burger King – Transportation Study* for the proposed restaurant.

The trip distribution and assignment for the trips generated by the credit during the morning and evening peak hours is shown in Figure 2.





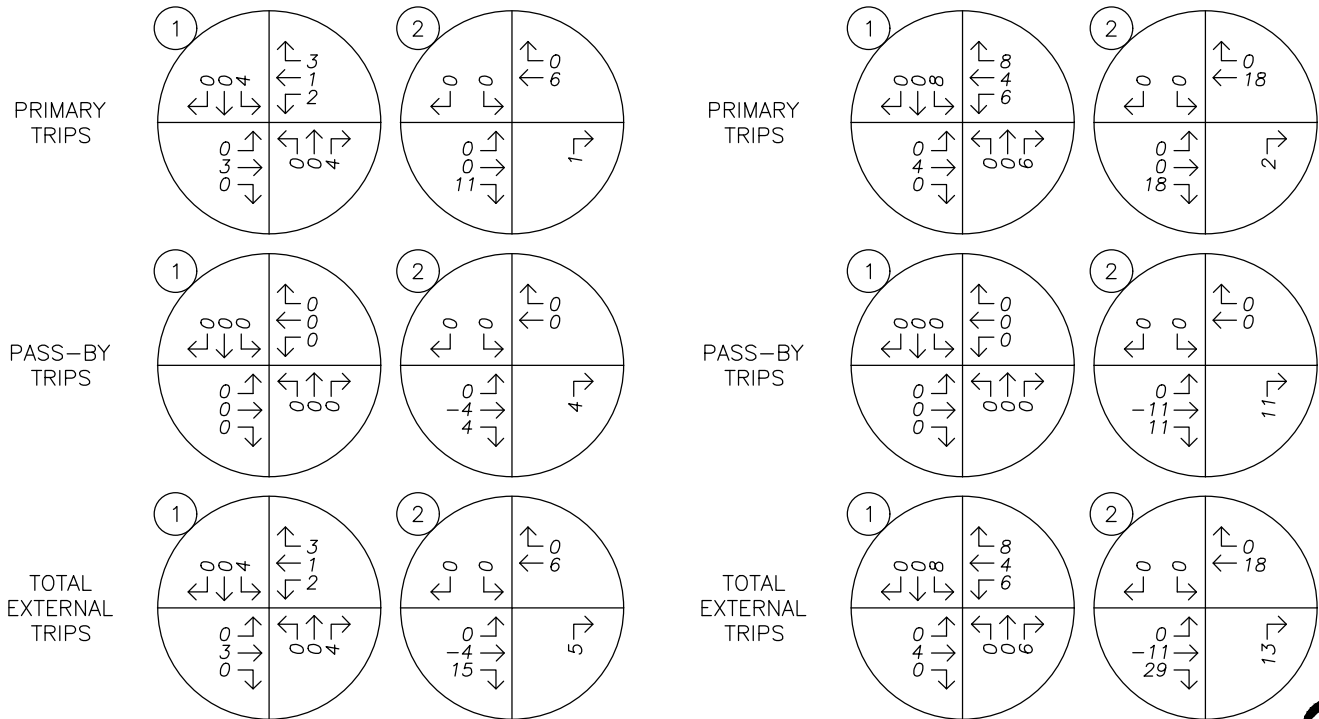
LEGEND

XX% PERCENT OF PRIMARY TRIPS

EXTERNAL SITE TRIP GENERATION				
	IN	OUT	PASS-BY	TOTAL
AM	12	7	8	27
PM	20	20	22	62

AM PEAK HOUR

PM PEAK HOUR



Traffic Volumes

To provide an analysis of future year impacts with both the credit union and proposed Popeyes restaurant in operation, an estimate of year 2021 existing, year 2023 background (the anticipated/conservative estimate for the opening year of the proposed restaurant), and year 2023 site buildout traffic conditions is necessary.

To estimate existing year 2021 traffic conditions, the 2021 “Background Volumes” from Figure 3 of the *Warrenton Burger King – Transportation Study* were referenced and trip impacts from the credit union were added to these volumes. Note that since these 2021 volumes were grown and estimated from traffic counts collected in 2019, no transportation impacts associated with COVID-19 were captured in these volume estimates. Therefore, no COVID-19 related adjustment factors are necessary to account for the influence of schools and businesses operating at limited capacities since March 2020.

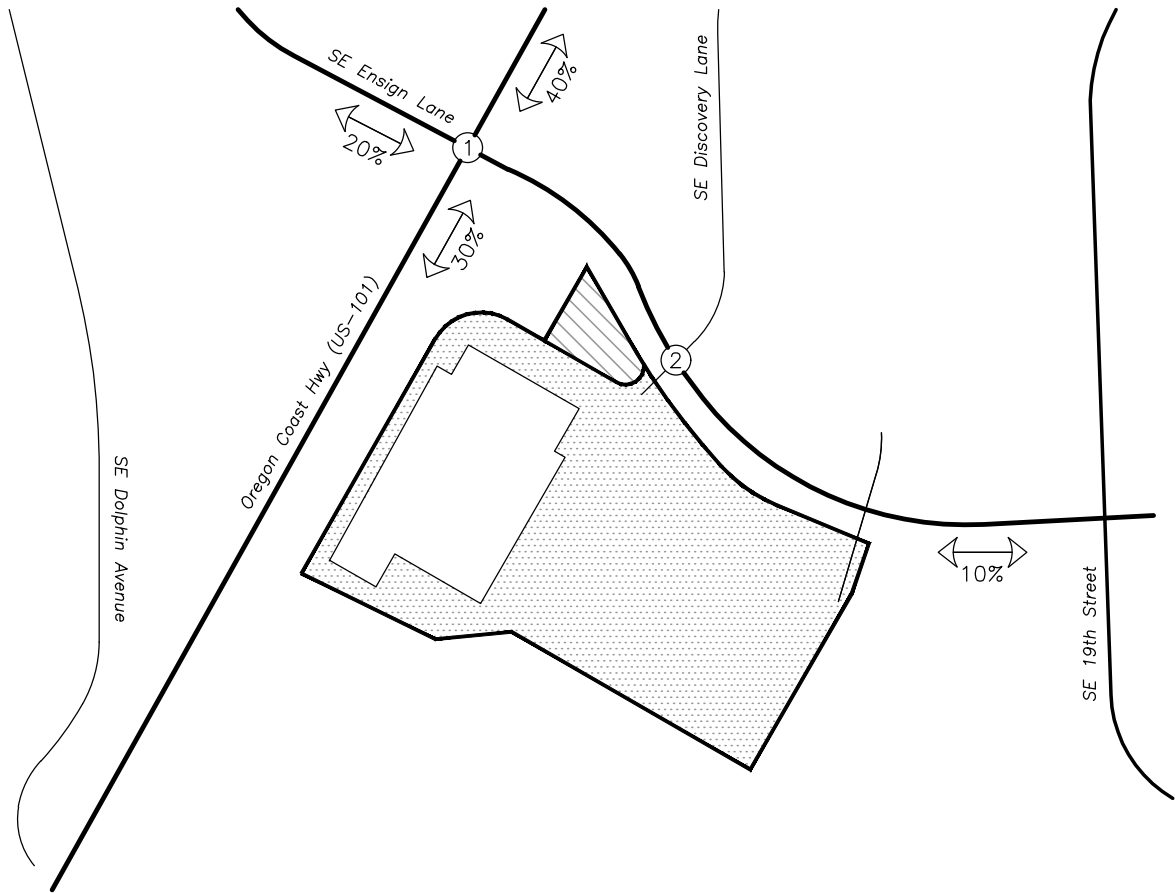
To estimate year 2023 background conditions without impacts from the proposed Popeyes restaurant, the estimated 2021 existing volumes were grown in a manner consistent with the *Warrenton Burger King – Transportation Study*. To re-iterate, the following growth rates were applied to the study intersection traffic volumes:

- Through volumes along US-101: A linear growth rate of 1.12 percent per year over a two-year period were applied to these volumes in accordance with ODOT’s Analysis Procedures Manual (APM).
- All other turning movements: A conservative, compounded growth rate of 2 percent per year over a two-year period were applied to these volumes.

To estimate year 2023 site buildout conditions, peak hour trips generated by the proposed Popeye’s restaurant were added to the year 2023 background condition volumes.

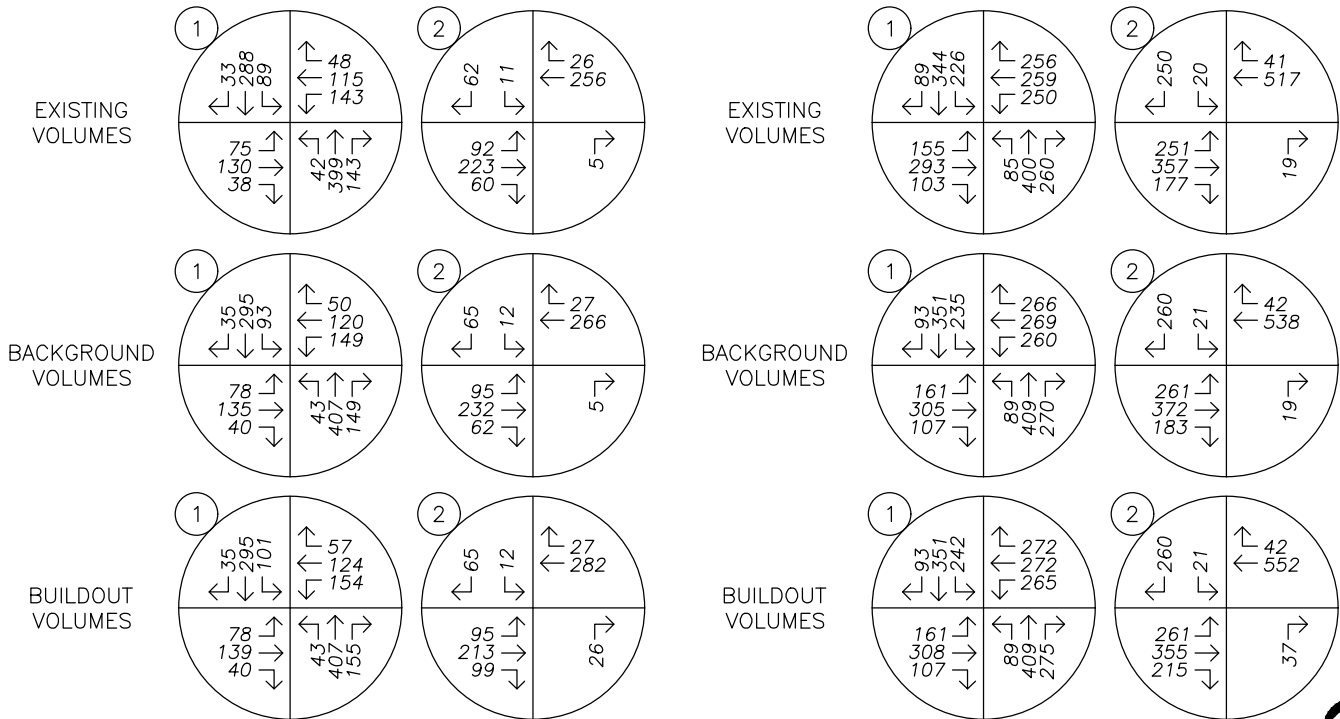
Figure 3 shows the 2021 existing condition, 2023 background condition, and 2023 buildout condition traffic volumes at the study intersections for the morning and evening peak hours.





AM PEAK HOUR

PM PEAK HOUR



no scale

TRAFFIC VOLUMES

2021 Existing, 2023 Background, & 2023 Buildout Conditions
AM & PM Peak Hours

Figure 3

Popeyes Restaurant

9/29/2021

Operation Analysis

Intersection Capacity Analysis

An updated capacity and delay analysis was conducted at the study intersections which incorporate trips generated by the credit union as well as the proposed Popeyes restaurant. The analysis was conducted per the signalized and unsignalized intersection analysis methodologies in the *Highway Capacity Manual*⁴ (HCM). Intersections are generally evaluated based on the average control delay experienced by vehicles and are assigned a grade according to their operation. The level of service (LOS) of an intersection can range from LOS A, which indicates very little or no delay experienced by vehicles, to LOS F, which indicates a high degree of congestion and delay. The volume-to-capacity (v/c) ratio is a measure that compares the traffic volumes (demand) against the available capacity of an intersection.

Performance Standards

According to the City of Warrenton's TSP, signalized and unsignalized intersections under City jurisdiction are required to operate at LOS D during the peak hour of analysis.

Per Clatsop County's *Transportation System Plan*⁵ (TSP) as well as direction by Clatsop County staff, the following minimum operation standards apply at intersections under City and County jurisdiction:

- Signalized, roundabout, and all-way stop-controlled intersections shall operate at LOS E or better with a v/c ratio no greater than 0.85.
- Two-way stop-controlled intersections shall operate at LOS E or better with a v/c ratio no greater than 0.90.

The intersection of US-101 at SE Ensign Lane operates under the jurisdiction of ODOT and must meet standards established in the *Oregon Highway Plan*⁶. Per the City of Warrenton's TSP, alternative mobility standards which include utilizing a v/c ratio of 0.85 for signalized intersections along US-101, within Warrenton city limits, were recommended to the Oregon Transportation Commission (OTC). These alternative standards have been adopted by the OTC, whereby an 0.85 v/c ratio standard of operation was applied to the study intersection along US-101.

Delay & Capacity Analysis

The LOS, delay, and v/c results of the capacity analysis are shown in Table 3 for the morning and evening peak hours. Note the TrafficWare Synchro software utilized for analysis does not report the overall v/c ratio of signalized intersections in the HCM 6th Edition capacity reports. For these intersections, the v/c ratio was calculated based on methods detailed in ODOT's *APM Section 13 Signalized Intersection Analysis*. Detailed calculations as well as tables showing the relationship between delay and LOS are included in the appendix to this report.

⁴ Transportation Research Board, *Highway Capacity Manual*, 6th Edition, 2016.

⁵ Clatsop County, *2015 Clatsop County Transportation System Plan*, October 2015.

⁶ Oregon Department of Transportation, *1999 Oregon Highway Plan*, 1999.



Table 3: Intersection Capacity Analysis Summary

		AM Peak Hour			PM Peak Hour		
		LOS	Delay (s)	v/c	LOS	Delay (s)	v/c
1. US-101 at SE Ensign Lane							
	2021 Existing Conditions	C	32	0.43	D	42	0.65
	2023 Background Conditions	C	33	0.44	D	43	0.67
	2023 Buildout Conditions	C	33	0.45	D	43	0.68
2. Shopping Center Access at SE Ensign Lane							
	2021 Existing Conditions	B	12	0.08	C	23	0.38
	2023 Background Conditions	B	13	0.08	C	24	0.40
	2023 Buildout Conditions	B	13	0.09	D	25	0.40

Table Notes: **BOLDED** text indicates intersection operation above jurisdictional standards.

Based on the results of the operational analysis, both study intersections are currently operating acceptably per agency standards and are projected to continue operating acceptably through the 2023 buildout year of the site. Accordingly, no operational mitigation is necessary or recommended.



Conclusions

The proposed Popeyes is projected to generate approximately 2 more net new morning peak hour trips, 1 more net new evening peak hour trip, and 34 more net new daily trips than the prior studied Burger King restaurant. The addition of 1 to 2 peak hour trips to the study intersections will not significantly impact these intersections or degrade intersection operation to levels below acceptable standards. Therefore, with respect to trip generation and the building size the proposed Popeyes restaurant will not create significantly more impacts than the Burger King restaurant.

The proposed Popeyes drive-thru lane is expected to have sufficient storage to accommodate queues which are expected at the proposed restaurant. Note that an additional 70 feet of queue storage space through the proposed Popeyes parking lot could accommodate approximately 3 more vehicles without having queues extend back to the drive aisle along the northeast side of the Walmart building.

The intersection of US-101 at SE Ensign Lane as well as the RIRO shopping center access at SE Ensign Lane are currently operating acceptably per agency standards and are projected to continue operating acceptably through the 2023 buildout year of the site with trip impacts from both the nearby TLC Credit Union and the proposed Popeyes restaurant. No operational mitigation is necessary or recommended.

If you have any questions or concerns regarding this analysis or need further assistance, please don't hesitate to contact us.





TRIP GENERATION CALCULATIONS

Land Use: Fast-Food Restaurant with a Drive-Thru Window
Land Use Code: 934
Setting/Location: General Urban/Suburban
Variable: 1,000 Sq. Ft. GFA
Variable Value: 2.133

AM PEAK HOUR

Trip Rate: 40.19

	Enter	Exit	Total
Directional Distribution	51%	49%	
Trip Ends	44	42	86

PM PEAK HOUR

Trip Rate: 32.67

	Enter	Exit	Total
Directional Distribution	52%	48%	
Trip Ends	36	34	70

WEEKDAY

Trip Rate: 470.95

	Enter	Exit	Total
Directional Distribution	50%	50%	
Trip Ends	502	502	1,004

SATURDAY

Trip Rate: 616.12

	Enter	Exit	Total
Directional Distribution	50%	50%	
Trip Ends	657	657	1,314



TRIP GENERATION CALCULATIONS

Land Use: Drive-in Bank
Land Use Code: 912
Setting/Location: General Urban/Suburban
Variable: 1000 Sq Ft Gross Floor Area
Variable Quantity: 3.25

AM PEAK HOUR

Trip Rate: 9.50

	Enter	Exit	Total
Directional Distribution	58%	42%	
Trip Ends	18	13	31

PM PEAK HOUR

Trip Rate: 20.45

	Enter	Exit	Total
Directional Distribution	50%	50%	
Trip Ends	33	33	66

WEEKDAY

Trip Rate: 100.03

	Enter	Exit	Total
Directional Distribution	50%	50%	
Trip Ends	163	163	326

SATURDAY

Trip Rate: 86.48

	Enter	Exit	Total
Directional Distribution	50%	50%	
Trip Ends	141	141	282



LEVEL OF SERVICE

Level of service is used to describe the quality of traffic flow. Levels of service A to C are considered good, and rural roads are usually designed for level of service C. Urban streets and signalized intersections are typically designed for level of service D. Level of service E is considered to be the limit of acceptable delay. For unsignalized intersections, level of service E is generally considered acceptable. Here is a more complete description of levels of service:

Level of service A: Very low delay at intersections, with all traffic signal cycles clearing and no vehicles waiting through more than one signal cycle. On highways, low volume and high speeds, with speeds not restricted by other vehicles.

Level of service B: Operating speeds beginning to be affected by other traffic; short traffic delays at intersections. Higher average intersection delay than for level of service A resulting from more vehicles stopping.

Level of service C: Operating speeds and maneuverability closely controlled by other traffic; higher delays at intersections than for level of service B due to a significant number of vehicles stopping. Not all signal cycles clear the waiting vehicles. This is the recommended design standard for rural highways.

Level of service D: Tolerable operating speeds; long traffic delays occur at intersections. The influence of congestion is noticeable. At traffic signals many vehicles stop, and the proportion of vehicles not stopping declines. The number of signal cycle failures, for which vehicles must wait through more than one signal cycle, are noticeable. This is typically the design level for urban signalized intersections.

Level of service E: Restricted speeds, very long traffic delays at traffic signals, and traffic volumes near capacity. Flow is unstable so that any interruption, no matter how minor, will cause queues to form and service to deteriorate to level of service F. Traffic signal cycle failures are frequent occurrences. For unsignalized intersections, level of service E or better is generally considered acceptable.

Level of service F: Extreme delays, resulting in long queues which may interfere with other traffic movements. There may be stoppages of long duration, and speeds may drop to zero. There may be frequent signal cycle failures. Level of service F will typically result when vehicle arrival rates are greater than capacity. It is considered unacceptable by most drivers.



*LEVEL OF SERVICE CRITERIA
FOR SIGNALIZED INTERSECTIONS*

LEVEL OF SERVICE	CONTROL DELAY PER VEHICLE (Seconds)
A	<10
B	10-20
C	20-35
D	35-55
E	55-80
F	>80

*LEVEL OF SERVICE CRITERIA
FOR UNSIGNALIZED INTERSECTIONS*

LEVEL OF SERVICE	CONTROL DELAY PER VEHICLE (Seconds)
A	<10
B	10-15
C	15-25
D	25-35
E	35-50
F	>50

HCM 6th Signalized Intersection Summary

1: US-101 & SE Ensign Lane

09/29/2021



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	75	130	38	143	115	48	42	399	143	89	288	33
Future Volume (veh/h)	75	130	38	143	115	48	42	399	143	89	288	33
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1856	1856	1856	1811	1811	1811	1826	1826	1826	1811	1811	1811
Adj Flow Rate, veh/h	89	155	45	170	137	57	50	475	170	106	343	39
Peak Hour Factor	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84
Percent Heavy Veh, %	3	3	3	6	6	6	5	5	5	6	6	6
Cap, veh/h	113	185	54	230	252	330	64	1981	990	131	1902	215
Arrive On Green	0.06	0.13	0.13	0.07	0.14	0.14	0.04	0.57	0.57	0.08	0.61	0.61
Sat Flow, veh/h	1767	1382	401	3346	1811	1535	1739	3469	1547	1725	3116	352
Grp Volume(v), veh/h	89	0	200	170	137	57	50	475	170	106	188	194
Grp Sat Flow(s),veh/h/ln	1767	0	1783	1673	1811	1535	1739	1735	1547	1725	1721	1748
Q Serve(g_s), s	6.0	0.0	13.1	6.0	8.5	3.6	3.4	8.2	5.3	7.3	5.7	5.8
Cycle Q Clear(g_c), s	6.0	0.0	13.1	6.0	8.5	3.6	3.4	8.2	5.3	7.3	5.7	5.8
Prop In Lane	1.00		0.22	1.00		1.00	1.00		1.00	1.00		0.20
Lane Grp Cap(c), veh/h	113	0	239	230	252	330	64	1981	990	131	1050	1067
V/C Ratio(X)	0.79	0.00	0.84	0.74	0.54	0.17	0.78	0.24	0.17	0.81	0.18	0.18
Avail Cap(c_a), veh/h	258	0	438	432	415	469	181	1981	990	295	1050	1067
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	55.4	0.0	50.7	54.8	48.1	38.4	57.3	12.8	8.7	54.6	10.2	10.2
Incr Delay (d2), s/veh	11.5	0.0	7.6	4.6	1.8	0.2	18.1	0.3	0.4	10.9	0.4	0.4
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	3.0	0.0	6.4	2.7	4.0	1.4	1.8	3.0	1.9	3.5	2.1	2.2
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	66.8	0.0	58.3	59.4	50.0	38.6	75.4	13.1	9.1	65.5	10.6	10.6
LnGrp LOS	E	A	E	E	D	D	E	B	A	E	B	B
Approach Vol, veh/h		289			364			695			488	
Approach Delay, s/veh		60.9			52.6			16.6			22.5	
Approach LOS		E			D			B			C	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	13.6	73.0	12.8	20.6	8.9	77.7	12.2	21.2				
Change Period (Y+Rc), s	4.5	4.5	4.5	4.5	4.5	4.5	4.5	4.5				
Max Green Setting (Gmax), s	20.5	36.5	15.5	29.5	12.5	44.5	17.5	27.5				
Max Q Clear Time (g_c+I1), s	9.3	10.2	8.0	15.1	5.4	7.8	8.0	10.5				
Green Ext Time (p_c), s	0.2	3.5	0.3	1.0	0.0	2.1	0.1	0.8				
Intersection Summary												
HCM 6th Ctrl Delay				32.3								
HCM 6th LOS				C								

HCM 6th TWSC
2: RIRO Access & SE Ensign Lane

09/29/2021

Intersection												
Int Delay, s/veh	2.1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↕			↕				↖	↖		↖
Traffic Vol, veh/h	92	223	60	0	256	26	0	0	5	11	0	62
Future Vol, veh/h	92	223	60	0	256	26	0	0	5	11	0	62
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	110	-	-	-	-	-	-	-	0	0	-	0
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	2	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	100	242	65	0	278	28	0	0	5	12	0	67

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	306	0	0	-	-	0	-	-	154	613	-	153
Stage 1	-	-	-	-	-	-	-	-	-	292	-	-
Stage 2	-	-	-	-	-	-	-	-	-	321	-	-
Critical Hdwy	4.14	-	-	-	-	-	-	-	6.94	7.54	-	6.94
Critical Hdwy Stg 1	-	-	-	-	-	-	-	-	-	6.54	-	-
Critical Hdwy Stg 2	-	-	-	-	-	-	-	-	-	6.54	-	-
Follow-up Hdwy	2.22	-	-	-	-	-	-	-	3.32	3.52	-	3.32
Pot Cap-1 Maneuver	1252	-	-	0	-	-	0	0	864	377	0	866
Stage 1	-	-	-	0	-	-	0	0	-	692	0	-
Stage 2	-	-	-	0	-	-	0	0	-	665	0	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	1252	-	-	-	-	-	-	-	864	352	-	866
Mov Cap-2 Maneuver	-	-	-	-	-	-	-	-	-	505	-	-
Stage 1	-	-	-	-	-	-	-	-	-	637	-	-
Stage 2	-	-	-	-	-	-	-	-	-	608	-	-

Approach	EB			WB			NB			SB		
HCM Control Delay, s	2			0			9.2			9.9		
HCM LOS							A			A		

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBT	WBR	SBLn1	SBLn2
Capacity (veh/h)	864	1252	-	-	-	-	505	866
HCM Lane V/C Ratio	0.006	0.08	-	-	-	-	0.024	0.078
HCM Control Delay (s)	9.2	8.1	-	-	-	-	12.3	9.5
HCM Lane LOS	A	A	-	-	-	-	B	A
HCM 95th %tile Q(veh)	0	0.3	-	-	-	-	0.1	0.3

HCM 6th Signalized Intersection Summary

1: US-101 & SE Ensign Lane

09/29/2021



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	155	293	103	250	259	256	85	400	260	226	344	89
Future Volume (veh/h)	155	293	103	250	259	256	85	400	260	226	344	89
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		0.99	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1885	1885	1885	1856	1856	1856	1856	1856	1856	1856	1856	1856
Adj Flow Rate, veh/h	160	302	106	258	267	264	88	412	268	233	355	92
Peak Hour Factor	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97
Percent Heavy Veh, %	1	1	1	3	3	3	3	3	3	3	3	3
Cap, veh/h	189	336	118	321	445	607	111	1253	705	264	1227	314
Arrive On Green	0.11	0.25	0.25	0.09	0.24	0.24	0.06	0.36	0.36	0.15	0.44	0.44
Sat Flow, veh/h	1795	1333	468	3428	1856	1550	1767	3526	1571	1767	2779	711
Grp Volume(v), veh/h	160	0	408	258	267	264	88	412	268	233	224	223
Grp Sat Flow(s),veh/h/ln	1795	0	1800	1714	1856	1550	1767	1763	1571	1767	1763	1727
Q Serve(g_s), s	10.5	0.0	26.3	8.9	15.3	15.0	5.9	10.2	13.6	15.5	9.7	10.0
Cycle Q Clear(g_c), s	10.5	0.0	26.3	8.9	15.3	15.0	5.9	10.2	13.6	15.5	9.7	10.0
Prop In Lane	1.00		0.26	1.00		1.00	1.00		1.00	1.00		0.41
Lane Grp Cap(c), veh/h	189	0	454	321	445	607	111	1253	705	264	779	763
V/C Ratio(X)	0.85	0.00	0.90	0.80	0.60	0.44	0.79	0.33	0.38	0.88	0.29	0.29
Avail Cap(c_a), veh/h	284	0	578	443	541	687	187	1253	705	386	779	763
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	52.7	0.0	43.4	53.3	40.5	27.0	55.5	28.2	22.0	50.0	21.4	21.5
Incr Delay (d2), s/veh	13.6	0.0	14.5	7.4	1.3	0.5	11.9	0.7	1.6	15.2	0.9	1.0
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	5.5	0.0	13.6	4.2	7.2	5.7	2.9	4.3	5.4	7.8	4.0	4.1
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	66.4	0.0	57.9	60.7	41.8	27.5	67.4	28.9	23.5	65.3	22.3	22.5
LnGrp LOS	E	A	E	E	D	C	E	C	C	E	C	C
Approach Vol, veh/h		568			789			768			680	
Approach Delay, s/veh		60.3			43.2			31.5			37.1	
Approach LOS		E			D			C			D	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	22.4	47.1	15.7	34.7	12.0	57.5	17.2	33.3				
Change Period (Y+Rc), s	4.5	4.5	4.5	4.5	4.5	4.5	4.5	4.5				
Max Green Setting (Gmax), s	26.2	21.8	15.5	38.5	12.7	35.3	19.0	35.0				
Max Q Clear Time (g_c+I1), s	17.5	15.6	10.9	28.3	7.9	12.0	12.5	17.3				
Green Ext Time (p_c), s	0.4	1.8	0.4	1.9	0.1	2.4	0.2	2.5				
Intersection Summary												
HCM 6th Ctrl Delay			42.0									
HCM 6th LOS			D									

HCM 6th TWSC
2: RIRO Access & SE Ensign Lane

09/29/2021

Intersection												
Int Delay, s/veh	4											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↕			↕				↖	↖		↖
Traffic Vol, veh/h	251	357	177	0	517	41	0	0	19	20	0	250
Future Vol, veh/h	251	357	177	0	517	41	0	0	19	20	0	250
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	110	-	-	-	-	-	-	-	0	0	-	0
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	2	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	95	95	95	95	95	95	95	95	95	95	95	95
Heavy Vehicles, %	3	3	3	4	4	4	0	0	0	2	2	2
Mvmt Flow	264	376	186	0	544	43	0	0	20	21	0	263

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	587	0	0	-	-	0	-	-	281	1282	-	294
Stage 1	-	-	-	-	-	-	-	-	-	566	-	-
Stage 2	-	-	-	-	-	-	-	-	-	716	-	-
Critical Hdwy	4.16	-	-	-	-	-	-	-	6.9	7.54	-	6.94
Critical Hdwy Stg 1	-	-	-	-	-	-	-	-	-	6.54	-	-
Critical Hdwy Stg 2	-	-	-	-	-	-	-	-	-	6.54	-	-
Follow-up Hdwy	2.23	-	-	-	-	-	-	-	3.3	3.52	-	3.32
Pot Cap-1 Maneuver	977	-	-	0	-	0	0	0	722	122	0	702
Stage 1	-	-	-	0	-	0	0	0	-	476	0	-
Stage 2	-	-	-	0	-	0	0	0	-	387	0	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	977	-	-	-	-	-	-	-	722	94	-	702
Mov Cap-2 Maneuver	-	-	-	-	-	-	-	-	-	222	-	-
Stage 1	-	-	-	-	-	-	-	-	-	347	-	-
Stage 2	-	-	-	-	-	-	-	-	-	275	-	-

Approach	EB			WB			NB			SB		
HCM Control Delay, s	3.2			0			10.1			13.9		
HCM LOS							B			B		

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBT	WBR	SBLn1	SBLn2
Capacity (veh/h)	722	977	-	-	-	-	222	702
HCM Lane V/C Ratio	0.028	0.27	-	-	-	-	0.095	0.375
HCM Control Delay (s)	10.1	10	-	-	-	-	22.9	13.2
HCM Lane LOS	B	B	-	-	-	-	C	B
HCM 95th %tile Q(veh)	0.1	1.1	-	-	-	-	0.3	1.7

HCM 6th Signalized Intersection Summary

1: US-101 & SE Ensign Lane

09/29/2021



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↶	↷		↶↷	↶	↷	↶	↶↶	↷	↶	↶↷	
Traffic Volume (veh/h)	78	135	40	149	120	50	43	407	149	93	295	35
Future Volume (veh/h)	78	135	40	149	120	50	43	407	149	93	295	35
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1856	1856	1856	1811	1811	1811	1826	1826	1826	1811	1811	1811
Adj Flow Rate, veh/h	93	161	48	177	143	60	51	485	177	111	351	42
Peak Hour Factor	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84
Percent Heavy Veh, %	3	3	3	6	6	6	5	5	5	6	6	6
Cap, veh/h	117	191	57	238	260	342	65	1944	977	137	1865	222
Arrive On Green	0.07	0.14	0.14	0.07	0.14	0.14	0.04	0.56	0.56	0.08	0.60	0.60
Sat Flow, veh/h	1767	1373	409	3346	1811	1535	1739	3469	1547	1725	3097	368
Grp Volume(v), veh/h	93	0	209	177	143	60	51	485	177	111	194	199
Grp Sat Flow(s),veh/h/ln	1767	0	1782	1673	1811	1535	1739	1735	1547	1725	1721	1745
Q Serve(g_s), s	6.2	0.0	13.7	6.2	8.8	3.8	3.5	8.6	5.7	7.6	6.1	6.2
Cycle Q Clear(g_c), s	6.2	0.0	13.7	6.2	8.8	3.8	3.5	8.6	5.7	7.6	6.1	6.2
Prop In Lane	1.00		0.23	1.00		1.00	1.00		1.00	1.00		0.21
Lane Grp Cap(c), veh/h	117	0	248	238	260	342	65	1944	977	137	1036	1051
V/C Ratio(X)	0.79	0.00	0.84	0.75	0.55	0.18	0.78	0.25	0.18	0.81	0.19	0.19
Avail Cap(c_a), veh/h	258	0	438	432	415	474	181	1944	977	295	1036	1051
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	55.2	0.0	50.4	54.7	47.8	37.7	57.2	13.5	9.2	54.3	10.7	10.7
Incr Delay (d2), s/veh	11.3	0.0	7.6	4.6	1.8	0.2	17.8	0.3	0.4	10.8	0.4	0.4
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	3.2	0.0	6.7	2.8	4.1	1.5	1.8	3.2	2.0	3.6	2.2	2.3
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	66.5	0.0	57.9	59.3	49.6	37.9	75.0	13.8	9.6	65.1	11.1	11.1
LnGrp LOS	E	A	E	E	D	D	E	B	A	E	B	B
Approach Vol, veh/h		302			380			713			504	
Approach Delay, s/veh		60.6			52.3			17.1			23.0	
Approach LOS		E			D			B			C	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	14.0	71.7	13.0	21.2	9.0	76.8	12.5	21.8				
Change Period (Y+Rc), s	4.5	4.5	4.5	4.5	4.5	4.5	4.5	4.5				
Max Green Setting (Gmax), s	20.5	36.5	15.5	29.5	12.5	44.5	17.5	27.5				
Max Q Clear Time (g_c+I1), s	9.6	10.6	8.2	15.7	5.5	8.2	8.2	10.8				
Green Ext Time (p_c), s	0.2	3.6	0.3	1.0	0.0	2.2	0.1	0.9				
Intersection Summary												
HCM 6th Ctrl Delay				32.6								
HCM 6th LOS				C								

HCM 6th TWSC
2: RIRO Access & SE Ensign Lane

09/29/2021

Intersection												
Int Delay, s/veh	2.1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↕			↕				↖	↖		↖
Traffic Vol, veh/h	95	232	62	0	266	27	0	0	5	12	0	65
Future Vol, veh/h	95	232	62	0	266	27	0	0	5	12	0	65
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	110	-	-	-	-	-	-	-	0	0	-	0
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	2	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	103	252	67	0	289	29	0	0	5	13	0	71

Major/Minor	Major1	Major2	Minor1	Minor2
Conflicting Flow All	318	0	0	160
Stage 1	-	-	-	304
Stage 2	-	-	-	332
Critical Hdwy	4.14	-	-	6.94
Critical Hdwy Stg 1	-	-	-	6.54
Critical Hdwy Stg 2	-	-	-	6.54
Follow-up Hdwy	2.22	-	-	3.32
Pot Cap-1 Maneuver	1239	-	0	857
Stage 1	-	-	0	681
Stage 2	-	-	0	655
Platoon blocked, %	-	-	-	-
Mov Cap-1 Maneuver	1239	-	-	857
Mov Cap-2 Maneuver	-	-	-	493
Stage 1	-	-	-	624
Stage 2	-	-	-	597

Approach	EB	WB	NB	SB
HCM Control Delay, s	2	0	9.2	10.1
HCM LOS			A	B

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBT	WBR	SBLn1	SBLn2
Capacity (veh/h)	857	1239	-	-	-	-	493	858
HCM Lane V/C Ratio	0.006	0.083	-	-	-	-	0.026	0.082
HCM Control Delay (s)	9.2	8.2	-	-	-	-	12.5	9.6
HCM Lane LOS	A	A	-	-	-	-	B	A
HCM 95th %tile Q(veh)	0	0.3	-	-	-	-	0.1	0.3

HCM 6th Signalized Intersection Summary

1: US-101 & SE Ensign Lane

09/29/2021



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	161	305	107	260	269	266	89	409	270	235	351	93
Future Volume (veh/h)	161	305	107	260	269	266	89	409	270	235	351	93
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		0.99	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1885	1885	1885	1856	1856	1856	1856	1856	1856	1856	1856	1856
Adj Flow Rate, veh/h	166	314	110	268	277	274	92	422	278	242	362	96
Peak Hour Factor	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97
Percent Heavy Veh, %	1	1	1	3	3	3	3	3	3	3	3	3
Cap, veh/h	196	347	122	329	458	625	116	1198	685	272	1184	310
Arrive On Green	0.11	0.26	0.26	0.10	0.25	0.25	0.07	0.34	0.34	0.15	0.43	0.43
Sat Flow, veh/h	1795	1333	467	3428	1856	1550	1767	3526	1571	1767	2763	724
Grp Volume(v), veh/h	166	0	424	268	277	274	92	422	278	242	229	229
Grp Sat Flow(s),veh/h/ln	1795	0	1800	1714	1856	1550	1767	1763	1571	1767	1763	1724
Q Serve(g_s), s	10.9	0.0	27.3	9.2	15.9	15.4	6.2	10.8	14.6	16.1	10.3	10.5
Cycle Q Clear(g_c), s	10.9	0.0	27.3	9.2	15.9	15.4	6.2	10.8	14.6	16.1	10.3	10.5
Prop In Lane	1.00		0.26	1.00		1.00	1.00		1.00	1.00		0.42
Lane Grp Cap(c), veh/h	196	0	469	329	458	625	116	1198	685	272	755	739
V/C Ratio(X)	0.85	0.00	0.90	0.82	0.60	0.44	0.80	0.35	0.41	0.89	0.30	0.31
Avail Cap(c_a), veh/h	298	0	578	414	512	670	190	1198	685	376	755	739
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	52.5	0.0	42.9	53.2	40.0	26.1	55.3	29.7	23.2	49.7	22.5	22.6
Incr Delay (d2), s/veh	13.2	0.0	15.6	9.7	1.7	0.5	11.7	0.8	1.8	17.4	1.0	1.1
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	5.7	0.0	14.2	4.4	7.5	5.8	3.1	4.6	5.8	8.2	4.3	4.3
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	65.7	0.0	58.6	62.9	41.6	26.6	67.0	30.5	25.0	67.1	23.6	23.7
LnGrp LOS	E	A	E	E	D	C	E	C	C	E	C	C
Approach Vol, veh/h		590			819			792			700	
Approach Delay, s/veh		60.6			43.6			32.8			38.7	
Approach LOS		E			D			C			D	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	23.0	45.3	16.0	35.7	12.3	55.9	17.6	34.2				
Change Period (Y+Rc), s	4.5	4.5	4.5	4.5	4.5	4.5	4.5	4.5				
Max Green Setting (Gmax), s	25.5	23.5	14.5	38.5	12.9	36.1	19.9	33.1				
Max Q Clear Time (g_c+I1), s	18.1	16.6	11.2	29.3	8.2	12.5	12.9	17.9				
Green Ext Time (p_c), s	0.4	2.0	0.3	1.9	0.1	2.5	0.2	2.4				
Intersection Summary												
HCM 6th Ctrl Delay			42.9									
HCM 6th LOS			D									

HCM 6th TWSC
2: RIRO Access & SE Ensign Lane

09/29/2021

Intersection												
Int Delay, s/veh	4.1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↕↗			↕↗				↖	↖		↖
Traffic Vol, veh/h	261	372	183	0	538	42	0	0	19	21	0	260
Future Vol, veh/h	261	372	183	0	538	42	0	0	19	21	0	260
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	110	-	-	-	-	-	-	-	0	0	-	0
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	2	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	95	95	95	95	95	95	95	95	95	95	95	95
Heavy Vehicles, %	3	3	3	4	4	4	0	0	0	2	2	2
Mvmt Flow	275	392	193	0	566	44	0	0	20	22	0	274

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	610	0	0	-	-	0	-	-	293	1334	-	305
Stage 1	-	-	-	-	-	-	-	-	-	588	-	-
Stage 2	-	-	-	-	-	-	-	-	-	746	-	-
Critical Hdwy	4.16	-	-	-	-	-	-	-	6.9	7.54	-	6.94
Critical Hdwy Stg 1	-	-	-	-	-	-	-	-	-	6.54	-	-
Critical Hdwy Stg 2	-	-	-	-	-	-	-	-	-	6.54	-	-
Follow-up Hdwy	2.23	-	-	-	-	-	-	-	3.3	3.52	-	3.32
Pot Cap-1 Maneuver	958	-	-	0	-	0	0	0	709	112	0	691
Stage 1	-	-	-	0	-	0	0	0	-	462	0	-
Stage 2	-	-	-	0	-	0	0	0	-	372	0	-
Platoon blocked, %		-	-	-	-							
Mov Cap-1 Maneuver	958	-	-	-	-	-	-	-	709	85	-	691
Mov Cap-2 Maneuver	-	-	-	-	-	-	-	-	-	208	-	-
Stage 1	-	-	-	-	-	-	-	-	-	329	-	-
Stage 2	-	-	-	-	-	-	-	-	-	258	-	-

Approach	EB			WB			NB			SB		
HCM Control Delay, s	3.3			0			10.2			14.4		
HCM LOS							B			B		

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBT	WBR	SBLn1	SBLn2
Capacity (veh/h)	709	958	-	-	-	-	208	691
HCM Lane V/C Ratio	0.028	0.287	-	-	-	-	0.106	0.396
HCM Control Delay (s)	10.2	10.3	-	-	-	-	24.4	13.6
HCM Lane LOS	B	B	-	-	-	-	C	B
HCM 95th %tile Q(veh)	0.1	1.2	-	-	-	-	0.4	1.9

HCM 6th Signalized Intersection Summary

1: US-101 & SE Ensign Lane

09/29/2021



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	78	139	40	154	124	57	43	407	155	101	295	35
Future Volume (veh/h)	78	139	40	154	124	57	43	407	155	101	295	35
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1856	1856	1856	1811	1811	1811	1826	1826	1826	1811	1811	1811
Adj Flow Rate, veh/h	93	165	48	183	148	68	51	485	185	120	351	42
Peak Hour Factor	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84
Percent Heavy Veh, %	3	3	3	6	6	6	5	5	5	6	6	6
Cap, veh/h	117	195	57	244	268	357	65	1910	965	147	1853	220
Arrive On Green	0.07	0.14	0.14	0.07	0.15	0.15	0.04	0.55	0.55	0.09	0.60	0.60
Sat Flow, veh/h	1767	1381	402	3346	1811	1535	1739	3469	1547	1725	3097	368
Grp Volume(v), veh/h	93	0	213	183	148	68	51	485	185	120	194	199
Grp Sat Flow(s),veh/h/ln	1767	0	1783	1673	1811	1535	1739	1735	1547	1725	1721	1745
Q Serve(g_s), s	6.2	0.0	14.0	6.4	9.1	4.3	3.5	8.8	6.1	8.2	6.1	6.2
Cycle Q Clear(g_c), s	6.2	0.0	14.0	6.4	9.1	4.3	3.5	8.8	6.1	8.2	6.1	6.2
Prop In Lane	1.00		0.23	1.00		1.00	1.00		1.00	1.00		0.21
Lane Grp Cap(c), veh/h	117	0	252	244	268	357	65	1910	965	147	1029	1044
V/C Ratio(X)	0.79	0.00	0.84	0.75	0.55	0.19	0.78	0.25	0.19	0.82	0.19	0.19
Avail Cap(c_a), veh/h	258	0	438	432	415	482	181	1910	965	309	1029	1044
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	55.2	0.0	50.2	54.6	47.5	36.9	57.2	14.1	9.7	54.0	10.9	10.9
Incr Delay (d2), s/veh	11.3	0.0	7.6	4.6	1.8	0.3	17.8	0.3	0.4	10.5	0.4	0.4
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	3.2	0.0	6.8	2.9	4.3	1.7	1.8	3.3	0.1	3.9	2.3	2.3
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	66.5	0.0	57.8	59.2	49.2	37.2	75.0	14.4	10.1	64.5	11.3	11.3
LnGrp LOS	E	A	E	E	D	D	E	B	B	E	B	B
Approach Vol, veh/h		306			399			721				513
Approach Delay, s/veh		60.4			51.7			17.6				23.8
Approach LOS		E			D			B				C
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	14.7	70.6	13.2	21.5	9.0	76.3	12.5	22.2				
Change Period (Y+Rc), s	4.5	4.5	4.5	4.5	4.5	4.5	4.5	4.5				
Max Green Setting (Gmax), s	21.5	35.5	15.5	29.5	12.5	44.5	17.5	27.5				
Max Q Clear Time (g_c+I1), s	10.2	10.8	8.4	16.0	5.5	8.2	8.2	11.1				
Green Ext Time (p_c), s	0.2	3.6	0.3	1.0	0.0	2.2	0.1	0.9				
Intersection Summary												
HCM 6th Ctrl Delay				33.0								
HCM 6th LOS				C								

HCM 6th TWSC
2: RIRO Access & SE Ensign Lane

09/29/2021

Intersection												
Int Delay, s/veh	2.2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↖↗			↖↗				↖	↖		↖
Traffic Vol, veh/h	95	213	99	0	282	27	0	0	26	12	0	65
Future Vol, veh/h	95	213	99	0	282	27	0	0	26	12	0	65
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	110	-	-	-	-	-	-	-	0	0	-	0
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	2	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	103	232	108	0	307	29	0	0	28	13	0	71

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	336	0	0	-	-	0	-	-	170	644	-	168
Stage 1	-	-	-	-	-	-	-	-	-	322	-	-
Stage 2	-	-	-	-	-	-	-	-	-	322	-	-
Critical Hdwy	4.14	-	-	-	-	-	-	-	6.94	7.54	-	6.94
Critical Hdwy Stg 1	-	-	-	-	-	-	-	-	-	6.54	-	-
Critical Hdwy Stg 2	-	-	-	-	-	-	-	-	-	6.54	-	-
Follow-up Hdwy	2.22	-	-	-	-	-	-	-	3.32	3.52	-	3.32
Pot Cap-1 Maneuver	1220	-	-	0	-	-	0	0	844	358	0	847
Stage 1	-	-	-	0	-	-	0	0	-	664	0	-
Stage 2	-	-	-	0	-	-	0	0	-	664	0	-
Platoon blocked, %		-	-	-	-	-	-	-				
Mov Cap-1 Maneuver	1220	-	-	-	-	-	-	-	844	324	-	847
Mov Cap-2 Maneuver	-	-	-	-	-	-	-	-	-	481	-	-
Stage 1	-	-	-	-	-	-	-	-	-	608	-	-
Stage 2	-	-	-	-	-	-	-	-	-	588	-	-

Approach	EB			WB			NB			SB		
HCM Control Delay, s	1.9			0			9.4			10.1		
HCM LOS							A			B		

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBT	WBR	SBLn1	SBLn2
Capacity (veh/h)	844	1220	-	-	-	-	481	847
HCM Lane V/C Ratio	0.033	0.085	-	-	-	-	0.027	0.083
HCM Control Delay (s)	9.4	8.2	-	-	-	-	12.7	9.6
HCM Lane LOS	A	A	-	-	-	-	B	A
HCM 95th %tile Q(veh)	0.1	0.3	-	-	-	-	0.1	0.3

HCM 6th Signalized Intersection Summary

1: US-101 & SE Ensign Lane

09/29/2021



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	161	308	107	265	272	272	89	409	275	242	351	93
Future Volume (veh/h)	161	308	107	265	272	272	89	409	275	242	351	93
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		0.99	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1885	1885	1885	1856	1856	1856	1856	1856	1856	1856	1856	1856
Adj Flow Rate, veh/h	166	318	110	273	280	280	92	422	284	249	362	96
Peak Hour Factor	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97
Percent Heavy Veh, %	1	1	1	3	3	3	3	3	3	3	3	3
Cap, veh/h	196	351	121	333	465	637	116	1173	675	279	1175	308
Arrive On Green	0.11	0.26	0.26	0.10	0.25	0.25	0.07	0.33	0.33	0.16	0.43	0.43
Sat Flow, veh/h	1795	1338	463	3428	1856	1550	1767	3526	1571	1767	2763	724
Grp Volume(v), veh/h	166	0	428	273	280	280	92	422	284	249	229	229
Grp Sat Flow(s),veh/h/ln	1795	0	1801	1714	1856	1550	1767	1763	1571	1767	1763	1724
Q Serve(g_s), s	10.9	0.0	27.6	9.4	16.0	15.6	6.2	10.9	15.1	16.6	10.3	10.6
Cycle Q Clear(g_c), s	10.9	0.0	27.6	9.4	16.0	15.6	6.2	10.9	15.1	16.6	10.3	10.6
Prop In Lane	1.00		0.26	1.00		1.00	1.00		1.00	1.00		0.42
Lane Grp Cap(c), veh/h	196	0	472	333	465	637	116	1173	675	279	750	733
V/C Ratio(X)	0.85	0.00	0.91	0.82	0.60	0.44	0.80	0.36	0.42	0.89	0.31	0.31
Avail Cap(c_a), veh/h	298	0	578	414	512	676	190	1173	675	376	750	733
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	52.5	0.0	42.8	53.1	39.7	25.6	55.3	30.4	23.8	49.5	22.8	22.9
Incr Delay (d2), s/veh	13.2	0.0	15.9	10.1	1.7	0.5	11.7	0.9	1.9	18.3	1.1	1.1
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	5.7	0.0	14.4	4.5	7.6	5.9	3.1	4.6	6.0	8.5	4.3	4.3
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	65.7	0.0	58.7	63.3	41.4	26.1	67.0	31.2	25.7	67.8	23.8	24.0
LnGrp LOS	E	A	E	E	D	C	E	C	C	E	C	C
Approach Vol, veh/h		594			833			798			707	
Approach Delay, s/veh		60.7			43.4			33.4			39.4	
Approach LOS		E			D			C			D	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	23.5	44.4	16.2	36.0	12.3	55.5	17.6	34.6				
Change Period (Y+Rc), s	4.5	4.5	4.5	4.5	4.5	4.5	4.5	4.5				
Max Green Setting (Gmax), s	25.5	23.5	14.5	38.5	12.9	36.1	19.9	33.1				
Max Q Clear Time (g_c+I1), s	18.6	17.1	11.4	29.6	8.2	12.6	12.9	18.0				
Green Ext Time (p_c), s	0.4	1.9	0.3	1.9	0.1	2.5	0.2	2.5				
Intersection Summary												
HCM 6th Ctrl Delay				43.2								
HCM 6th LOS				D								

HCM 6th TWSC
2: RIRO Access & SE Ensign Lane

09/29/2021

Intersection												
Int Delay, s/veh	4.1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↖↗			↖↗				↖	↖		↖
Traffic Vol, veh/h	261	355	215	0	552	42	0	0	37	21	0	260
Future Vol, veh/h	261	355	215	0	552	42	0	0	37	21	0	260
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	110	-	-	-	-	-	-	-	0	0	-	0
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	2	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	95	95	95	95	95	95	95	95	95	95	95	95
Heavy Vehicles, %	3	3	3	4	4	4	0	0	0	2	2	2
Mvmt Flow	275	374	226	0	581	44	0	0	39	22	0	274

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	625	0	0	-	-	0	-	-	300	1340	-	313
Stage 1	-	-	-	-	-	-	-	-	-	603	-	-
Stage 2	-	-	-	-	-	-	-	-	-	737	-	-
Critical Hdwy	4.16	-	-	-	-	-	-	-	6.9	7.54	-	6.94
Critical Hdwy Stg 1	-	-	-	-	-	-	-	-	-	6.54	-	-
Critical Hdwy Stg 2	-	-	-	-	-	-	-	-	-	6.54	-	-
Follow-up Hdwy	2.23	-	-	-	-	-	-	-	3.3	3.52	-	3.32
Pot Cap-1 Maneuver	946	-	-	0	-	0	0	0	702	111	0	683
Stage 1	-	-	-	0	-	0	0	0	-	453	0	-
Stage 2	-	-	-	0	-	0	0	0	-	376	0	-
Platoon blocked, %		-	-	-	-							
Mov Cap-1 Maneuver	946	-	-	-	-	-	-	-	702	81	-	683
Mov Cap-2 Maneuver	-	-	-	-	-	-	-	-	-	202	-	-
Stage 1	-	-	-	-	-	-	-	-	-	321	-	-
Stage 2	-	-	-	-	-	-	-	-	-	252	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	3.3	0	10.4	14.6
HCM LOS			B	B

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBT	WBR	SBLn1	SBLn2
Capacity (veh/h)	702	946	-	-	-	-	202	683
HCM Lane V/C Ratio	0.055	0.29	-	-	-	-	0.109	0.401
HCM Control Delay (s)	10.4	10.4	-	-	-	-	25	13.8
HCM Lane LOS	B	B	-	-	-	-	D	B
HCM 95th %tile Q(veh)	0.2	1.2	-	-	-	-	0.4	1.9

US-101 at SE Ensign Lane

Intersection v/c

APM Section 13.4.4: Critical Intersection v/c ratio

Method: Determine Critical Movements in HCM 2000 reports
 HCM 6th reports, determine adjusted and sat flow rates
 Adjust Flow/Sat Flow
 Sum up Crit Movement Flow Rates
 $X_c \text{ of intersection} = \text{sum}(\text{crit.move. Flow rates} * (C / (C-L)))$

AM Peak Hour																				
	Critical Movement				Adjust Flow			Saturated Flow			Adj/Sat Flows				Sum	C	L	Xc		
	EBTh+RT	WBL	NBTh	SBL	EBTh+RT	WBL	NBTh	SBL	EBTh+RT	WBL	NBTh	SBL								
2021 Existing Conditions					200	170	475	106	1783	3346	3469	1725	0.11217	0.050807	0.136927	0.061449	0.361354	120	18	0.425
2023 Background Conditions	EBTh+RT	WBL	NBTh	SBL	209	177	485	111	1782	3346	3469	1725	0.117284	0.052899	0.13981	0.064348	0.374341	120	18	0.440
2023 Buildout conditions					213	183	485	120	1783	3346	3469	1725	0.119462	0.054692	0.13981	0.069565	0.383529	120	18	0.451

US-101 at SE Ensign Lane

Intersection v/c

APM Section 13.4.4: Critical Intersection v/c ratio

Method: Determine Critical Movements in HCM 2000 reports
 HCM 6th reports, determine adjusted and sat flow rates
 Adjust Flow/Sat Flow
 Sum up Crit Movement Flow Rates
 $X_c \text{ of intersection} = \text{sum}(\text{crit.move. Flow rates} * (C / (C-L)))$

PM Peak Hour																				
	Critical Movement				Adjust Flow				Saturated Flow				Adj/Sat Flows				Sum	C	L	Xc
	EBTh+RT	WBL	NBTh	SBL	EBTh+RT	WBL	NBTh	SBL	EBTh+RT	WBL	NBTh	SBL	EBTh+RT	WBL	NBTh	SBL				
2021 Existing Conditions					408	258	412	233	1801	3428	3526	1767	0.226541	0.075263	0.116846	0.131862	0.550512	120	18	0.648
2023 Background Conditions	EBTh+RT	WBL	NBTh	SBL	424	268	422	242	1800	3428	3526	1767	0.235556	0.07818	0.119682	0.136955	0.570373	120	18	0.671
2023 Buildout conditions					428	273	422	249	1801	3428	3526	1767	0.237646	0.079638	0.119682	0.140917	0.577883	120	18	0.680



August 18, 2021

To: Charlie Patton/Ambrosia QSR
From: Will Caplinger, Interim City Planner, City of Warrenton
Copy: Van Wilfinger, Building Official; Colin Stelzig, Public Works Director; Brian Alsbury, Fire Chief; Trisha Hayrynen, Engineering Technician; Janice Weese, Building & Planning Clerk

Re: August 5, 2021 Pre-application meeting for Popeye's Louisiana Kitchen restaurant
Site: Tax Lot 81027CB01901
Zone: General Commercial (C-1)

This memo outlines information discussed during our preapplication conference. Please use this memo as a checklist to help assure that your application is complete when submitted. This memo does not constitute a land use decision and is provided as a courtesy to assist you with developing complete plans and applications. Comments are based on our discussion at the conference and on the Pre-Application Conference application received on July 23, 2021.

On the following pages you will find links to the sections of the Warrenton Municipal Code (WMC) that apply to the submittal, review, and approval of your application. Although staff endeavors to provide information on all relevant criteria for this application, please be advised that WMC Section 16.209.070.C.3 contains this disclaimer: *Failure of the Community Development Director or his/her designee to provide any of the information required by this subsection C shall not constitute a waiver of any of the standards, criteria, or requirements for the application.*

An eating and drinking establishment providing drive-up/drive-through service is a Conditional Use in the C-1 District per WMC 16.40.030.A.6, which requires a Type III procedure. The project is also subject to commercial site design review, which normally requires a Type II procedure, but these two applications will be consolidated for review and decision per WMC 16.208.070.D.2. Design standards that may apply to the project are contained in WMC Division 3. Information on the applications, permitting procedures, and design standards is provided via links listed on the following page.

Both local and state law allow the city to take up to 30 days to determine if an application is complete, but staff usually issues a completeness determination in less time. Both local and state law also require the city to complete the process to approve or disapprove the application, including the resolution of all local appeals, within 120 days of staff deeming the application complete. The Community & Economic Development Department performance review time for a Type III application from "completeness" to "notice of decision" is generally 6-8 weeks but can take longer depending on workload. Please note that Type III applications require a notice to be mailed to owners of property within 200 feet of the subject property and to appropriate responsible agencies, et al. Staff will provide an address list, but the envelopes must be addressed and stamped by the applicant, then returned to

the city for mailing. City staff can perform the envelope preparation component for a fee TBD.

The Warrenton Municipal Code sections referenced below will be used in reviewing the Conditional Use and the Site Design Review submittals. The applications require narrative responses to each applicable code section. Staff can provide you with a template for those responses if necessary.

Planning/Zoning Comments (Code links):

1. WMC Sections 16.40.030 through .060 describe Conditional Uses and related Development Standards in the C-1 District:
[16.40.030 Conditional Uses. \(qcode.us\)](#)
[16.40.040 Development Standards. \(qcode.us\)](#)
[16.40.050 Design Standards. \(qcode.us\)](#)
[16.40.060 Other Applicable Standards. \(qcode.us\)](#)
2. WMC Section 16.208.050 describes a Type III procedure:
[16.208.050 Type III Procedure \(Quasi-Judicial\). \(qcode.us\)](#)
3. WMC Section 16.208.070.D.2 contains the provisions for a consolidated review and decision:
[16.208.070 General Provisions. \(qcode.us\)](#)
4. WMC Section 16.212.040 describes the procedure for a Site Design Review application:
[16.212.040 Site Design Review. \(qcode.us\)](#)
5. WMC Chapter 16.220 describes the procedure for a Conditional Use application:
[Chapter 16.220 CONDITIONAL USE PERMITS \(qcode.us\)](#)
6. Design Standards that apply to this project include, but may not be limited to:
[16.116.030 Architectural and Site Design Standards. \(qcode.us\)](#)
[Chapter 16.120 ACCESS AND CIRCULATION \(qcode.us\)](#)
[Chapter 16.124 LANDSCAPING, STREET TREES, FENCES AND WALLS \(qcode.us\)](#)
[Chapter 16.128 VEHICLE AND BICYCLE PARKING \(qcode.us\)](#)
[Chapter 16.140 STORMWATER AND SURFACE WATER MANAGEMENT \(qcode.us\)](#)
[Chapter 16.144 SIGNS \(qcode.us\)](#)
[Chapter 16.152 GRADING, EXCAVATING, AND EROSION CONTROL PLANS \(qcode.us\)](#)

Building Department Comments:

The plans, as submitted, are conceptual in nature and do not contain sufficient information to allow a comprehensive Building Department review at this time. Upon completion of the pre-app, land use, and development review and approval, plans will be required, including a site plan, building plans, plumbing plans, and mechanical plans, if applicable. Electrical plans and permits are reviewed and inspected by Clatsop County Building Codes Division.

Public Works Comments:

Public Works understands that a new restaurant with a drive-through is proposed on tax Lot 81027CB01901. With this information, staff at public works has provided the following items that will need to be addressed in your design and planning documents:

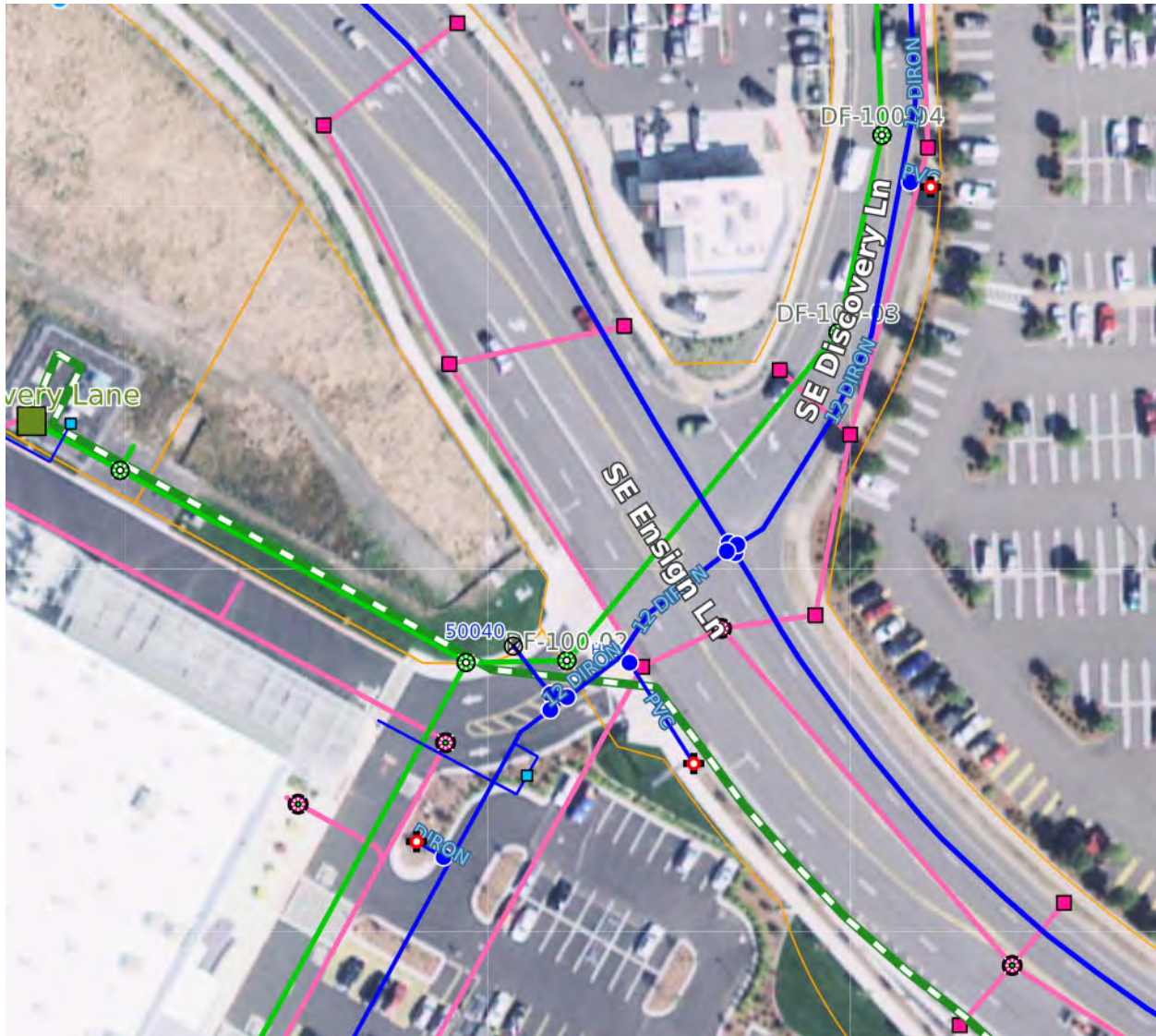
1. The developer is required to follow the City of Warrenton Development Standards. These standards can be found in Title 16 of the Warrenton Municipal Code. Please provide

documentation showing how this development will meet those standards set forth in the development code. Below is a link to the Development Code

<http://qcode.us/codes/warrenton/view.php?topic=16&frames=on>

2. The developer must follow the City's Water and Sewer Regulations. These regulations are included under Title 13 of the Warrenton Municipal Code. Please provide documentation showing how this development will meet those standards set forth in the development code. Below is a link to the Title 13 of our Code:
<http://qcode.us/codes/warrenton/view.php?topic=13&frames=on>
3. The developer is required to follow the Engineering Standards & Design Criteria Manual. Please provide documentation showing how the development meets the standards set forth in this manual. This manual can be found at the
<http://www.ci.warrenton.or.us/publicworks/page/engineering-specifications-design-guide>
4. Sewer services for commercial projects shall be a minimum of 6" diameter.
5. Grease interceptor sizing and installation shall conform to the current edition of the UPC.
 - a. All drains and fixtures in the establishment that may receive FOG shall drain through the interceptor. No drains from toilets, showers or other domestic discharges shall connect to the interceptor.
 - b. Temperatures in excess of 140 degrees shall not be discharged to the interceptor. Dishwashers shall not drain to the grease interceptor unless the volume/temperature (in combinations with any detergents or soaps) will not interfere with the operation of the interceptor.
 - c. A device shall be installed to control the rate of flow through the interceptor so that the rate of flow will not exceed the manufactures rate capacity based on gallons per minute.
6. In the future the city may establish a permit program for grease interceptors specifying various requirements such as:
 - a. Requirement for proper operation and maintenance
 - b. A Maintenance and frequency schedule
 - c. Requirements for maintaining and submitting logs and records including hauling records and waste manifests
 - d. Requirements to self-monitor (sample collection and analysis)
 - e. Establishment of Best Management Practices.
7. New water meter and service connections will be installed by the owner/contractor.
8. All commercial property shall have a backflow device at the meter for premise isolation.
9. The City will require an easement at meter and vault if located on private property.
10. Sidewalks shall be a minimum of 5 feet wide and shall meet ODOT standards.
11. Streetlights are required for all new developments. Show proposed street light locations and submit plan to Pacific Power & Light for circuit design.
12. Please work with the Fire Chief to determine appropriate fire hydrant spacing for this development. Developer team is responsible for determining if fire flows are available at this location. This can include hydrant testing and/or water modeling.
13. All on-site driveways, parking areas, aisles and turn-a-rounds shall have on-site collection or infiltration of surface waters to eliminate sheet flow of such waters onto public rights-of-way and abutting property. Surface water facility plans shall be prepared by a qualified engineer and constructed in accordance with City standards. Stormwater report shall detail pre and post stormwater conditions, including the adjacent ROWs and flows from existing storm system.
14. A stormwater report is required to show that that existing infrastructure is adequately sized. It is our understanding that this development will drain to an existing treatment swale located in ODOT ROW. The developer will need to show that the existing treatment swale has the capacity to treat runoff from this property, or provide additional treatment.

15. The City has design standards for refuse enclosures that include the required turning radius and access standard. Enclosure shall include room for recycling also.
16. An agreement with the adjacent property owner will need to be provided to allow the garbage truck access to turn around on their property.
17. If necessary, new fire connection to existing watermain shall include three valves.
18. If necessary, new watermains shall be a minimum of 8”.
19. It is assumed that this development will include one water service and one sewer service.
20. A traffic Impact Analysis is required for land use review. This analysis will look at the intersection of Walmart and SE Ensign Lane and the restaurant access lane intersection with Walmart access. This analysis will look at stacking, safety, and maneuverability.
21. No transformers or other structures can be placed over underground utilities. It appears in the preliminary plan that the transformer will be moved over the existing utilities. This will need to be changed.



Fire Department Comments:

Based on the information provided, access and fire flow should not be an issue for the Fire Department.

Estimated Permit Fees:

**Fees indicated below represent estimates based on the initial information presented in the pre-application conference and are subject to change. Estimates of System Development Charges and Building Permit fees are not included.*

Conditional Use application fee is \$1,000.

Site Design Review Type II application fee is \$500.

Grading permits and engineering design are reviewed by a third-party consultant and require a deposit and direct charge for services. This will be calculated at the time of submittal.

If you have any questions about the requirements or any City related issues, please contact Will Caplinger at cityplanner@ci.warrenton.or.us or (503) 468-1015, or Janice Weese at jweese@ci.warrenton.or.us or 503-861-0920.



Notice of Pending Type III Quasi-Judicial Decision

November 1st, 2021

To: Adjacent Property Owners and Interested Parties:

On behalf of North Coast Retail LLC., PM Design has submitted an application for a commercial site design review and a conditional use permit on Tax Lot 81027CB01901. The subject property's street address is 1771 SE Ensign Lane, Warrenton OR.

The application file is available for public review until December 9th, with a Quasi-Judicial decision to be made after the close of the review and comment period. Applicable Warrenton Municipal Code criteria include Chapter 16.40, General Commercial District, Section 16.120 Access & Circulation, Section 16.208.050 Type III Quasi-Judicial Procedures, and Chapter 16.220 Conditional Use Permits.

All evidence to be relied upon to make a decision on this application is in the public record and available for review at the Warrenton Building and Planning Department, 225 S. Main. Ave., Warrenton, at no cost. Copies can be made at a reasonable cost. Following the close of the comment period, the Planning Commission will issue a Type III Quasi-Judicial Decision which will be mailed to the applicant and all parties who submit written comments or who are otherwise legally entitled to notice.

HOW TO PARTICIPATE: All interested persons are invited to submit written comments to Scott Hazelton, Planning Director, Warrenton City Hall, PO Box 250, Warrenton, OR 97146 by December 9th, 2021. Failure to participate in this Quasi-Judicial review in writing or failure to address relevant issues with sufficient specificity may preclude your right to appeal the administrative decision on this application.

Notice to mortgagees, lien holders, vendors or sellers: The city of Warrenton Development Code requires that if you receive this notice it shall promptly be forwarded to the purchaser.

FOR FURTHER INFORMATION contact Scott Hazelton, Planning Director at 503.861.0920 Monday through Friday, 8:30 a.m. to noon/1:00 p.m. to 5:00 p.m.

Scott Hazelton, Planning Director



Notice of Pending Type III Quasi-Judicial Decision

On behalf of North Coast Retail LLC., PM Design has submitted an application for a commercial site design review and a conditional use permit on Tax Lot 81027CB01901. The subject property's street address is 1771 SE Ensign Lane, Warrenton OR.

The application file is available for public review until December 9th, with a Quasi-Judicial decision to be made after the close of the review and comment period. Applicable Warrenton Municipal Code criteria include Chapter 16.40, General Commercial District, Section 16.120 Access & Circulation, Section 16.208.050 Type III Quasi-Judicial Procedures, and Chapter 16.220 Conditional Use Permits.

All evidence to be relied upon to make a decision on this application is in the public record and available for review at the Warrenton Building and Planning Department, 225 S. Main. Ave., Warrenton, at no cost. Copies can be made at a reasonable cost. Following the close of the comment period, the Planning Commission will issue a Type III Quasi-Judicial Decision which will be mailed to the applicant and all parties who submit written comments or who are otherwise legally entitled to notice.

HOW TO PARTICIPATE: All interested persons are invited to submit written comments to Scott Hazelton, Planning Director, Warrenton City Hall, PO Box 250, Warrenton, OR 97146 by December 9th, 2021. Failure to participate in this Quasi-Judicial review in writing or failure to address relevant issues with sufficient specificity may preclude your right to appeal the administrative decision on this application.

Notice to mortgagees, lien holders, vendors or sellers: The City of Warrenton Development Code requires that if you receive this notice it shall promptly be forwarded to the purchaser.

FOR FURTHER INFORMATION contact Scott Hazelton, Planning Director at 503.861.0920 Monday through Friday, 8:30 a.m. to noon/1:00 p.m. to 5:00 p.m.

Published: *The Columbia Press*

Date: November 26, 2021

First Name Albert (Bert)

Last Name Little

Email bdlittle1020@gmail.com

Question/Comment

I am concerned about the request to build another fast food (Popeyes) building next to TLC and Walmart. The traffic is my concern, especially during vacation times. I believe this will back the traffic back to HWY 101 creating possible car accidents. If the city would start thinking about building on the East side of Costco, which in my opinion is the place to put it. Reason, city is growing, this area is close to where the new school system will be built with K-12 all in one area. Kids could walk there for lunch, High school could drive there etc... There will a tremendous amount of traffic using Ensign once the school is complete. Visiting schools will look for a place to eat and need some room to park the bus. We need to think about the future of our city of Warrenton. No quick fix is a good idea. Example Wendy's bad location but hind sight should help make better decisions in the future. Thank you for letting me vent.



December 1, 2021

To: Warrenton Planning Commission
From: Scott Hazelton, Planning Director
Re: Appeal (AP-21-1) of Administrative Decision on SIG-21-7

BACKGROUND & STAFF RECOMMENDATION

On October 26, 2021, Steve Fuhrmann of Rudnick Electric Signs submitted SIG-21-7 on behalf of the Warrenton Hammond School District (“the District”) to construct a new monument sign at 1050 SE Warrior Way (Warrenton) on Tax Lot 810340000105. The property is in the High Density Residential (RH) Zone.

The application was deemed complete on November 12, 2021. The application was denied on November 12, 2021 by staff. The Official Notice of Decision is attached as Attachment 5. On November 8, 2021, Staff met with representatives of the District to discuss steps forward, including the possibility of a zone change, obtaining a variance, or beginning the appeal process. The District expressed that there was no desire to pursue a zone change which would allow the proposed signage. The school stated that not pursuing a rezone to Open Space and Institutional from the High Density Residential was due to a reduction in resale value of the property.

Pursuant to WMC Section 16.36.050, the residential high-density zone does allow for signage as provided by WMC Section 16.144. Under WMC Section 16.144.030, approved conditional uses in the RH Zone, “may have one permanent nameplate sign with up to four square feet of sign area, placed on the exterior of the structure.” There is no other authorization in WMC.144.030 for other than a four square foot (4’ x 4’) sign placed on the side of the building. Accordingly, staff denied the applicant’s request for a monument sign, as staff does not have the authority to approve permits that do not conform to the Warrenton Municipal Code without the applicant first requesting and receiving approval of a variance under WMC Section 16.272, establishing the existence of a valid non-conforming use under WMC Section 16.276, or another similar such exception.

The District argues that the approval of SDR-20-1 and PUD-20-1 allowed for a sign that was in excess of the dimensions required by the code. Staff has reviewed the notice of decision for SDR-20-1 and PUD-20-1. The condition of approval the applicant refers to requires a sign permit to be separately applied for in the future and a

consolidated application for signage was neither submitted by the applicant nor reviewed by the City at the time Application SDR-20-1/PUD-20-1 was reviewed. As such, the current application, as a standalone sign permit application, must adhere to the zoning district in which the parcels are located unless, as discussed above, a valid exception is established.

The primary purpose and intent of the staff report is to make findings on whether the application satisfies criteria and standards specified in the Warrenton Municipal Code (WMC). The City maintains consistency with Comprehensive Plan provisions through the enactment and application of land use regulations. Hence, the staff report does not make specific findings on compliance with Comprehensive Plan Policies or provisions, but adheres to the WMC chapter order listed below.

The applicant submitted plans that do not satisfy WMC Section 16.144.030. Staff recommends that the Commission DENY the applicant's appeal for the reasons set forth above. The staff report addresses the applicable standards and criteria and how the application fails to satisfy all of the applicable criteria. Please review the staff report in full before the meeting.

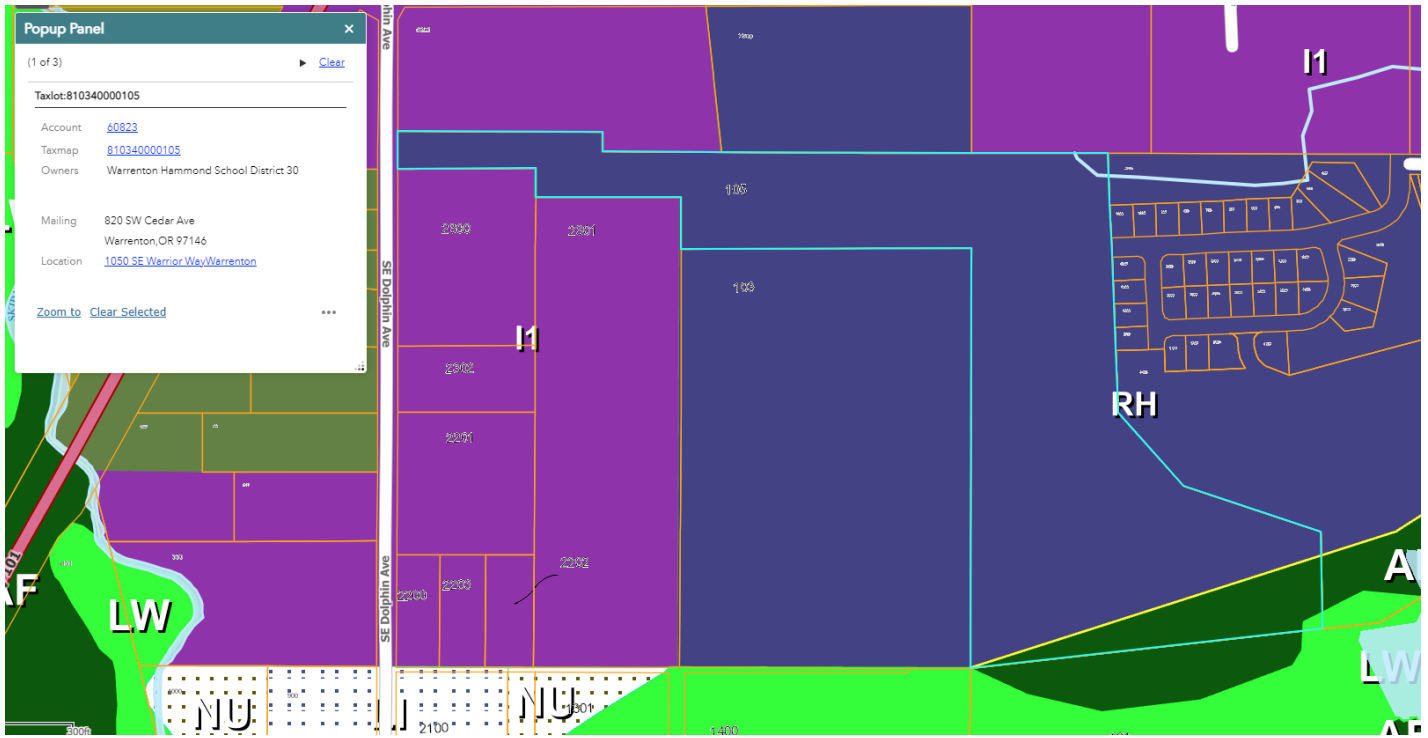


Figure 1: School site outlined in the RH Zone

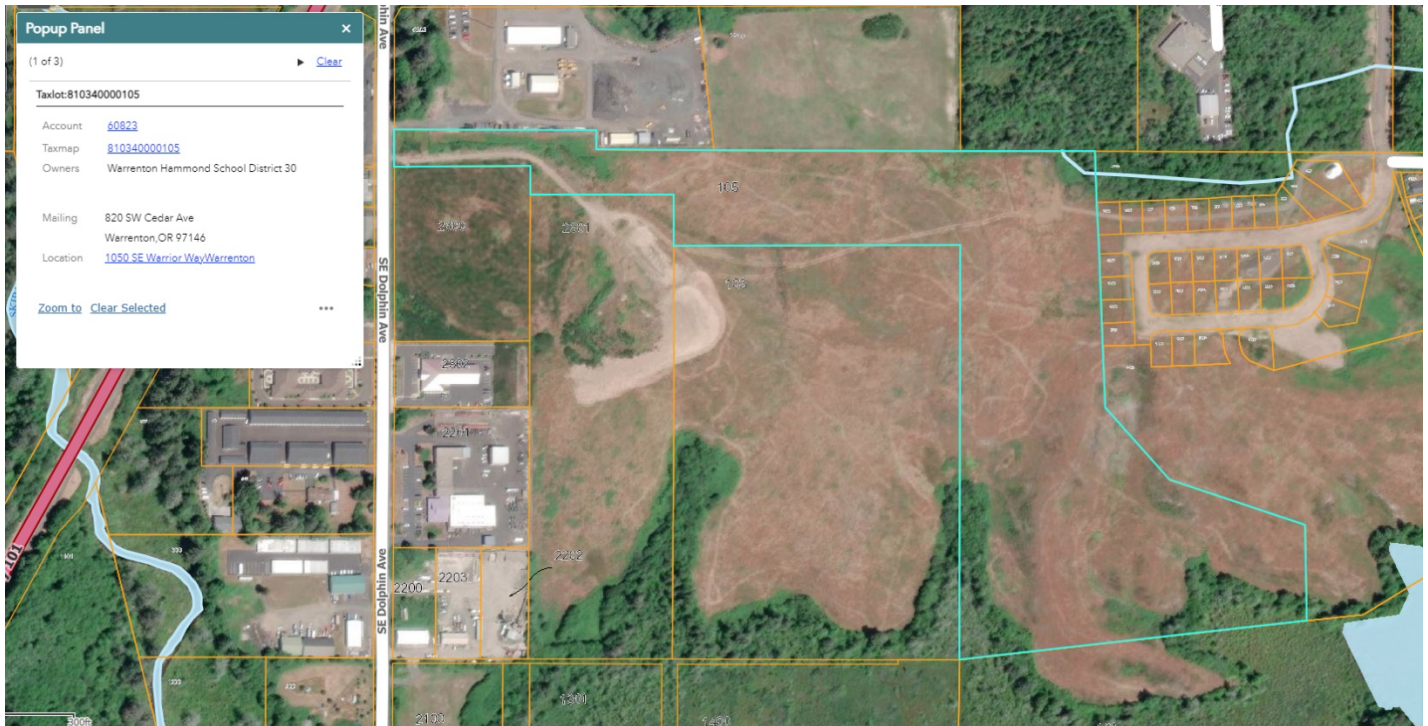


Figure 2: School Site, Aerial

APPLICABLE CRITERIA AND STANDARDS

Chapter 16.144 Signs

FINDINGS

Chapter 16.144 Signs

16.144.030: Sign Requirements for R-40, R-10, R-M and R-H Zones.

Staff Findings: This criterion has not been met. The proposed sign is not the type nor the size that is allowed in the RH Zone. This denial is the purpose of the appeal.

Staff Conclusion for Chapter 16.144: Sign Requirements for R-40, R-10, R-M and R-H Zones.

Staff finds that the criteria of Chapter 16.144: Sign Requirements for R-40, R-10, R-M and R-H Zones has not been met by the applicant.

Conclusion, Recommendation, & Conditions of Approval

Conclusion and Recommendation

The findings above demonstrate that the proposed design of the Warrenton Hammond School District is not consistent with the RH zoning district development standards and the design standards of the development code. Staff recommends that the Planning Commission denies this appeal.

Suggested Motion:

I motion to deny the Appeal application (AP-21-1) for the Warrenton Hammond School District Sign, located on Tax Lot 810340000105, based on the findings in the staff report and discussion.

Attachments

- 1 Sign Application
- 2 Appeal Application
- 3 Site Design Plan Set
- 4 Staff Correspondence with Applicant
- 5 Notice of Decision

**SIGN
APPLICATION**



Commercial Building Permit Application

City of Warrenton
 Planning and Building Department
 225 S Main Ave • P.O. Box 250 • Warrenton, OR 97146
 Ph (503) 861-0920 • Fax (503) 861-2351
 Email: buildingclerk@ci.warrenton.or.us

NOTICE: Submission of this application begins the Development Clearance Review process (when applicable).

Applicant shall receive development clearance review approval from City of Warrenton Public Works, Planning, and Fire Departments prior to the initiation of Building Department construction plan review procedures. Any and all conditions of approval shall be noted on construction documents.

DEPARTMENT USE ONLY	
Permit no.:	
Submittal Date:	
Permit Issue Date:	
Map/Taxlot Number:	
DEVELOPMENT CLEARANCE	
<input type="checkbox"/> Public Works Approved	Date:
<input type="checkbox"/> Planning Approved	Date:
<input type="checkbox"/> Fire Department Approved	Date:
<input type="checkbox"/> Conditions of approval have been provided to applicant.	
<input type="checkbox"/> Development clearance not applicable for this permit.	

This permit is issued under OARs 918-460-0050 and 918-050-0110. Permits expire if work is not started within 180 days of issuance or if work is suspended for 180 days.

REQUIRED: Please review 'Commercial Plan Review Submittal Checklist' and provide plans and documents required for review.

APPLICANT INFORMATION	
Name: Rudnick Electric Signs	
Mailing address: 1400 SE Township RD	
City/state/ZIP: Canby, OR 97013	
Phone: 503-263-3600	Mobile phone:
Email: sales@rudnickelectricsigns.com	
PROPERTY OWNER INFORMATION	
<input type="checkbox"/> Same as Applicant Information	
Name: City of Warrenton	
Mailing address: 225 S Main St	
City/state/ZIP: Warrenton, OR - Contact Mike Moha	
Phone: 503-861-2281	Mobile phone:
JOB SITE INFORMATION AND LOCATION	
Job site address: 1050 SE Worrier way	
City: Warrenton	
State/ZIP: 97146	
Zoning:	
Floodplain: <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
INSTALLATION INFORMATION	
<input checked="" type="checkbox"/> I am licensed with Building Code Division.	
License no.: CCB 213108	Expires: 12/13/22
<input type="checkbox"/> I am registered with the Construction Contractors Board.	
CCB license no.:	Expires:
<input type="checkbox"/> I am the property owner hiring a construction contractor.	
License no.:	Expires:
<input type="checkbox"/> I am a property owner doing my own work.	
CONTRACTOR INFORMATION	
Business name: Rudnick Electric Signs	
Address: 1400 SE Township RD	
City/state/ZIP: Canby OR	
Phone: 503-263-3600	Fax:
Email: sales@rudnickelectricsigns.com	
City of Warrenton Business Lic. No.:	
Contact Name: Steve Fuhrmann	
Contact Phone: 503-263-3600	

TYPE OF CONSTRUCTION	
<input type="checkbox"/> New	<input type="checkbox"/> Tenant Improvement (Addition, Alteration)
<input type="checkbox"/> Repair, Replacement	<input checked="" type="checkbox"/> Other (specify) Pylon Sign

VALUATION INFORMATION	
<i>Projects for which square footage is not used to determine permit fees, fees are based on the value of the work performed. Please indicate the value (rounded to the nearest dollar) of all equipment, materials, labor, overhead, and profit for the work indicated on this application.</i>	
Project Valuation: 13,000.00	
Square Footage (new building, remodels, or additions):	
Description of Work: New Pylon sign for School	
Existing building square footage:	
Construction Type:	
Occupancy Type (Existing):	Occupancy Type (New):
Number of Buildings:	
Number of Housing Units:	
Number of Stories:	
Building height: feet inches	
Publicly owned: <input type="checkbox"/> Yes <input type="checkbox"/> No	
Existing fire sprinklers: <input type="checkbox"/> Yes <input type="checkbox"/> No	
Fire sprinklers included in project: <input type="checkbox"/> Yes <input type="checkbox"/> No	
Existing fire alarm system: <input type="checkbox"/> Yes <input type="checkbox"/> No	
Fire alarms included in project: <input type="checkbox"/> Yes <input type="checkbox"/> No	

SIGNATURE	
I hereby certify that, to my knowledge, the above information is true and correct. All work to be performed shall be done in accordance with all governing laws and rules, and shall comply with applicable building code and all required conditions of approvals:	
Applicant's Signature:	
Applicant's Name (Print): Steve Fuhrmann	Date: 10.26.21

DEPARTMENT USE ONLY

1 - Building Permit Fees

1a - Permit Fees:

1b - State Surcharge 12% (*Line 1a X 0.12*):

2 - Plan Review Fees

2a - Plan Review Permit Fee (*Permit Fee x 0.65*):

2b - Fire & Life Safety (*Permit Fee x 0.40*):

3- TOTAL PERMIT FEES (*Sum of 1a+1b+2a+2b*):

3a - Amount Paid on Building Permit Fees.....:

Check No.: Receipt No.: Date:

4 - Miscellaneous Fees

4a - Additional plan reviews or inspections
for which no fee is specified (*\$106.00 per Hour*) :

4b - Re-Inspection Fee (*\$125 per inspection*):

4c - Research Fees (*\$53.00 per Half-Hour; min. charge \$53.00*):

5-TOTAL MISCELLANEOUS FEES (*Sum of 4a+4b+4c*):

5a - Amount Paid on Remainder of Fees.....:

Check No.: Receipt No.: Date:

6-BALANCE DUE ON BULDING PERMIT FEES
(Sum of 3-3a+5-5a) :

WARRENTON-HAMMOND SCHOOL DISTRICT NO. 30

820 SW Cedar, Warrenton, OR 97146-9799 • Phone (503) 861-2281 • Fax (503) 861-2911

October 4, 2021

City of Warrenton
225 S Main Ave.
Warrenton, OR 97146

Re: Rudnick Electric Signs Permit

City Of Warrenton:

This letter is to verify that Rudnick Electric Signs LLC has our permission to request permitting for the installation of an electric sign at the Warrenton Middle School located at 1050 SE Warrior Way, Warrenton, OR 97146.

I, Mike Moha, Business Manager, as an authorized owner representative of the Warrenton Middle School property authorize Rudnick Electric Signs LLC to acquire the required permits to install the sign.

If you have any questions or concerns, please feel free to contact me at the email or phone number below.

Thank you,



Mike Moha
Business Manager
Warrenton-Hammond School District
503-861-2281
moham@warrentonk12.org

Warrenton

MIDDLE SCHOOL

ORIGINAL DESIGN

August 17, 2021



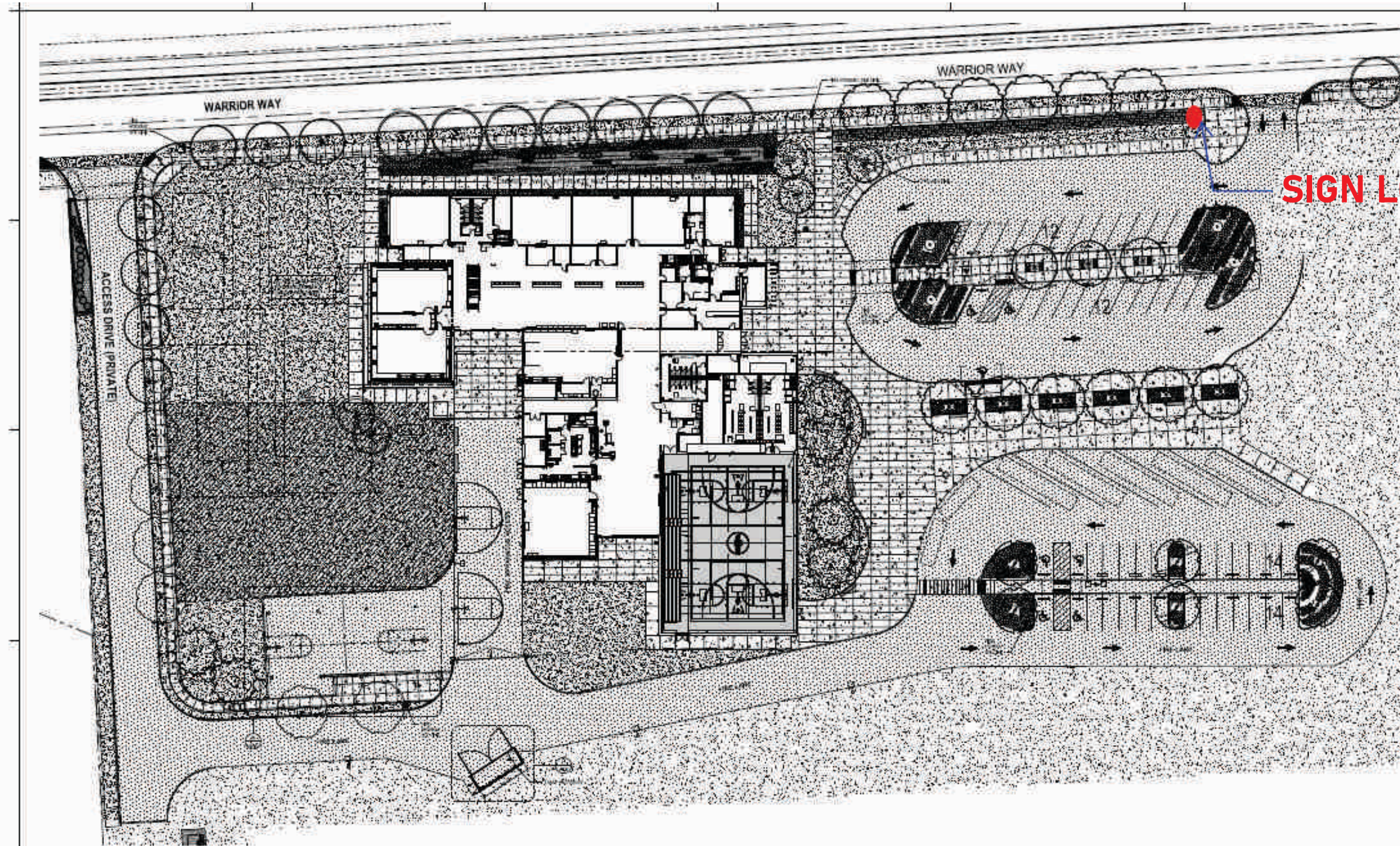
Steven Rudnick

steve@rudnickelectricsigns.com

1400 Southeast Township Road,
Canby, OR 97013

(503) 263-3600

www.rudnickelectricsigns.net



Steven Rudnick
 Phone: 503.655.2610
 Fax: 503.980.7919
steve@rudnickelectricsigns.com
 1625 Washington St.
 Canby, OR 97045
www.rudnickelectricsigns.net

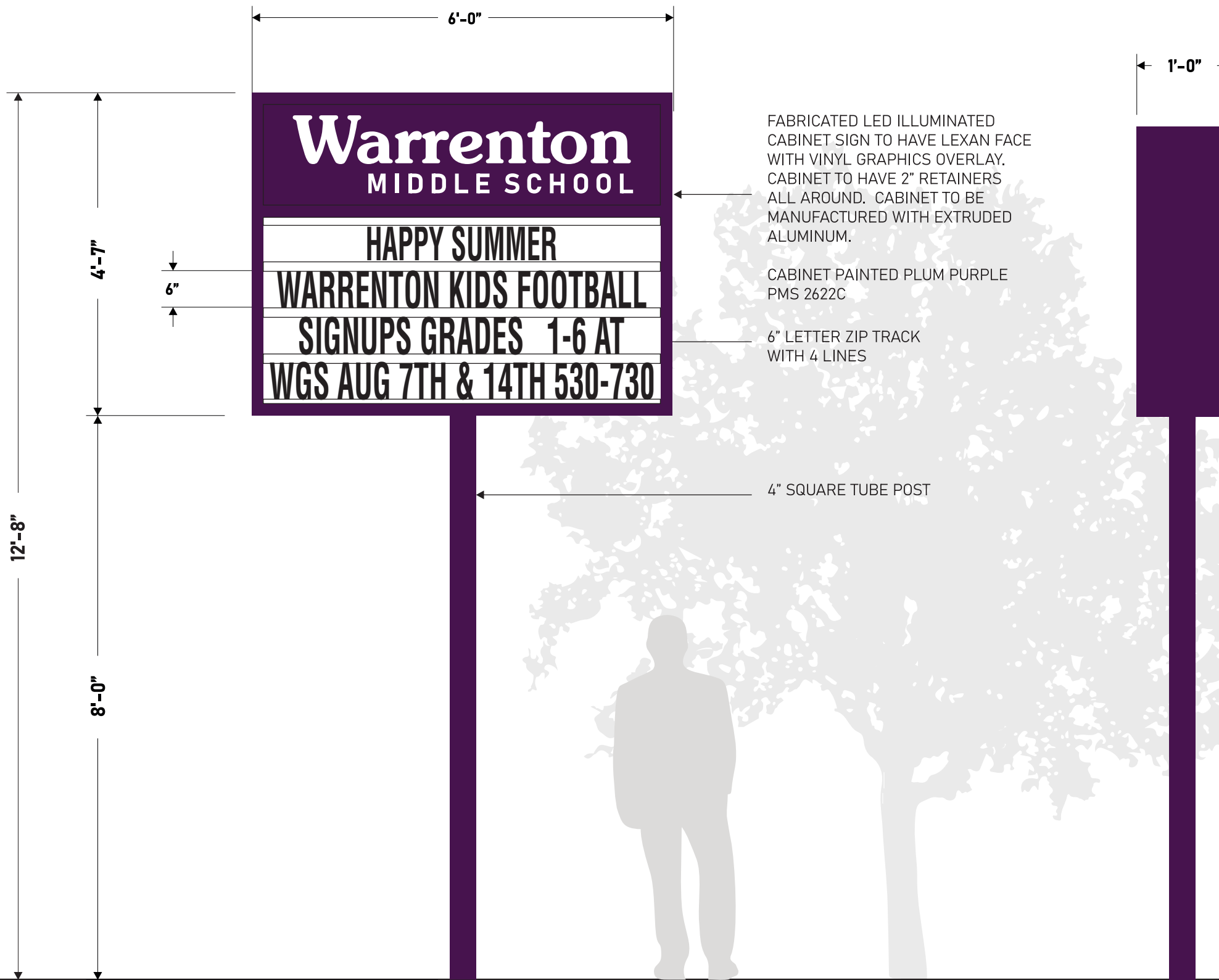


Client

Warrenton
 MIDDLE SCHOOL

Revisions	
Approval	
Client Signature	Date

© Copyright 2014
Rudnick Electric Signs, LLC
 This original design & specifications are the exclusive property of Rudnick Electric Signs LLC. The use of this design to produce a similar sign without written authorization from Rudnick Electric Signs is strictly prohibited.



FABRICATED LED ILLUMINATED CABINET SIGN TO HAVE LEXAN FACE WITH VINYL GRAPHICS OVERLAY. CABINET TO HAVE 2" RETAINERS ALL AROUND. CABINET TO BE MANUFACTURED WITH EXTRUDED ALUMINUM.

CABINET PAINTED PLUM PURPLE PMS 2622C

6" LETTER ZIP TRACK WITH 4 LINES

4" SQUARE TUBE POST

SIGN A

MEASUREMENTS

SCALE: 3/4"=1'-0"



Steven Rudnick
 Phone: 503.655.2610
 Fax: 503.980.7919
steve@rudnickelectricsigns.com
 1625 Washington St.
 Canby, OR 97045
www.rudnickelectricsigns.net

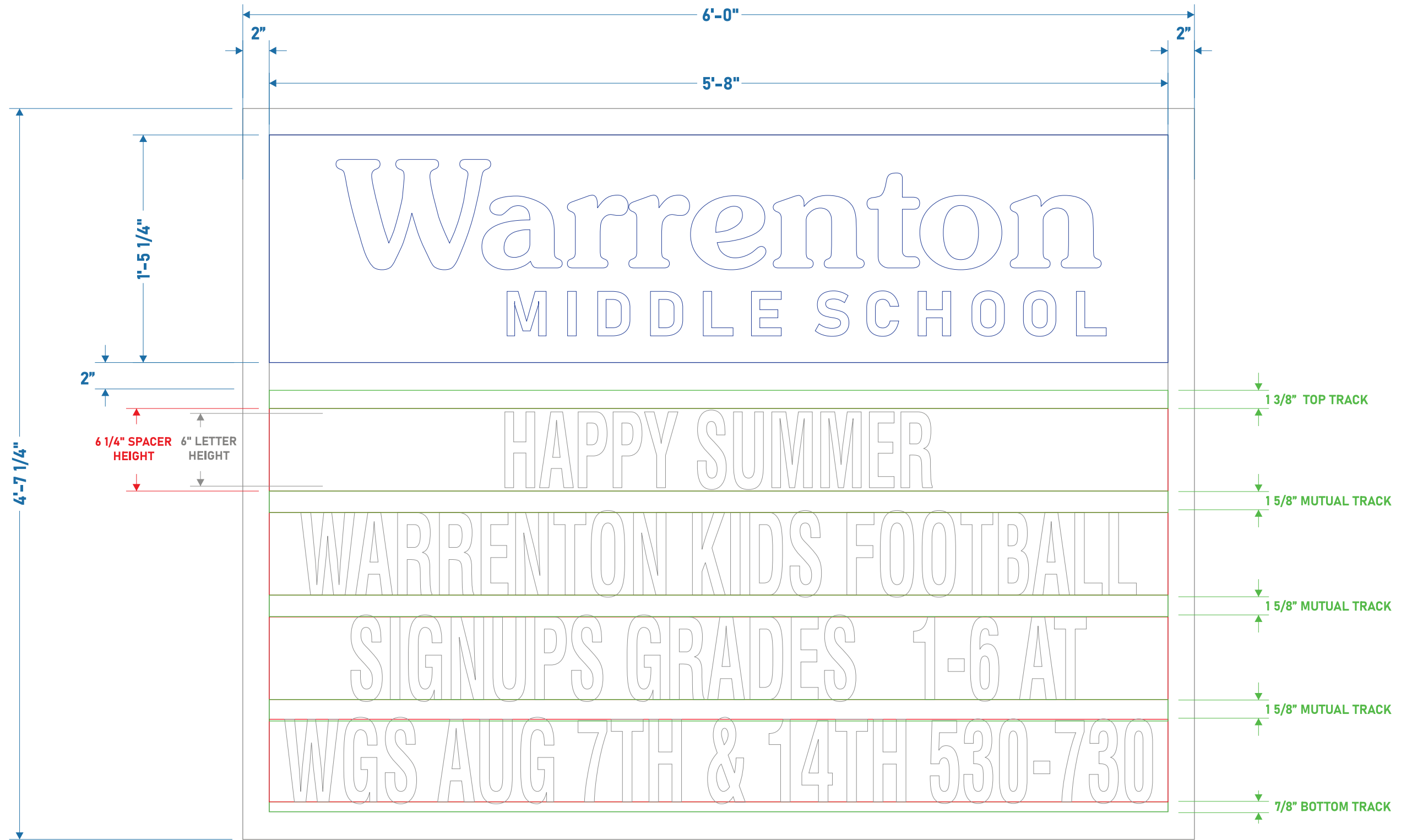


Client

Warrenton
MIDDLE SCHOOL

Revisions	
Approval	
Client Signature	Date

© Copyright 2014
Rudnick Electric Signs, LLC
 This original design & specifications are the exclusive property of Rudnick Electric Signs LLC. The use of this design to produce a similar sign without written authorization from Rudnick Electric Signs is strictly prohibited.



Steven Rudnick
 Phone: 503.655.2610
 Fax: 503.980.7919
steve@rudnickelectricsigns.com
 1625 Washington St.
 Canby, OR 97045
www.rudnickelectricsigns.net



Client

Warrenton
 MIDDLE SCHOOL

Revisions	
Approval	
Client Signature	Date

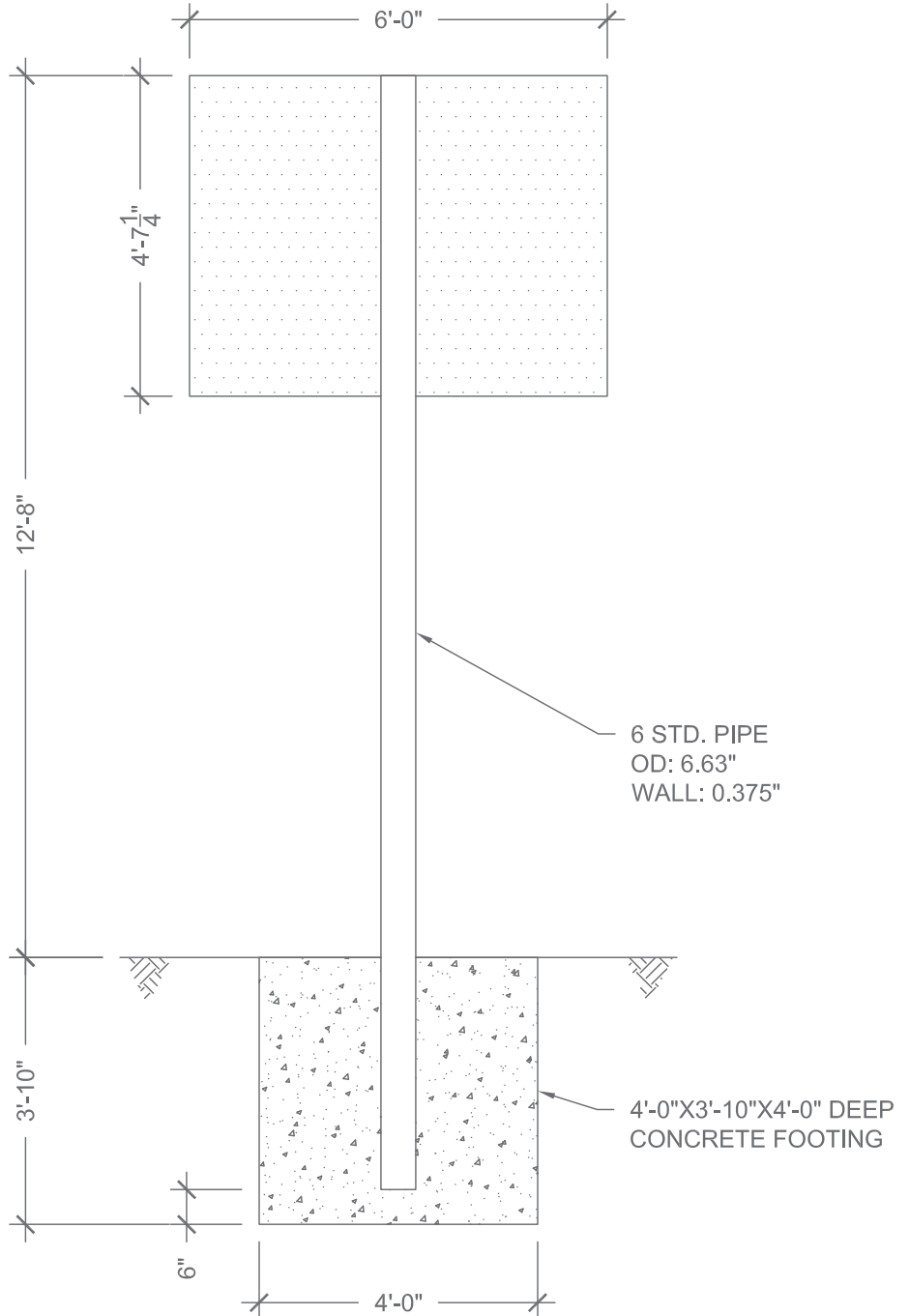
© Copyright 2014
Rudnick Electric Signs, LLC
 This original design & specifications are the exclusive property of Rudnick Electric Signs LLC. The use of this design to produce a similar sign without written authorization from Rudnick Electric Signs is strictly prohibited.



10815 RANCHO BERNARDO ROAD
 SUITE 260, SAN DIEGO, CA 92127
 PROJECTMANAGER@SULLAWAYENG.COM
 PHONE: 1-858-312-5150 FAX: 1-858-777-3534

PROJECT: WARRENTON MIDDLE SCHOOL, SE DOLPHIN AVE, WARRENTON, OR
 PROJECT #: 31786
 CLIENT: RUDNICK ELECTRIC SIGNS LLC

DATE: 8-30-2021
 ENGINEER: MAC
 LAST REVISED:



EXPIRES:06-30-2022

8-30-2021

GENERAL NOTES

1. DESIGN CODE: IBC 2018
2. DESIGN LOADS: ASCE 7-16
3. WIND VELOCITY: 135 MPH EXPOSURE C
4. CONCRETE 2500 PSI MIN.
5. PIPE STEEL ASTM A53, Fy= 35 KSI MIN.
6. PROVIDE PROTECTION AGAINST DISSIMILAR METALS USING ANTI-CORROSIVE PAINT OR NEOPRENE GASKETS.
7. LATERAL SOIL BEARING PER IBC CLASS 4 (150 PSF/FT)
8. ALL DIMENSIONS TO BE VERIFIED PRIOR TO FABRICATION.



FRONT ELEVATION



10815 Rancho Bernardo RD., SD, CA 92127
 projectmanager@sullawayeng.com
 Phone: 858-312-5150 Fax: 858-777-3534

PROJECT: WARRENTON MIDDLE SCHOOL
 PROJ. NO.: 31876
 CLIENT: RUDNICK ELECTRIC SIGNS LLC

DATE: 8/30/21
 ENGINEER: MAC

v5.5

units; pounds, feet unless noted otherwise

Applied Wind Loads; from ASCE 7-16

$F = q_z * G * C_f * A_f$ with $q_z = 0.00256 K_z K_{zt} K_d V^2$ (29.3.2 & 29.4)
 $C_f = 1.779$ (Fig. 29.3-1) 1.00 max. height= 12.67
 $K_{zt} = 1.0$ (26.8.2) (=1.0 unless unusual landscape) $s = 4.60$
 $K_z =$ from table 28.3-1 Exposure= c
 $K_d = 0.85$ for signs (table 26.6-1)
 $V = 135$ mph
 $G = 0.85$ (26.9) weight= 0.317 kips
 $s/h = 0.363$ $M_{DL} = 0.00$ k-ft
 $B/s = 1.30$

Pole Loads	structure component	height at section c.g.	K_z	q_z	pressure $q_z * G * C_f$	A_f	shear	Wind Moment M_w			
	1	4.0	0.850	33.7	50.98	4.0	206	829			
	2	10.3645	0.850	33.7	50.98	27.6	1408	14597			
							sums:	31.7	1614	15.43	(M_w) k-ft arm= 9.6
			$P_u = 0.38$ kip				$M = 15.43$ k-ft	$M = \sqrt{(M_{DL}^2 + M_w^2)}$			
			$M_u = \sqrt{(1.2 M_{DL}^2 + 1.0 M_w^2)} = 15.43$ k-ft								

Pole Design section; pipe

$M_u \leq \phi M_n$ with $M_n = f_y Z$ $f_y = 35$ ksi $\phi = 0.9$

H	M_u (k-ft)	Z req'd. (in)	Size(in)	t (in)	Z	USE
at grade	15.4	5.88	5	0.258	6.8	PIPE 6 STD, $\Phi M_n = 27.8$ k-ft

Footing Design footprint: rectangle

$\omega = 1.3$ IBC 1605.3.2 IBC Table 1806.2, sections 1806.3.4, 1807.3.2 $S = (1.3x2x)$
 $P = 1.26$ kip $S1 = S x d / 3$ $A = 2.34 x P / (S1 x b)$ $S = 400$
 $S1 = 509$ $d = 0.5xA (1 + (1 + 4.36x h/A)^{.5})$ IBC 1807.3.2.1
 $A = 1.02$

footing: **4' - 0" by 4' - 0" 3' - 10" deep**

**APPEAL
APPLICATION**

WARRENTON-HAMMOND SCHOOL DISTRICT NO. 30

820 SW Cedar, Warrenton, OR 97146-9799 • Phone (503) 861-2281 • Fax (503) 861-2911

November 15, 2021

City Of Warrenton, Planning Department

Dear Mr. Hazelton:

Please let this letter serve as the Warrenton-Hammond School District formal appeal of the Administrative Decision for Sign Permit 21-7 dated November 12, 2021.

This appeal is submitted by Mike Moha, Business Manager of Warrenton-Hammond School District as authorized by the Board of Directors for such actions.

Statement Explaining the Specific Issues of the Appeal: The permit application for a sign at the new Warrenton Middle School is being denied in its provision, size, and type based on the high density residential zoning. The District identified a sign as part of its submission through PUD 20-6 and has submitted the required sign permit application. This appeal is to physically allow the sign and at its proposed size and type given the allowed use as a school.

Sincerely,

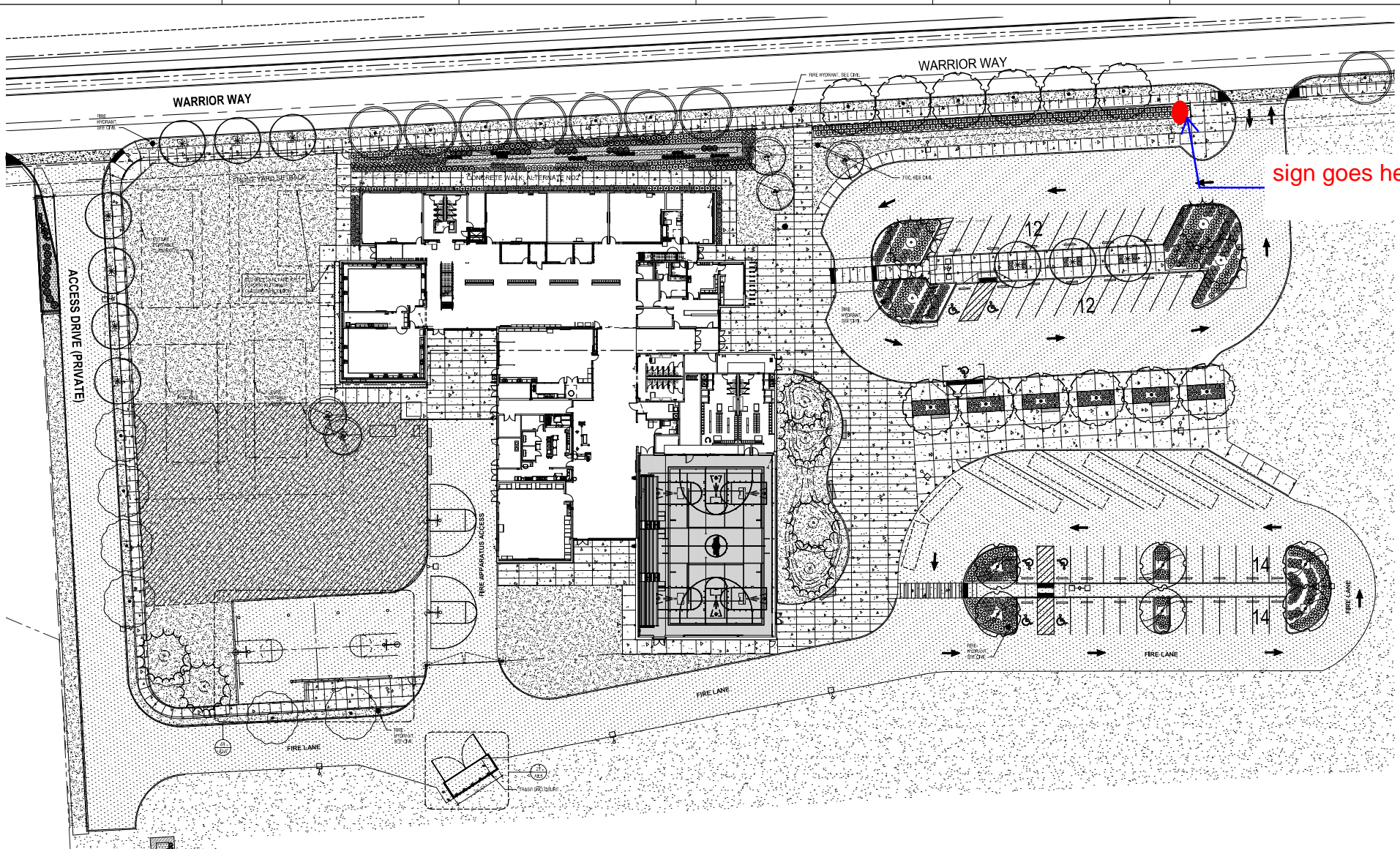


Mike Moha

Business Manager

Warrenton-Hammond School District

SITE DESIGN



ARCHITECTURAL SITE PLAN
 SCALE: 1" = 20'-0"

SITE NOTES

- 1. REFER TO SITE DEVELOPMENT PACKAGE FOR ADDITIONAL INFORMATION.
- GRADING**
 - 2. 2' MINIMUM SPOTS
 - 2. 2' MINIMUM DRIVEWAYS
 - 1. 4' MINIMUM CYCLE PARKING SPOT
- BIKE PARKING**
 - CONCRETE BIKE PARKING = 20
- STAIRS**
 - FRONT YARD = 19

H:\01\2020\07\2020\Warrenton - Hammond\01\2020\Warrenton - Hammond - A0.5.rvt
 7/29/2020 11:46:53 AM

Correspondence

From: [Scott Rose](#)
To: [Scott Hazelton](#)
Cc: [Rick Yeo](#)
Subject: RE: PUD/Master Plan - City denying permit for WMS Sign
Date: Thursday, November 11, 2021 9:56:53 AM
Attachments: [image001.png](#)

Ok. I will see what I can do.

Scott Rose

Senior Project Manager/Leed AP
R&C Management Group LLC
503-312-5404 – cell
www.randcmanagement.com

From: Scott Hazelton <shazelton@ci.warrenton.or.us>
Sent: Thursday, November 11, 2021 9:53 AM
To: Scott Rose <Scott@randcmanagement.com>
Cc: Rick Yeo <Rick@randcmanagement.com>
Subject: RE: PUD/Master Plan - City denying permit for WMS Sign

If they want to be on the December meeting with proper notice time and everything we need their appeal or variance application by next week. I would recommend we meet tomorrow.

Thanks,

Scott Hazelton

Planning Director | City of Warrenton
Office : 503-861-0920
Mobile : 503-440-4082
Fax : 503-861-2351
P.O. Box 250 | 225 S Main Ave
Warrenton, OR 97146
ci.warrenton.or.us | [facebook.com](https://www.facebook.com)



"Making a difference through excellence of service"

This message may contain confidential and/or proprietary information and is intended for the person/entity to which it was originally addressed. If you have received this email by error, please contact the City and then shred the original document. Any use by others is strictly prohibited.

From: Scott Rose <Scott@randcmanagement.com>
Sent: Thursday, November 11, 2021 9:46 AM
To: Scott Hazelton <shazelton@ci.warrenton.or.us>
Cc: Rick Yeo <Rick@randcmanagement.com>
Subject: RE: PUD/Master Plan - City denying permit for WMS Sign

Ok thanks. I will check with them. I am sure they will want a sit down next week.

Best,

Scott

Scott Rose
Senior Project Manager/Leed AP
R&C Management Group LLC
503-312-5404 – cell
www.randcmanagement.com

From: Scott Hazelton <shazelton@ci.warrenton.or.us>
Sent: Thursday, November 11, 2021 9:43 AM
To: Scott Rose <Scott@randcmanagement.com>
Cc: Rick Yeo <Rick@randcmanagement.com>
Subject: RE: PUD/Master Plan - City denying permit for WMS Sign

The district has yet to discuss meeting with me. If they would like to I am available. But as of right now I have not been asked to meet with the district.

Thanks,

Scott Hazelton

Planning Director | City of Warrenton
Office : 503-861-0920
Mobile : 503-440-4082
Fax : 503-861-2351
P.O. Box 250 | 225 S Main Ave
Warrenton, OR 97146
ci.warrenton.or.us | [facebook.com](https://www.facebook.com/cityofwarrenton)



"Making a difference through excellence of service"

This message may contain confidential and/or proprietary information and is intended for the person/entity to which it was originally addressed. If you have received this email by error, please contact the City and then shred the original document. Any use by others is strictly prohibited.

From: Scott Rose <Scott@randcmanagement.com>
Sent: Thursday, November 11, 2021 9:41 AM
To: Scott Hazelton <shazelton@ci.warrenton.or.us>
Cc: Rick Yeo <Rick@randcmanagement.com>
Subject: RE: PUD/Master Plan - City denying permit for WMS Sign

Thank you Scott. This is helpful. I am in Portland so not able to meet. I will share this with the District for your meeting, so they can discuss with you the best course of action.

I will see if Tom wants either Rick or I there too.

Thanks again,

Scott

Scott Rose
Senior Project Manager/Leed AP
R&C Management Group LLC
503-312-5404 – cell
www.randcmanagement.com

From: Scott Hazelton <shazelton@ci.warrenton.or.us>
Sent: Thursday, November 11, 2021 9:28 AM
To: Scott Rose <Scott@randcmanagement.com>
Cc: Rick Yeo <Rick@randcmanagement.com>
Subject: RE: PUD/Master Plan - City denying permit for WMS Sign

An appeal to the planning commission of my administrative decision is how the appeal process would work, which is why I have asked to meet with the district to really discuss all the nuances with each decision. A variance is \$1,000 on top of the \$300 sign permit that would be heard at the December 9th Planning Commission meeting. An appeal is \$500 with the \$300 sign permit that would be heard at the December 9th Planning Commission meeting. A code amendment I am unsure of the cost or timeline on that, but can research that if desired. A rezone is \$2,000 and is the best long term solution due to several factors but it does not appear the school is willing to follow that procedure. I am available today on my mobile if you would like to discuss or I can meet with you.

Thanks,

Scott Hazelton

Planning Director | City of Warrenton

Office : 503-861-0920
Mobile : 503-440-4082
Fax : 503-861-2351
P.O. Box 250 | 225 S Main Ave
Warrenton, OR 97146
ci.warrenton.or.us | [facebook.com](https://www.facebook.com)



“Making a difference through excellence of service”

This message may contain confidential and/or proprietary information and is intended for the person/entity to which it was originally addressed. If you have received this email by error, please contact the City and then shred the original document. Any use by others is strictly prohibited.

From: Scott Rose <Scott@randcmanagement.com>
Sent: Wednesday, November 10, 2021 7:29 PM
To: Scott Hazelton <shazelton@ci.warrenton.or.us>
Cc: Rick Yeo <Rick@randcmanagement.com>
Subject: RE: PUD/Master Plan - City denying permit for WMS Sign

Scott H –

Thank you for your prompt reply. I noted variance just based on your language below where you offered three options: “rezone, variance, or code change”. A variance at this juncture seems to be the quickest and least cost option of the three. It sounded like your rule interpretation was firm, so an appeal was not an option.

If you can send me the City process for filing for a variance and the expected review time, that would be helpful.

Thank you,

Scott R.

Scott Rose
Senior Project Manager/Leed AP
R&C Management Group LLC
503-312-5404 – cell
www.randcmanagement.com

From: Scott Hazelton <shazelton@ci.warrenton.or.us>

Sent: Wednesday, November 10, 2021 8:04 AM
To: Scott Rose <Scott@randcmanagement.com>
Cc: Rick Yeo <Rick@randcmanagement.com>
Subject: RE: PUD/Master Plan - City denying permit for WMS Sign

Just to clarify, you would like to pursue the variance and not the appeal process? I am happy to discuss each option if you would like.

Thanks,

Scott Hazelton

Planning Director | City of Warrenton
Office : 503-861-0920
Mobile : 503-440-4082
Fax : 503-861-2351
P.O. Box 250 | 225 S Main Ave
Warrenton, OR 97146
ci.warrenton.or.us | [facebook.com](https://www.facebook.com)



"Making a difference through excellence of service"

This message may contain confidential and/or proprietary information and is intended for the person/entity to which it was originally addressed. If you have received this email by error, please contact the City and then shred the original document. Any use by others is strictly prohibited.

From: Scott Rose <Scott@randcmanagement.com>
Sent: Wednesday, November 10, 2021 7:55 AM
To: Scott Hazelton <shazelton@ci.warrenton.or.us>
Cc: Rick Yeo <Rick@randcmanagement.com>
Subject: FW: PUD/Master Plan - City denying permit for WMS Sign

Scott Hazelton –

Good morning. As the District's bond project manager, I was forwarded this message. While it seemed reasonable to us to conclude that the land use process that we went through to place a school on this site would allow for all its components, including a basic reader board sign, I must respect your direction here.

Please send me the information regarding your variance process. While I understand that is undesirable from the City's perspective, it is also our quickest and least cost option, which we are

obligated to pursue as a public entity.

Thank you,

Scott

Scott Rose

Senior Project Manager/Leed AP
R&C Management Group LLC
503-312-5404 – cell
www.randcmanagement.com

----- Forwarded message -----

From: **Scott Hazelton** <shazelton@ci.warrenton.or.us>
Date: Tue, Nov 9, 2021 at 4:15 PM
Subject: RE: PUD/Master Plan
To: Mike Moha <moham@warrentonk12.org>
Cc: Linda Engbretson <lengbretson@ci.warrenton.or.us>

Hi Mike,

So before we request payment and accept the application, if that is still what the district wishes to do, I have to say based on what I can find this application would be denied considering the zoning. While I understand the resell concerns of the school and because of that the rezoning option is not desirable I have to abide by the code. The presumption that the sign was approved with the PUD would have been the case if the approval was written differently. Unfortunately everything that I have found states that the sign permit would have to adhere to [Chapter 16.144](#) and [Chapter 16.192](#) both which restricts signs in the RH zoning. A potential for a variance, while not desirable to the city is always an option. The possibility of granting something that is 7x the approved standard could be a concern long term. I know this seems over the top but I do not have the ability to deviate from the code without a variance regardless of who the applicant is. We still have the ability to get approval, either through a rezone, variance, or code change, so I do not want it to seem as if we are permanently denying the sign but without supplementary action the sign would be a code violation and therefore I cannot permit this sign.

I am willing to have another meeting to discuss the options before us and can draft a memo explaining each process if necessary. I hope that this is not seen that the city is being difficult to work with, we just must abide by our duty to process every application fairly regardless of the applicant.

Thanks,

Scott Hazelton

Planning Director | City of Warrenton
Office : 503-861-0920

Mobile : 503-440-4082
Fax : 503-861-2351
P.O. Box 250 | 225 S Main Ave
Warrenton, OR 97146
ci.warrenton.or.us | [facebook.com](https://www.facebook.com)



"Making a difference through excellence of service"

This message may contain confidential and/or proprietary information and is intended for the person/entity to which it was originally addressed. If you have received this email by error, please contact the City and then shred the original document. Any use by others is strictly prohibited.

From: Mike Moha <moham@warrenton12.org>
Sent: Tuesday, November 9, 2021 3:53 PM
To: Scott Hazelton <shazelton@ci.warrenton.or.us>
Subject: Re: PUD/Master Plan

Hi Scott,

Here are pictures of the plans that show the sign in the location we are putting it in. We were and still are under the impression we are okay to put in the sign, with getting the permit.

Thanks,

Mike

On Tue, Nov 9, 2021 at 3:14 PM Scott Hazelton <shazelton@ci.warrenton.or.us> wrote:

Hey Mike,

Were you able to find a copy of the PUD or Master Plan document or plans?

Thanks,

Scott Hazelton

Planning Director | City of Warrenton
Office : 503-861-0920
Mobile : 503-440-4082
Fax : 503-861-2351
P.O. Box 250 | 225 S Main Ave

Warrenton, OR 97146

ci.warrenton.or.us | [facebook.com](https://www.facebook.com)



"Making a difference through excellence of service"

This message may contain confidential and/or proprietary information and is intended for the person/entity to which it was originally addressed. If you have received this email by error, please contact the City and then shred the original document. Any use by others is strictly prohibited.

--

Mike Moha
Business Manager
Warrenton-Hammond School District
moham@warrentonk12.org
503-861-2281

--

Mike Moha
Business Manager
Warrenton-Hammond School District
moham@warrentonk12.org
503-861-2281

From: [Scott Rose](#)
To: [Scott Hazelton](#)
Subject: RE: Appeal Process
Date: Friday, November 12, 2021 12:20:52 PM
Attachments: [image001.png](#)

Thank you Scott.

Scott Rose
Senior Project Manager/Leed AP
R&C Management Group LLC
503-312-5404 – cell
www.randcmanagement.com

From: Scott Hazelton <shazelton@ci.warrenton.or.us>
Sent: Friday, November 12, 2021 11:03 AM
To: Scott Rose <Scott@randcmanagement.com>
Subject: Appeal Process

Hi Scott,

Here is the code process as outlined by the code. Replying to this email with an appeal should wait until I submit the notice of decision, which I can issue as soon as we receive payment and I can get it written. The appeal should come from Tom or Mike's email though as the "property owner" and applicant. This is because technically they are the only ones that have standing with this application. If you have any questions let me know.

D. Appeal. A Type I ministerial decision may be appealed to the Planning Commission as follows:

1. Who May Appeal. The applicant or property owner have legal standing to appeal a Type I ministerial decision.
2. Appeal Procedure.
 - a. Notice of Appeal. Any person with standing to appeal, as provided in paragraph 1 of this subsection, may appeal a Type I ministerial decision by filing a notice of appeal according to the following procedures.
 - i. Time for Filing. A notice of appeal shall be filed with the Community Development Director within 14 days from the date the notice of decision is mailed, or otherwise provided to the applicant, whichever occurs first.
 - ii. Content of Notice of Appeal. The notice of appeal shall contain:
 - (A) An identification of the decision being appealed, including the date of the decision.

(B) A statement demonstrating the person filing the notice of appeal has standing to appeal.

(C) A statement explaining the specific issues raised on appeal.

(D) Filing fee.

iii. The amount of the filing fee shall be established by the City. The maximum fee for an initial hearing shall be the City's cost for preparing and for conducting the hearing, or the statutory maximum, whichever is less.

b. Scope of Appeal. The appeal of a Type I ministerial decision by a person with standing shall be limited to the specific issues raised during the review period, unless the hearings body allows additional evidence or testimony concerning any other relevant issue. The hearings body may allow such additional evidence if it determines that such evidence is necessary to resolve the case. Only in extraordinary circumstances should new issues be considered by the hearings body on appeal of a Type I ministerial decision.

c. Appeal Procedures. Type III notice and hearing procedures shall be used for all Type I ministerial appeals, as provided in Section 16.208.050.

d. Record of the public hearing is subject to Section 16.208.060(M).

Thanks,

Scott Hazelton

Planning Director | City of Warrenton

Office : 503-861-0920

Mobile : 503-440-4082

Fax : 503-861-2351

P.O. Box 250 | 225 S Main Ave

Warrenton, OR 97146

ci.warrenton.or.us | [facebook.com](https://www.facebook.com/cityofwarrenton)



"Making a difference through excellence of service"

This message may contain confidential and/or proprietary information and is intended for the person/entity to which it was originally addressed. If you have received this email by error, please contact the City and then shred the original document. Any use by others is strictly prohibited.

From: [Tom Rogozinski](#)
To: [Scott Hazelton](#)
Cc: [Mike Moha](#); [Linda Engbretson](#); [Scott Rose](#)
Subject: Re: PUD/Master Plan
Date: Wednesday, November 10, 2021 8:15:59 AM
Attachments: [image001.png](#)

Scott:

I am just getting caught up on this email strand.

While I appreciate the detailed explanation, you are correct that this is "over the top" and exceptionally frustrating. My board and community will definitely be vocal in their frustration as they will certainly perceive it as the city choosing to interpret written guidance in an exceptionally restrictive fashion.

My specific question is with your claim that the city does not have the discretion to approve the sign based on the approved master plan. The site design review document (p. 24) specifically states, "A sign permit shall be required if the applicant chooses to add signage on the building or a monument at the front entrance." We are seeking the permit for the sign as per that condition. Is it not logical to believe any sign for a school would be well beyond the size allowed under the residential code? It makes little sense for us to have been advised to get a permit for a sign that would meet the residential code and be basically useless for the purposes of a school.

Our position is that the city has the discretion to approve this permit with no further hoops being jumped, but is simply opting not to.

On behalf of our school board and school community, I ask the city to reconsider its position on this.

Tom Rogozinski
Superintendent
Warrenton Hammond School District
Warrenton, OR
(503) 468-9178

On Tue, Nov 9, 2021 at 7:33 PM Scott Hazelton <shazelton@ci.warrenton.or.us> wrote:

Mike,

That is true, the process on the PUD/SDR was followed correctly, however the sign permit was not part of that. Everything that I have shows that a sign permit would be applied for after the PUD/SDR, as has happened. The issue is that we now have an active permit, whose approval has nothing directly related to the PUD/SDR approval, that would not conform to the zone. If the PUD/SDR had incorporated a statement along the lines of "Signage maybe

placed that matches the other schools” or something to the effect of special permission that does not conform to Chapter 16.144 then it would not be an issue. However that is not the case and I must process this sign application as it pertains to the code currently, not what was previously approved, because a sign permit was not approved.

Again you are welcome to continue with this current sign and appeal staff’s decision. If that is the path you wish to pursue the timeline to appeal by December is shrinking, and the current permit must be paid for before staff can officially review. I would hope that is not the choice that is made, but that is one of the district’s options. A variance, rezone, or code change would be more agreeable to staff but those processes take a greater amount of time. I am happy to come and meet with you and the superintendent to discuss options tomorrow if that is agreeable. I am willing to assist in any way that I can, but the application I have currently before me is in violation of Warrenton Municipal Code.

Thanks,

Scott Hazelton

Planning Director | City of Warrenton

Office : 503-861-0920

Mobile : 503-440-4082

Fax : 503-861-2351

P.O. Box 250 | 225 S Main Ave

Warrenton, OR 97146

ci.warrenton.or.us | [facebook.com](https://www.facebook.com/cityofwarrenton)



“Making a difference through excellence of service”

This message may contain confidential and/or proprietary information and is intended for the person/entity to which it was originally addressed. If you have received this email by error, please contact the City and then shred the original document. Any use by others is strictly prohibited.

From: Mike Moha <moham@warrentonk12.org>
Sent: Tuesday, November 9, 2021 6:30 PM
To: Scott Hazelton <shazelton@ci.warrenton.or.us>
Cc: Linda Engbretson <lengbretson@ci.warrenton.or.us>; Tom Rogozinski <rogozinskit@warrentonk12.org>
Subject: Re: PUD/Master Plan

Scott,

Thank you for the explanation.

Our issue is we did follow the process and did do what we were supposed to do in March 2020 and prior. Why is it an issue now?

On Tue, Nov 9, 2021, 4:47 PM Scott Hazelton <shazelton@ci.warrenton.or.us> wrote:

Mike,

I understand the frustration, the other schools are in the OSI zone as I suggested the new campus be rezoned to. It is required in the conditions of approval to apply for a sign permit, but the sign permit must still be processed according to the code. I cannot simply issue a permit that is a violation of city code regardless of how long it took to get to this position. I have laid out four options for moving forward. I am happy to meet with you, and anyone else, to discuss the four options in greater detail. There is a path forward here but there is additional work that must be done for the sign to be in compliance with city code. Once again I want to stress that I understand the frustration and will be more than willing to assist with the process to get the sign built in compliance with city code.

Thanks,

Scott Hazelton

Planning Director | City of Warrenton

Office : 503-861-0920

Mobile : 503-440-4082

Fax : 503-861-2351

P.O. Box 250 | 225 S Main Ave

Warrenton, OR 97146

ci.warrenton.or.us | [facebook.com](https://www.facebook.com)



“Making a difference through excellence of service”

This message may contain confidential and/or proprietary information and is intended for the person/entity to which it was originally addressed. If you have received this email by error, please contact the City and then shred the original document. Any use by others is strictly prohibited.

From: Mike Moha <moham@warrenton12.org>

Sent: Tuesday, November 9, 2021 4:42 PM

To: Scott Hazelton <shazelton@ci.warrenton.or.us>

Cc: Linda Engbretson <lengbretson@ci.warrenton.or.us>; Tom Rogozinski <rogozinskit@warrenton12.org>

Subject: Re: PUD/Master Plan

Scott,

This is not the outcome we were expecting. Every school has a sign. That is why it was in our plan and that requiring a sign permit was in the conditions of approval. I find it very frustrating that we are in this situation. We are meeting the conditions of our approval and we should be issued the permit without any more hoops to jump through.

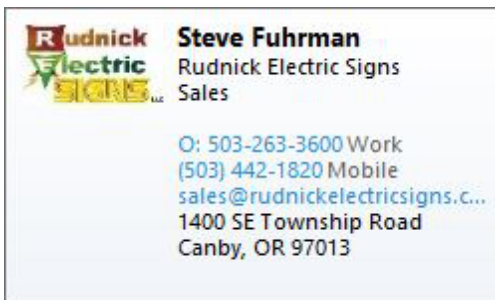
Thank you,

Mike

From: [Sales](#)
To: [Scott Hazelton](#)
Subject: RE: Sign Permit - Warrenton School
Date: Tuesday, October 26, 2021 1:41:34 PM
Attachments: [image003.png](#)
[Application signs 9 28 09.doc](#)
[permit_application - commercial_building.pdf](#)
[Warrenton Middle School 8-23-21.pdf](#)
[LLA.pdf](#)
[31876-ENG-SS.pdf](#)
[WHSD site.pdf](#)
[Steve Fuhrman.vcf](#)

Scott, I have attached the permits and additional information for the new pylon sign for Warrenton School; please e-mail me if additional information is needed.

Thanks



Steve Fuhrmann
Rudnick Electric Signs
1400 SE Township RD, Canby OR 97013
O. 503-263-3600
c) 503-442-1820
sales@rudnickelectricsigns.com

From: Scott Hazelton <shazelton@ci.warrenton.or.us>
Sent: Tuesday, October 26, 2021 12:49 PM
To: Sales <sales@rudnickelectricsigns.com>
Subject: Sign Permit

Thanks,

Scott Hazelton

Planning Director | City of Warrenton
Office : 503-861-0920
Mobile : 503-440-4082
Fax : 503-861-2351
P.O. Box 250 | 225 S Main Ave

Warrenton, OR 97146

ci.warrenton.or.us | [facebook.com](https://www.facebook.com/cityofwarrenton)



“Making a difference through excellence of service”

This message may contain confidential and/or proprietary information and is intended for the person/entity to which it was originally addressed. If you have received this email by error, please contact the City and then shred the original document. Any use by others is strictly prohibited.

Notice of Decision



**NOTICE OF ADMINISTRATIVE DECISION
SIGN PERMIT 21-7**

DATE OF DECISION: November 12th, 2021

SUBJECT OF REVIEW: 1050 SE Warrior Way Warrenton, OR

APPLICANT &

PROPERTY OWNER: Mike Moha, Warrenton Hammond School District 30

Mike Moha, on behalf of Warrenton Hammond School District 30 has submitted an application for a sign on Tax Lot 810340000103. The subject property is located adjacent at 1050 SE Warrior Way Warrenton OR in the RH, High Density Residential zoning district.

Applicable criteria from the Warrenton Municipal Code for this application are:

Chapter 16.36	High Density Residential (RH) District
Chapter 16.144	Signs

Previous Land Use Actions & Existing Conditions

The subject property includes the Warrenton Hammond Middle School. The use of the property is allowed through PUD/SDR 20-6. The area is zoned RH, High Density Residential.

FINDINGS

1. The subject property is zoned High Density (R-H) Residential, however the School use is allowed through PUD 20-6.
2. The proposed sign is outside of the size and type allowed in the R-H Zone.
3. PUD 20-6 required that a sign permit be submitted as a condition of approval if a sign was desired.

DECISION

1. This site design review application is **DENIED** based on the findings above, which address the required criteria for a sign on the property.

This decision can be reviewed or a copy obtained at the Community & Economic Development Department, Warrenton City Hall, Warrenton. The Community Development Director's **decision is final** unless appealed to the Warrenton Planning Commission pursuant to WMC 16.208.030. Individuals who may appeal this decision are the applicant, any person who was mailed written notice of the administrative decision, and any other person who participated in the proceeding by submitting written comments, or who is otherwise adversely affected or aggrieved by the decision.

Scott Hazelton, CFM, Planning Director

11-12-21

Date