

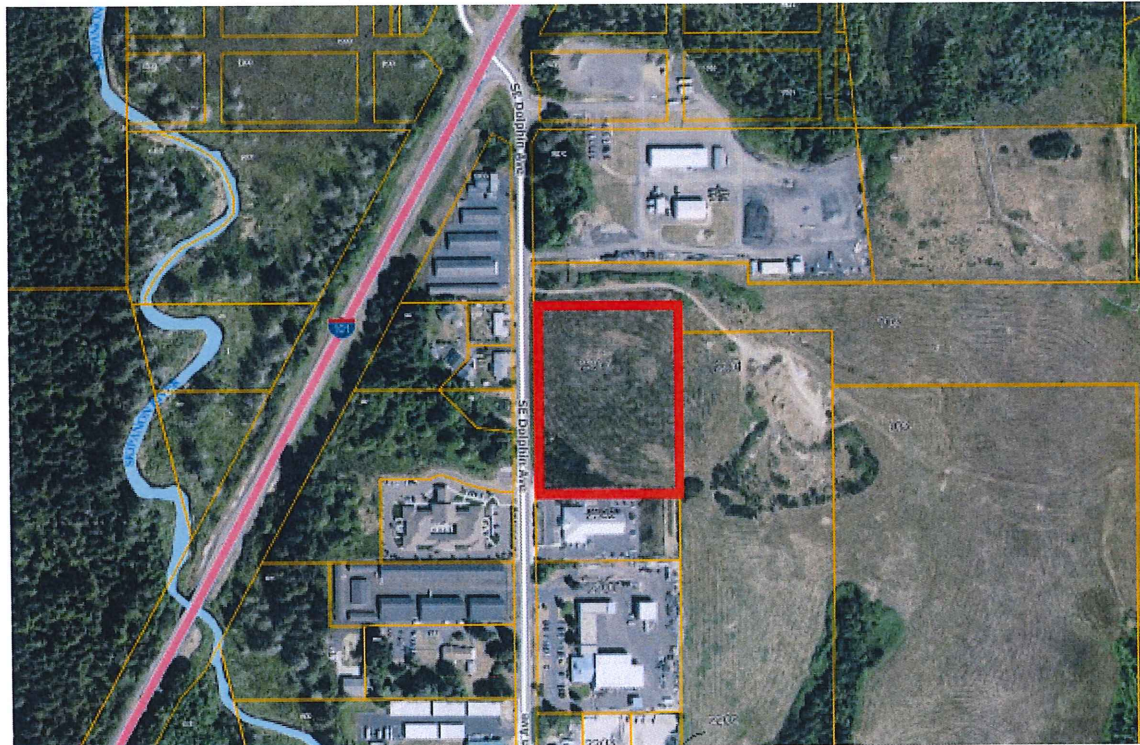
September 2, 2020

TO: Warrenton Planning Commission
FROM: Mark Barnes, Interim City Planner
RE: Site Design Review Application, SDR 20-05 and VAR 20-04, NW Natural Gas

Recommendation and Background

NW Natural Gas seeks site design review for new construction on SE Dolphin Avenue. The request also includes a variance to the City’s fence height limit to allow a 7-foot fence around the site’s perimeter. Staff recommends approval of the proposed site plan subject to conditions listed at the end of this staff report; and approval of the proposed variance.

This vacant 5.3-acre parcel is located on map/taxlot 8103402300, on the east side of SE Dolphin Avenue, immediately south of the Warrenton School District access road. The subject property is outlined in red on the aerial photograph below.



The property is zoned I-1 General Industrial, which allows “public utility facilities” as an outright use under 16.60.020.E. Northwest Natural Gas is a regulated public utility providing natural gas in Warrenton. The proposed development includes vehicle and equipment parking, equipment and materials storage, and maintenance facilities, parking, office/administrative space, and landscaping. Site design review by the Planning Commission is required under 16.212.040.A.1.b because the site is larger than two acres and involves more than 10,000 square feet of gross floor area.

Development Process & Review Timeline

A pre-application conference was held May 13, 2020, and notes were shared with the applicant in a memo dated May 20, 2020. Application materials were received July 7, 2020, and deemed complete July 16, 2020. The application was amended to include a variance on August 14, 2020.

Public notice was sent to adjacent property owners and interested parties on July 24 and August 21, 2020; and published in *The Columbia Press* on July 31, 2020, and again on August 28, 2020. No written public comments were received as of the date of this staff report. Affected agency notice was emailed on August 20, 2020. No comments have been received as of the date of this staff report.

The application is subject to the timelines established in ORS 227.178, requiring final City action on an application within 120 of completeness. The amended application was determined to be complete on August 20, 2020; so final action, including appeals if any, must be taken no later than December 18, 2020.

Existing Conditions

Staff conducted a site visit prior to the pre-application conference, and again on July 22, 2020. The site is vacant. Dolphin Avenue frontage lacks curbs or a sidewalk. Runoff is conveyed to the west, via a culvert under Dolphin Avenue.

Applicable Criteria

The proposal is a permitted use in the General Industrial (I1) zone under 16.60.020.E as a "public utility facility". NWNatural is a regulated public utility in Oregon that distributes and sells natural gas. The proposal is subject to the following Warrenton Development Code zoning and design standards and requirements:

- I-1 General Industrial zone development standards (16.60.040)
- Design Standards: Access & Circulation (16.120)
- Design Standards: Landscaping, Street Trees, Fences, and Walls (16.124)
- Design Standards: Vehicle & Bicycle Parking (16.128)
- Design Standards: Clear Vision Areas (16.132)
- Public Facilities Standards (16.136)
- Stormwater & Surface Water Management Standards (16.140)
- Large Scale Development (16.192)
- Site Design Review Application & Review Procedures (16.212)
- Variances (16.272)

16.60.040 -- I1 Zone Development Standards.

Air Quality. The air quality standards set by the Department of Environmental Quality shall be the guiding standards in this zone, except that open burning is prohibited in any case. (16.60.040.A.)

No open burning areas are shown on the site plan.

Noise. As may be permitted under all applicable laws and regulations. (16.60.040.B.)

The proposed use is not expected to generate noise other than that associated with trucks and related equipment.

Storage. All materials, including wastes, shall be stored and maintained in a manner that will not attract or aid the propagation of insects or rodents or other animals or birds, or otherwise create a health hazard or nuisance. (16.60.040.C.)

Storage areas are shown on the proposed site plan.

Fencing. Will be allowed inside a boundary planting screen and where it is necessary to protect property of the use concerned or to protect the public from a dangerous condition. Proposed fence locations and design shall be subject to City review. (16.60.040.D.)

The proposed site plan shows fencing around the entire site perimeter. The applicant has requested a variance to the City's six-foot limit on fence height. Variance criteria are reviewed elsewhere in this staff report.

Buffer. Where this zone adjoins another non-industrial zone there shall be a buffer area at least 10 feet wide to provide a dense evergreen landscape buffer which attains a mature height of eight feet, or such other screening measures as may be prescribed by the City in the event differences in elevation or other circumstances should defeat the purpose of this requirement. (16.60.040.E.)

Adjoining property to the immediate north is in the High Density Residential (RH) zone. This parcel, where it adjoins the subject property, consists of a relatively narrow (115 feet wide) strip of residentially-zoned land where Warrior Way will be built. As such it will not accommodate residential development. Based on this, the Planning Commission can find that the buffer requirement in 16.60.040.E, excerpted above, does not apply to this proposal.

Vibration. No vibration other than that caused by highway vehicles, trains and aircraft shall be permitted which is discernible without instruments at the property line of the use concerned. (16.60.040.F.)

The proposed use does not involve any industrial activities that would cause vibrations other than those related to motor vehicles.

Airport Interference. No use shall create electrical or lighting interference with the operations of the Port of Astoria Airport. (16.60.040.G.)

The project site is about two miles southwest of the airport, and not aligned with either of the airport's two active runways. Proposed exterior lighting will be downcast.

Setbacks. The minimum front, side and rear yard setbacks shall be 10 feet. When across a street from a non-industrial zone, the setback from the property line shall be 10 feet.

The building closest to a property line is the office, located about 25 feet from the west property line.

All development shall comply with the wetland and riparian area protection standards of Chapter 16.156. (16.60.040.I.)

The City's local wetland inventory does not indicate any wetlands or riparian areas on or adjoining the subject property. The site plan identifies a swale on the site's south side. A visual inspection (7/22) identified riparian vegetation in this swale. This riparian corridor is not identified in chapter 16.156 as among the regulated riparian corridors in Warrenton. Consequently, the protection standards in 16.156 are not applicable to this site.

Building Height. The maximum building height shall be 45 feet, except that it may be lower under either of the following circumstances:

- 1. Within 100 feet of a non-industrial zone, where the maximum building height shall be the same as the maximum building height in that zone. (16.60.040.J.)*

The tallest proposed building is 28 feet above grade. The C1 zone, across SE Dolphin Avenue and within 100 feet of the site, has a maximum building height of 45 feet.

Based on this, the Planning Commission should find that the proposed site plan meets, or can meet subject to the approval of a fence height variance, the I1 zone development standards in 16.60.040.

16.120.020 -- VEHICULAR ACCESS AND CIRCULATION

F. Access Options. When vehicle access is required for development (i.e., for off-street parking, delivery, service, drive-through facilities, etc.), access shall be provided by one of the following methods (a minimum of 10 feet per lane is required). These methods are "options" to the developer/subdivider, unless one method is specifically required under Division 2, or through conditions required by the hearings body.

Option 1. Access is from an existing or proposed alley or mid-block lane. If a property has access to an alley or lane, direct access to a public street is not permitted.

Option 2. Access is from a private street or driveway connected to an adjoining property that has direct access to a public street (i.e., "shared driveway"). A public access easement covering the driveway shall be recorded in this case to assure access to the closest public street for all users of the private street/drive.

Option 3. Access is from a public street adjacent to the development parcel. If practicable, the owner/developer may be required to close or consolidate an existing access point as a condition of approving a new access. Street accesses shall comply with the access spacing standards in subsection G of this section, and require an access permit in accordance with subsection C of this section.

Access is proposed from SE Dolphin Avenue. The proposed site plan uses an "option 3" design. The subject property has frontage on and direct access to Dolphin Avenue. The proposed site design uses two driveways across the site's 550 feet of street frontage.

G. Access Spacing. Driveway accesses shall be separated from other driveways and street intersections in accordance with the following standards and procedures:

1. Local Streets. A minimum of 25 feet separation (as measured from the sides of the driveway/street) shall be required on local streets (i.e., streets not designated as collectors or arterials) for all single-family detached dwellings, except as provided in paragraph 3 of this subsection. A minimum of 20 feet separation shall be required on local streets for all single-family attached dwellings, duplexes, and triplexes, except as provided in paragraph 3 of this subsection.

2. Arterial and Collector Streets. ...

3. *Special Provisions for All Streets.* Direct street access may be restricted for some land uses, in conformance with the provisions of Division 2, Land Use Districts. For example, access consolidation, shared access, and/or access separation greater than that specified by paragraphs 1 and 2 of this subsection, may be required by the City, County or ODOT for the purpose of protecting the function, safety and operation of the street for all users. (See subsection I of this section.) Where no other alternatives exist, the permitting agency may allow construction of an access connection along property line farthest from an intersection. In such cases, directional connections (i.e., right in/out, right in only, or right out only) may be required.

4. *Corner Clearance.* The distance from a street intersection to a driveway or other street access shall meet or exceed the minimum spacing requirements for the street classification in the Warrenton TSP.

SE Dolphin is classified as a "local street" in the City's Transportation System Plan. The spacing requirements in G.1, above, do not apply to this proposal; they apply only to residential developments. The I1 zone has no spacing standards for industrial driveways. The nearest street intersection is the corner of Dolphin Avenue and Warrior Way, near the northwest corner of the subject property. The site's northern driveway is about 40 feet from this intersection; however, the final intersection configuration has not yet been constructed. The southern driveway is more than 200 feet from the Warrior/Dolphin intersection. The Transportation System Plan (2018 update) recommends 15 feet of spacing between access points (Table 11, page 40). As shown on the proposed site plan, the proposed driveways meet the TSP's applicable spacing standard.

H. *Number of Access Points.* The number of street access points for multiple family, commercial, industrial, and public/institutional developments shall be minimized to protect the function, safety and operation of the street(s) and sidewalk(s) for all users. Shared access may be required, in conformance with subsection I of this section, in order to maintain the required access spacing, and minimize the number of access points.

The proposed site plan shows two access points across approximately 500 feet of frontage.

I. *Shared Driveways.* The number of driveway and private street intersections with public streets shall be minimized by the use of shared driveways with adjoining lots where feasible. The City shall require shared driveways as a condition of land division, development review, or site design review, as applicable, for traffic safety and access management purposes in accordance with the following standards: ...

The proposed driveways are not shared. The adjacent use to the immediate south, Oregon State Police, has unique security requirements that are not consistent with a shared driveway.

K. Driveway Openings and Widths. Driveway openings (or curb cuts) shall be the minimum width necessary to provide the required number of vehicle travel lanes (10 feet for each travel lane). The following standards (i.e., as measured where the front property line meets the sidewalk or right-of-way) are required to provide adequate site access, minimize surface water runoff, and avoid conflicts between vehicles and pedestrians. ... Access widths for all other uses shall be based on 10 feet of width for every travel lane, except that driveways providing direct access to parking spaces shall conform to the parking area standards in Chapter 16.128.

Two formal driveways are proposed. Both meet width and distance standards.

8. Loading Area Design. The design of driveways and on-site maneuvering and loading areas for commercial and industrial developments shall consider the anticipated storage length for entering and exiting vehicles, in order to prevent vehicles from backing into the flow of traffic on the public street or causing unsafe conflicts with on-site circulation.

NWNatural has special loading requirements designed into the proposed site plan. The proposed aisles appear to be adequate for the types of trucks used by NWNatural.

L1. Required Access. A fire equipment access drive that meets City construction standards shall be provided for any portion of an exterior wall of the first story of a building that is located more than 150 feet from an improved public street or approved fire equipment access drive. Plans for fire apparatus access roads shall be submitted to the Warrenton Fire Department and Warrenton City-appointed engineer for review and approval prior to issuance of building permits, grading permits, or start of construction. When fire apparatus access road(s) are required, the road(s) shall be installed and made serviceable prior to and during time of construction. Fire department access roads shall be provided and maintained in accordance with the fire department access requirements of the Uniform Fire Code, as amended.

The site plan is under review by the Fire Department; their comments, if any, will be provided separately.

M. Vertical Clearances. Driveways, private streets, aisles, turn-around areas and ramps shall have a minimum vertical clearance of 13 feet 6 inches for their entire length and width.

It does not appear from the site plan that any architectural or landscape features are proposed that would interfere with meeting this standard.

N. Vision Clearance. No signs, structures or vegetation in excess of three feet in height shall be placed in vision clearance areas, as shown in Figure 16.120.020.N. The minimum vision clearance area may be increased by the Community Development Director, City-appointed engineer, or Planning Commission upon finding that more sight distance is required (i.e., due to traffic speeds, roadway alignment, etc.).

Based on a review of the site plan, there are no materials proposed within the clearance areas at the property line boundary. The proposed buildings do not appear to be within the vision clearance area.

O.1. Surface Options. All driveways, parking areas, aisles, and turn-a-rounds in the City of Warrenton shall be paved with asphalt, concrete, or other comparable surfacing.

All driveways, parking areas, aisles and turn-arounds shown on the proposed site plan are paved.

O.2. Surface Water Management. All driveways, parking areas, aisles and turn-a-rounds shall have on-site collection or infiltration of surface waters to eliminate sheet flow of such waters onto public rights-of-way and abutting property. Surface water facility plans shall be prepared by a qualified person and constructed in conformance with City standards. Such plans shall attempt to follow the principle that water falling on a given site should be absorbed or retained on-site to the extent that the quantity and rate of water leaving the site after the development would not be significantly different than if the site had remained undeveloped.

Runoff is directed to a swale on the south side of the property, and then into a natural drainage that passes beneath SE Dolphin to the Highway 101 right-of-way, where it is conveyed south into the Skipanon River. A stormwater plan is included in the application materials.

16.136.020 -- Transportation Standards.

A. Development Standards. No development shall occur unless the lot or parcel abuts a public or private street, other than an alley, for at least 25 feet and is in conformance with the provisions of Chapter 16.120, Access and Circulation, and the following standards are met:

1. Streets within or adjacent to a development shall be improved in accordance with the Comprehensive Plan, Transportation System Plan, and the provisions of this chapter;
2. Development of new streets (public or private), and additional street width or improvements planned as a portion of an existing street, shall be improved in accordance with this section, and public streets shall be dedicated to the applicable City, County or State jurisdiction;
3. New streets and drives connected to a City collector or arterial street shall be paved; and
4. The City may accept a future improvement guarantee [e.g., owner agrees not to remonstrate (object) against the formation of a local improvement district in the future] in lieu of street improvements if one or more of the following conditions exist:
 - a. A partial improvement may create a potential safety hazard to motorists or pedestrians,
 - b. Due to the developed condition of adjacent properties it is unlikely that street improvements would be extended in the foreseeable future and the improvement associated with the project under review does not, by itself, provide increased street safety or capacity, or improved pedestrian circulation,
 - c. The improvement would be in conflict with an adopted capital improvement plan, or
 - d. The improvement is associated with an approved land partition on property zoned residential and the proposed land partition does not create any new streets.

Based on the proposed site plan, the Planning Commission should find the proposal consistent with the requirements of 16.136.020.A.

J. Sidewalks, Planter Strips, Bicycle Lanes. Sidewalks, planter strips, and bicycle lanes shall be installed in conformance with the standards in Table 16.136.010, applicable provisions of the Transportation System Plan, the Comprehensive Plan, and adopted street plans. Sidewalks are required on both sides of all streets (including streets inside easements), except for alley ways and walking paths. Maintenance of sidewalks, curbs, and planter strips is the continuing obligation of the adjacent property owner.

The proposed site plan includes a sidewalk and street trees along the site's Juniper Avenue frontage; and bike racks on the east side of the proposed office building. As a local street, dedicated bicycle lanes are not required on Juniper Avenue. Based on this, the Planning

Commission should find the proposal site plan consistent with the requirements of 16.136.020.J.

16.120.030 -- Pedestrian Access and Circulation

A.1. Continuous Pathways. The pathway system shall extend throughout the development site, and connect to all future phases of development, adjacent trails, public parks and open space areas whenever possible. The developer may also be required to connect or stub pathway(s) to adjacent streets and private property, in accordance with the provisions of Section 16.120.020, Vehicular Access and Circulation, and Chapter 16.136, Public Facilities Standards.

A.2. Safe, Direct, and Convenient Pathways. Pathways within developments shall provide safe, reasonably direct and convenient connections between primary building entrances and all adjacent streets and existing or planned transit stops, based on the following definitions:

a. Reasonably Direct. A route that does not deviate unnecessarily from a straight line or a route that does not involve a significant amount of out-of-direction travel for likely users.

b. Safe and Convenient. Bicycle and pedestrian routes that are reasonably free from hazards and provide a reasonably direct route of travel between destinations.

c. For commercial, industrial, mixed use, public, and institutional buildings, the "primary entrance" is the main public entrance to the building. In the case where no public entrance exists, street connections shall be provided to the main employee entrance.

A.3. Connections Within Development. For all developments subject to site design review, pathways shall connect all building entrances to one another. In addition, pathways shall connect all parking areas, storage areas, recreational facilities and common areas (as applicable), and adjacent developments to the site, as applicable.

The proposed site plan shows a system of striped walkways, sidewalks, and crosswalks extending throughout the site.

B.1. Vehicle/Pathway Separation. Where pathways are parallel and adjacent to a driveway or street (public or private), they shall be raised six inches and curbed, or separated from the driveway/street by a five-foot minimum strip with bollards, a landscape berm, or other physical barrier. If a raised path is used, the ends of the raised portions must be equipped with curb ramps.

B.3. *Crosswalks.* Where pathways cross a parking area, driveway, or street ("crosswalk"), they shall be clearly marked with contrasting paving materials, humps/raised crossings, or painted striping. An example of contrasting paving material is the use of a concrete crosswalk through an asphalt driveway. If painted striping is used, it shall consist of thermo-plastic striping or similar type of durable application.

B.4. *Pathway Surface.* Pathway surfaces shall be concrete, asphalt, brick/masonry pavers, or other durable surface, at least six feet wide, and shall conform to ADA requirements. Multi-use paths (i.e., for bicycles and pedestrians) shall be the same materials, at least six feet wide. (See also Chapter 16.136, Public Facilities Standards, for public multi-use pathway standards.)

B.5. *Accessible Routes.* Pathways shall comply with the Americans with Disabilities Act, which requires accessible routes of travel.

The proposed site plan appears to meet these standards.

Chapter 16.124 -- LANDSCAPING, STREET TREES, FENCES AND WALLS

16.124.070.B. *Landscaping Plan Required.* For every new development in the City of Warrenton requiring a City permit, a landscape plan is required. All landscape plans shall include the following minimum required details (see Section 16.212.040 for additional landscape plan requirements for projects requiring site design review)

The proposal includes a landscape plan on sheet L2.

16.124.070.C. *Landscape Area Standards.* The minimum percentage of required landscaping equals: ... 3. *General industrial districts: a minimum of 20% of the site shall be landscaped.*

The proposed landscape plan shows that interior landscaping will cover 110,625 square feet of the 231,847 square foot site, or about 48 percent of the site. These figures include 70,331 square feet of undeveloped open space on the site.

16.124.070.E.3. *Buffering and Screening Required.* Buffering and screening are required under the following conditions:
a. *Parking/Maneuvering Area Adjacent to Streets and Drives.* Where a parking or maneuvering area is adjacent and parallel to a street or driveway, a decorative wall (masonry or similar quality material), arcade, trellis, evergreen hedge, or similar screen shall be established parallel to the street or driveway. The required wall or screening

shall provide breaks, as necessary, to allow for access to the site and sidewalk by pedestrians via pathways. The design of the wall or screening shall also allow for visual surveillance of the site for security. Evergreen hedges used to comply with this standard shall be a minimum of 36 inches in height at maturity, and shall be of such species, number and spacing to provide the required screening within one year after planting. Any areas between the wall/hedge and the street/driveway line shall be landscaped with plants or other groundcover. All walls shall be maintained in good condition, or otherwise replaced by the owner.

b. Parking/Maneuvering Area Adjacent to Building. Where a parking or maneuvering area, or driveway, is adjacent to a building (other than a single-family, two-family, or three-family dwelling), the area shall be separated from the building by a raised pathway, plaza, or landscaped buffer not less than four feet in width. Raised curbs, bollards, wheel stops, or other design features shall be used to protect buildings from being damaged by vehicles. When parking areas are located adjacent to residential ground-floor living space (except for a single-family residence, duplex, and triplex), a landscape buffer is required to fulfill this requirement.

c. Screening of Mechanical Equipment, Outdoor Storage, Service and Delivery Areas, and Automobile-Oriented Uses. All mechanical equipment, outdoor storage and manufacturing, and service and delivery areas, shall be screened from view from all public streets and residential districts. Screening shall be provided by one or more the following: decorative wall (i.e., masonry or similar quality material), evergreen hedge, non-see-through fence, or a similar feature that provides a non-see-through barrier. Walls, fences, and hedges shall comply with the vision clearance requirements and provide for pedestrian circulation, in accordance with Chapter 16.120, Access and Circulation.

The proposed landscaping plan appears to provide adequate screening along Juniper Avenue to meet the requirements of subsections a and b. A trash enclosure and a generator are screened from Juniper Avenue by fencing, meeting the requirements of subsection c. Note that the trash enclosure as shown on the site plan does not appear to meet city standards (A.M. Engineering letter of August 25, 2020): see recommended condition 3.

Chapter 16.128 -- VEHICLE AND BICYCLE PARKING

16.128.030.A. General Provisions.

- 1. Groups of four or more off-street parking spaces shall be served by a driveway or aisle so that no backing movements or other maneuvering within a street or right-of-way, other than an alley, will be required. Section 16.120.020 contains driveway opening and width standards.*
- 2. Service drives or aisles to off-street parking areas shall be designed and constructed to facilitate the flow of traffic and to provide maximum safety to pedestrian, bicycle, and vehicular traffic on the site.*

3. *Service drives or aisles shall be clearly and permanently marked and defined through the use of bumper rails, fences, buildings, walls, painting, or other appropriate markers.*
4. *Fractional space requirements shall be counted as a whole space.*
5. *All parking lots shall be designed and constructed to meet the City standards of Section 16.120.020, this chapter, Chapter 16.136, and this Code.*

Proposed off-street parking is tabulated on sheet L1:

Staff parking: 21 spaces plus 1 van-accessible ADA space
Fleet parking: 26 spaces
Oversize (12'x60') fleet parking: 5 spaces

Minimum off-street parking requirements for public utilities, from the table in WMC 16.128.030.A, are: 1 space per 2 employees on the largest shift, plus 1 space per company vehicle. The 31 spaces of fleet parking presumably meet the parking needs for company-owned vehicles. The remaining 22 spaces are sufficient to meet the needs of up to 42 employees. The applicant has indicated that on-site employment is far less than this.

16.128.040.A. All uses shall provide bicycle parking in conformance with the following standards which are evaluated during development review or site design review.

The minimum bicycle parking standard for "other uses" from the table at 16.128.040.A is "2 spaces per primary use or 1 per 10 vehicle spaces, whichever is greater". Based on the proposed staff parking figure of 22 spaces, the long-term and short-term bicycle parking shown on sheet L1 meets or exceeds this standard.

Chapter 16.140 -- STORMWATER AND SURFACE WATER MANAGEMENT

The applicant provided a stormwater report with the request. The Public Works Department reviewed the report, and asked for additional information (A.M. Engineering letter of August 25, 2020). The Planning Commission can either delay approval until these deficiencies are corrected; or approve the site plan subject to a condition: *The applicant will update the stormwater report to address the deficiencies noted in the August 25, 2020, A.M. Engineering letter.* See proposed condition 1.

Chapter 16.144 -- SIGNS

No signage proposal was included with submitted materials. Any signage installed at the site shall be submitted for review prior to installation for compliance with 16.444 sign codes. See proposed approval condition 2.

Chapter 16.212 -- Site Design Review Application and Review Procedures

The proposal falls under a Type 3 public hearing for industrial development of more than two acres. The site plan submitted includes the required info.

Chapter 16.192 -- LARGE-SCALE DEVELOPMENTS

This proposal is subject to the large-scale development standards in chapter 16.192 because it involves industrial development on a two-acre or larger site. This chapter establishes standards for soil stability (16.192.030), stormwater management (16.192.040), utilities (16.192.050), schools (16.192.060), landscape suitability (16.192.070), and signs (16.192.080). These requirements are addressed in the application materials, and elsewhere in this staff report. Subject to the recommended approval conditions at the end of this staff report, the Planning Commission can find that the proposal meets the procedural and substantive requirements of chapter 16.192.

Chapter 16.272 -- VARIANCES

The applicant has proposed a variance to the maximum fence height standard in 16.124.050.B.2:

Fences in the General Industrial (I-1) Zone may exceed six feet if necessary to protect the welfare of the general public (i.e., airport runway safety, military, coast guard, or homeland security defense facilities, etc.) but not for protection of private property (i.e., auto repair lots, equipment yards, woodworking shops, etc.). Barbed and razor wire fencing is prohibited in all zones except as necessary to enclose livestock or to protect the welfare of the general public (not private property).

The purpose of the variance is to install a seven-foot fence for protection of NWNatural's equipment and supplies. Variance criteria in 16.272.020 are excerpted here, followed by proposed findings:

- A. The hardship was not created by the person requesting the variance;*
- B. The request is necessary to make reasonable use of the property. There will be an unreasonable economic impact upon the person requesting the variance if the request is denied;*
- C. The request will not substantially be injurious to the neighborhood in which the property is located. The variance will not result in physical impacts, such as visual, noise, traffic or increased potential for drainage, erosion and landslide hazards, beyond those impacts that would typically occur with development in the subject zone;*
- D. The request is not in conflict with the Comprehensive Plan;*
- E. The request is not in conflict with the Development Code. No variance may be granted which will result in a use not permitted in the applicable zone or which will increase the allowable residential density in any zone with the exception of individual lot size reduction; and*
- F. Physical circumstance(s) related to the property involved preclude conformance with the standard to be varied.*

Criterion A requires that the variance correct a hardship caused by the application of the standard; and that the hardship was not self-created. "Hardship" is not defined in the zoning ordinance or Comprehensive Plan, but is generally understood to be something that prevents reasonable use of the property. The applicant addresses this criterion by explaining that the seven-foot fence height is proven to deter theft and provide better security than a six-foot fence. The request can be approved if the Planning Commission finds that the hardship associated with a six-foot fence is sufficient to meet this standard. Staff recommends that the Planning Commission find that the hardship is not self-created.

Criterion B is that the variance is needed to make reasonable use of the property. Economic considerations are valid. The applicant addresses this criterion by stating that the increase in site security provided by the seven-foot fence is necessary to make reasonable use of the property. Staff generally concurs with this analysis, and recommends that the Planning Commission find the proposed variance consistent with criterion B.

Criterion C states that the variance must not be "substantially injurious" to the neighborhood. Physical impacts, such as visual, noise, traffic or increased potential for drainage, erosion or landslide hazards are listed in the criterion as potential injuries. The applicant addresses this criterion by stating that the variance will not result in physical impacts such as visual, noise, traffic or increased potential for drainage, erosion, or landslide hazards. There is no evidence that the higher fence will have negative impacts on the neighborhood. Based on this, staff recommends that the Planning Commission find the proposal consistent with criterion C.

Criterion D prohibits variances that conflict with the Comprehensive Plan. The Comprehensive Plan does not contain any policies related to fence height. Staff believes the fence height regulation is intended to implement Comprehensive Plan policies that encourage a small-town aesthetic for new development. There is no evidence that the variance would conflict with any part of the Comprehensive Plan. Based on this, staff recommends that the Planning Commission find the proposed variance consistent with criterion D.

Criterion E prohibits use variances; that is, a variance allowing a use that could not otherwise be permitted. This is not the case here, where the proposed use is allowed outright in the I1 zone, this concern is not relevant. Staff recommends that the Planning Commission find the proposed right-of-way width variance consistent with criterion E.

Criterion F states that the requested variance must be based on the site's physical characteristics. The site is relatively large and not located in a high-traffic area, so security considerations may justify a higher fence at this location. Based on this, the Planning Commission can find that the proposal meets the requirements of variance criterion F.

DISCUSSION & RECOMMENDATION

The proposal meets or can be conditioned to meet the applicable requirements of the City's Development Code. Based on this, the Planning Commission should approve the proposed fence height variance and the requested site design review subject to the following conditions.

Recommended Approval Conditions:

1. The applicant will update the stormwater report to address the deficiencies noted in the August 25, 2020, A.M. Engineering letter.
2. This approval does not include any signs. Following the requirements of Chapter 16.144 of the Warrenton Development Code, the applicant shall apply for and obtain approval for any new signs prior to installation of those signs.
3. The proposed waste enclosure will be redesigned to meet applicable Public Works Department standards before a building permit will be issued.

CITY OF WARRENTON
PLANNING AND BUILDING DEPARTMENT

COMMERCIAL SITE DESIGN
APPLICATION

To be accompanied by a Site Plan Map, copy of property deed
and if applicable, a Letter of Authorization.

OFFICE USE ONLY
FILE # SDR-20-5 FEE \$ 750.00
ZONING DISTRICT _____
RECEIPT # 8828985
DATE RECEIVED 7/6/20

The site plan review process is a method for assuring compliance with the City of Warrenton Comprehensive Plan and Development Code, and to ensure wise utilization of natural resources, and the proper integration of land uses utilizing appropriate landscaping or screening measures. A commercial enterprise must also consider traffic circulation patterns, off-street parking, refuse containers, safe exit and entrance to the business, building height, dust control, future widening of major thoroughfares, and signs. Please answer the questions as completely as possible.

Legal Description of the Subject Property: Township 8, Range 10, Section(s) 34, Tax Lot(s) 2300

Property street address SE Dolphin Avenue, Warrenton, Oregon 97146

I/WE, THE UNDERSIGNED APPLICANT(S) OR AUTHORIZED AGENT, AFFIRM BY MY/OUR SIGNATURE(S) THAT THE INFORMATION CONTAINED IN THE FOREGOING APPLICATION AND ASSOCIATED SUBMISSIONS IS TRUE AND CORRECT.

APPLICANT:

Printed Name: Mary Fierros Bower, LRS Architects mfierrosbower@lrsarchitects.com

Signature: *Mary Fierros Bower* Date: 7/2/20

Address: 720 NW Davis Street, Suite 300 Phone: 503.265.1572

City/State/Zip: Portland, OR 97209 Fax: _____

PROPERTY OWNER (if different from Applicant):

Printed Name: Wayne Pipes, NW Natural

Signature: *Wayne Pipes* Date: 7/2/20

Address: 250 SW Taylor Street Phone: 503.226.4211 X2496

E-mail Address: Wayne.Pipes@nwnatural.com

City/State/Zip: Portland, OR 97204-3038 Fax: _____

1. In detail, please describe your proposal:

The scope includes a new 1 story 4,408 SF office building designed to seismic category IV "Immediate occupancy". Including a new 7,740 SF vehicle storage & warehouse building designed to seismic category II "immediate occupancy", with drive-thru bays and truck washing equipment (Landa) room. A 5th drive thru bay is shown on the plans as an add alternate and is included in the square footage above. Site work scope includes, a covered truck washing pad, a covered trash enclosure, emergency generator, and staff and fleet parking spaces. Site structures include an enclosed vehicle storage garage for temperature sensitive vehicles, covered pipe storage metal shed, covered spoil & decant bin areas and fueling tank canopy.

2. Describe what type of business, commodity sold or manufactured, or service you are proposing.

The business activity or processes conducted at this facility are natural gas transmission / distribution maintenance support center (non-production) related. The proposed facility is "service" based and will not be "selling", "manufacturing", "producing" or "storing" any products / chemicals / hazardous materials. The subject resource center is a communications and dispatch center for gas line repair response crews, along with the fleet vehicle and materials storage needed for that purpose. The facility is not open to customers or the public. The existing NW Natural facility that serves the same purpose is currently in Astoria. Relocating it will increase resiliency by moving it outside of the tsunami zone, and because it will be built to a stronger seismic standard.

Current number of employees: 9 Proposed
Projected number of customers per day None, facility is not open to customers or the public
Days of operation M-F, (5 day work week) Hours of operation 7am - 3pm
Number of shipments/deliveries per day 1 delivery per week every 2 weeks
By what method will these be arriving/sent? Delivery Truck: 23' truck length + 45' trailer = 68' total length

3. Does this property have an existing business or businesses? No, greenfield site.

If yes, please list the business names and their addresses, and note these businesses on your site plan map.

4. Is there a residence or residences on this property? No.

If yes, please list the number of residences and please show these structures on your site plan map.

5. Availability of services: City water Yes, City sewer Yes

6. If you are an existing business, are materials or merchandise currently being stored on site?
NA

Where and how do you propose to store materials or merchandise for sale or processing?

7. What percentage of the property is currently landscaped? None

What percentage of the property do you propose to landscape as part of this project? 52%

8. How do you intend to irrigate the existing and proposed landscaping? _____

All new landscape areas will be irrigated with an automatic irrigation system.

9. Signs require the submittal of a separate application, which may be submitted in conjunction with this site plan application. Acknowledged. Signs are not proposed with this application.

10. Please explain how you propose to provide for the drainage of this property, or explain why no additional drainage consideration is necessary. The historic drainage pattern for the property involves collecting the run-off from the undeveloped property and routing all to west of SE Dolphin Avenue in existing open drainage ways. The site plan has been designed to include a large detention facility and flow control structure to limit the peak flow leaving the site to pre-development peak flow rate. A stormwater report has been prepared with this application

11. Please provide the type of development on the neighboring properties.

North: RH High Density Residential (SE Warrior Way access road) / I-1 General Industrial (Storage)

South: I-1 General Industrial (Oregon State Police)

East: RH High Density Residential (Access Road) / I-1 General Industrial (Undeveloped)

West: C-1 General Commercial (Across Street)

12. Provide samples of the building materials for the exterior of the building with detail description of where each type and color will be used in the construction and finishing of the building. Please see Exhibit B, as acknowledged in email correspondence with Kevin Cronin, E-samples are fine with notations of materials shown on ext. elev. drwgs.

13. Will all parking for your business be provided on the property? Yes X No _____ All parking must be shown on your site plan map. If off-street parking is to be provided on another property, please attach a copy of the parking easement or agreement from the property owner; or will off-street parking be provided along the abutting street.

14. How does this request comply with the Warrenton Development Code Chapter 16, Section 16.40 (General Commercial)? _____

Please see attached written narrative for how this request complies with the Warrenton Development Code Chapter 16, Section 16.60 (General Industrial).

15. Orientation of proposed building(s) (see Section 16.116.030 in the Warrenton Development Code) As acknowledged in the pre-application notes, aesthetic design standards for commercial development do not appear to apply to this property. Those standards begin "the City's development design standards are for the

commercial district along Highway 101, SW Dolphin, and SE Marlin Avenues." (WDC 16.116.030) This property is not on any of those streets, and therefore the design standards do not apply.

16. Please address (on separate sheet of paper) all applicable sections of Design Standards (copy attached) out of the Warrenton Development Code.
Please see attached written narrative for how this request complies with all applicable sections of the Warrenton Development Code. *****

PLEASE UNDERSTAND THAT THIS APPLICATION WILL NOT BE OFFICIALLY ACCEPTED UNTIL DEPARTMENT STAFF HAS DETERMINED THAT THE APPLICATION IS COMPLETELY FILLED OUT AND THE SITE PLAN MAP REQUIREMENTS HAVE BEEN COMPLETED.

Return Application To:

City of Warrenton
Planning and Building Department
PO Box 250
225 S. Main Street
Warrenton, Oregon 97146
Phone: 503-861-0920
Fax: 503-861-2351

MAP INSTRUCTIONS AND CHECKLIST

A Site Plan Map, which shows all existing and proposed structures and parking areas, must accompany this application. The following checklist identifies the specific information which should be included on this map.

- Title the map "Commercial Site Design".
- The map may be drawn on 8 ½ x 11 or 8 ½ x 14 inch white paper.
- Township, Range, Section and Tax Lot number of the subject property(ies) shall be included.
- North arrow, date, and map scale in one inch intervals (1" = 20') shall be noted.
- Shape, dimensions, and square footage of the parcel shall be shown. Draw the property line with a solid black line and label adjacent street(s), if any.
- Identify existing and proposed easements with a dotted line.
- Identify the location and direction of all water courses and drainage ways, as well as the location of the 100-year floodplain, if applicable.
- Illustrate all existing buildings and their sizes.
- Illustrate all proposed new construction with dashed lines (include dimensions).

X Illustrate parking area with number of spaces and access drive areas. If off-street parking is to be provided, even in part, on another property, please show its location on your site plan map, and attach a copy of the parking easement or agreement from the adjoining property owner.

X Illustrate the entrance and exit points to the property, pattern of traffic flow, loading and unloading area, sidewalks and bike paths.

X Illustrate the existing or proposed location, height, and material of all fences and walls.

X Illustrate existing or proposed trash and garbage container locations, including type of screening.

X Name of the person who prepared the map.

X Location, type and height of outdoor lighting.

X Location of mailboxes if known.

NA Locations, sizes, and types of signs (shall comply with Chapter 16.144 of the Warrenton Development Code).

X Map shall show entire tax lot plus surrounding properties.

X Identification of slopes greater than 10%.

X location, condition and width of all public and private streets, drives, sidewalks, pathways, right-of-ways, and easements on the site and adjoining the site.

NA Identify designated flood hazard area(s).

NA Show wetland and riparian areas, streams and/or wildlife areas.

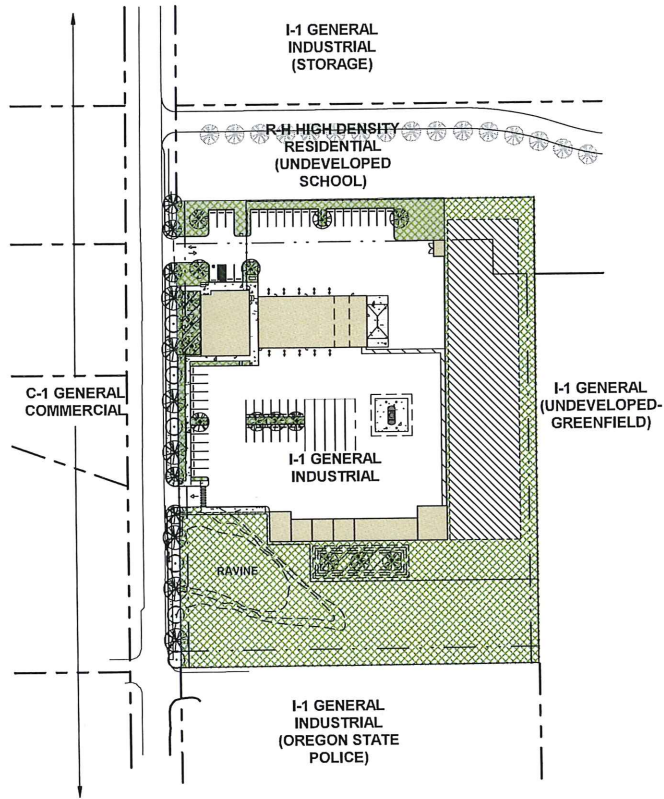
NA Any designated historic and cultural resources areas on the site and/or adjacent parcels or lots.

X Location, size and type of trees and other vegetation on the property.

SITE PLAN

A1

ZONING MAP WITH CONTEXT



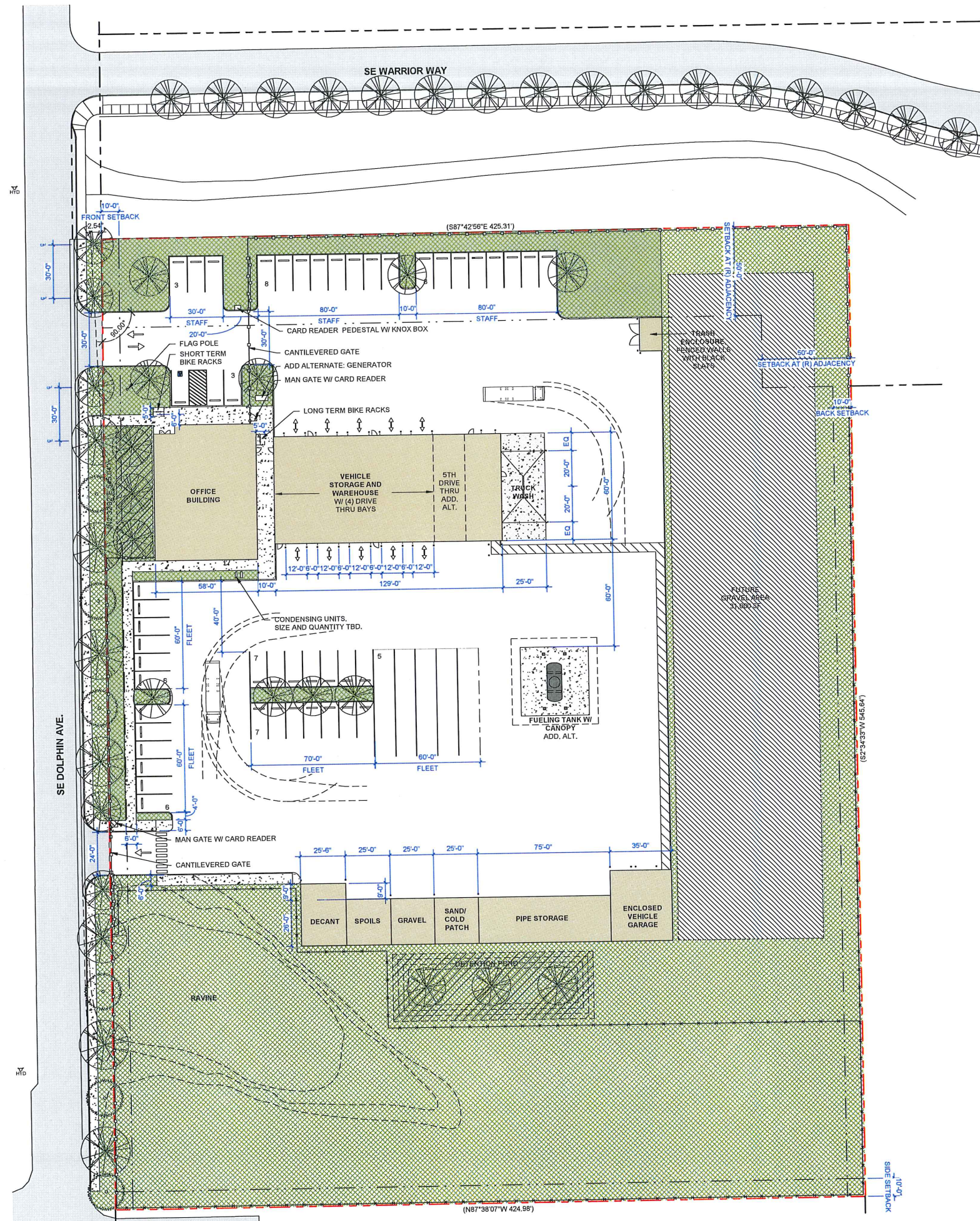
SITE PLAN LEGEND

- LANDSCAPED REGIONS
- UNDEVELOPED LAND
- SIDEWALK
- GRAVEL
- BUILDING
- PROPERTY LINE
- SETBACK LINE
- 6'-0" FENCE WITH BLACK PRIVACY SLATS
- 6'-0" FENCE

Scale: As indicated

NOT FOR CONSTRUCTION







LRS ARCHITECTS 218113 | NWN Warrenton | CONCEPTUAL DESIGN 07/02/20

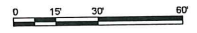
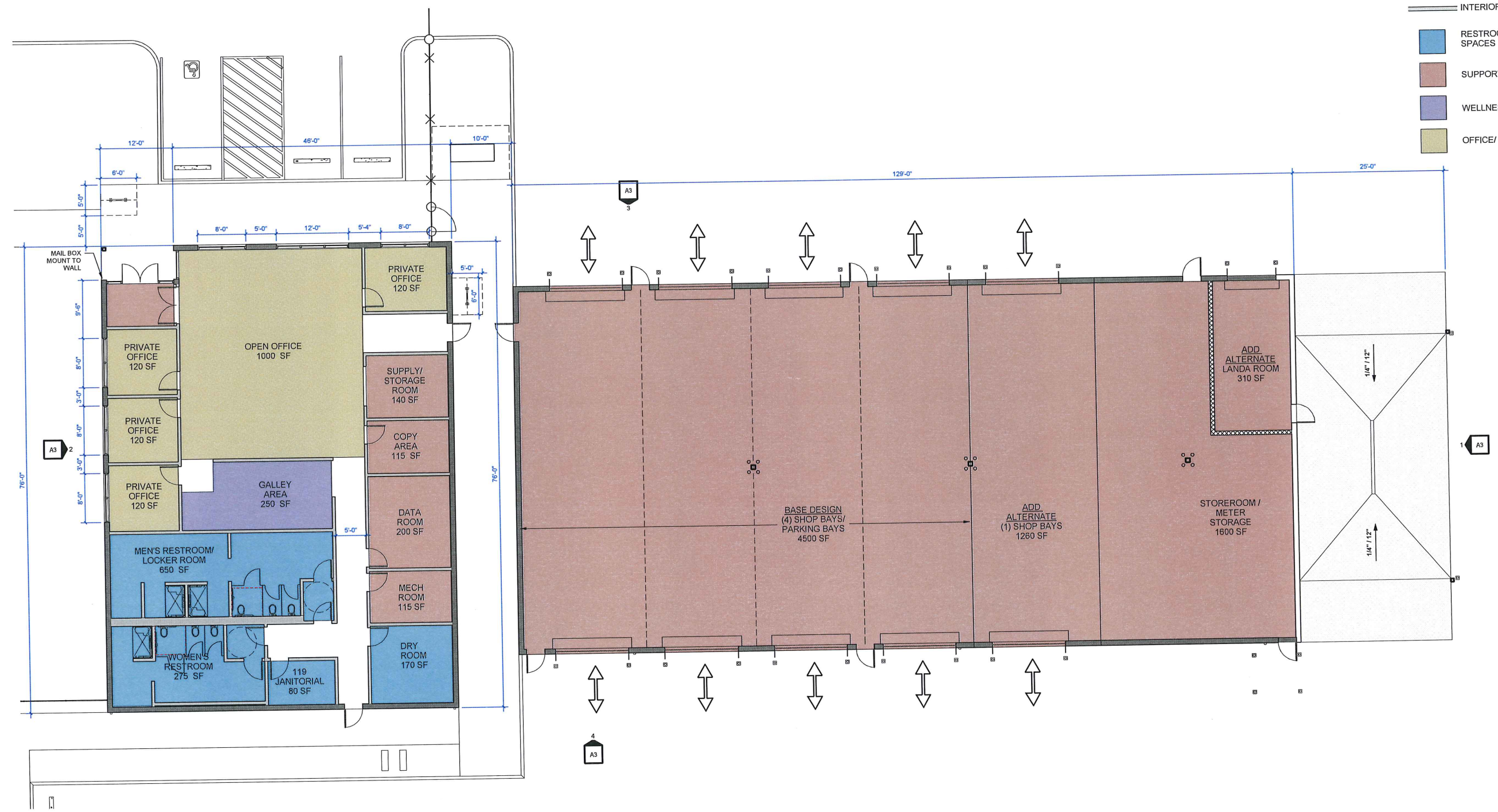


PROGRAMMATIC FLOOR PLAN

A2

PROGRAM LEGEND

-  EXTERIOR WALL
-  INTERIOR DIVIDING WALL
-  RESTROOM/ FACILITY SPACES
-  SUPPORT SPACES
-  WELLNESS SPACES
-  OFFICE/ WORK SPACES



Scale: As indicated

NOT FOR CONSTRUCTION



SITE BUILDINGS - ELEVATIONS

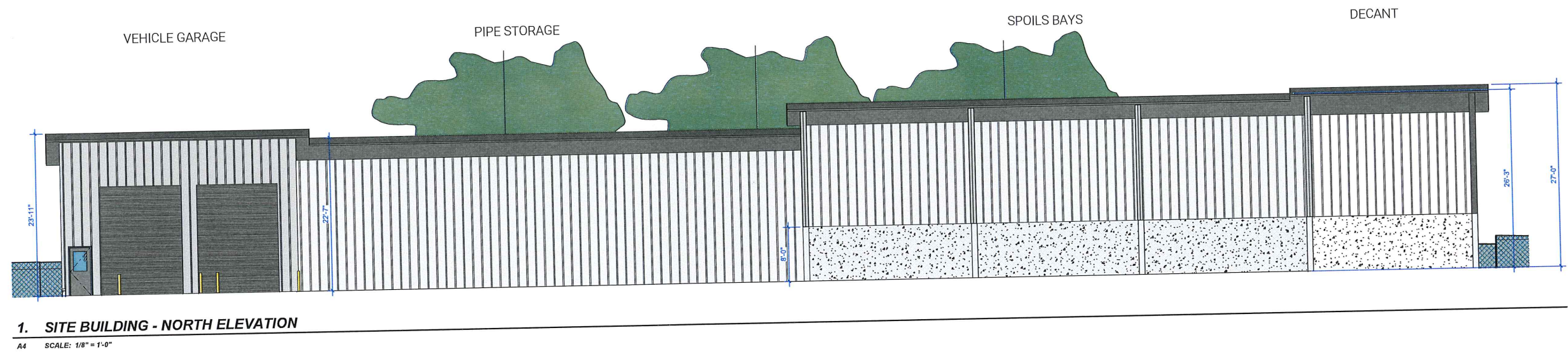
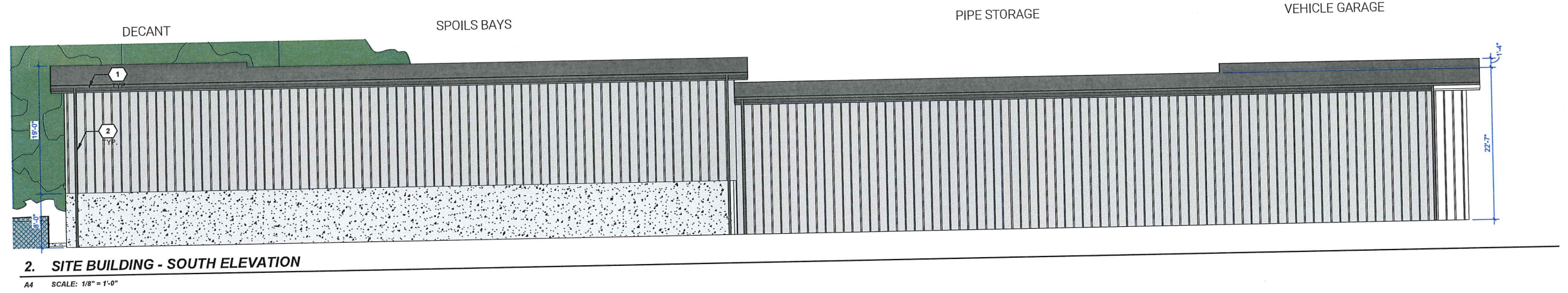
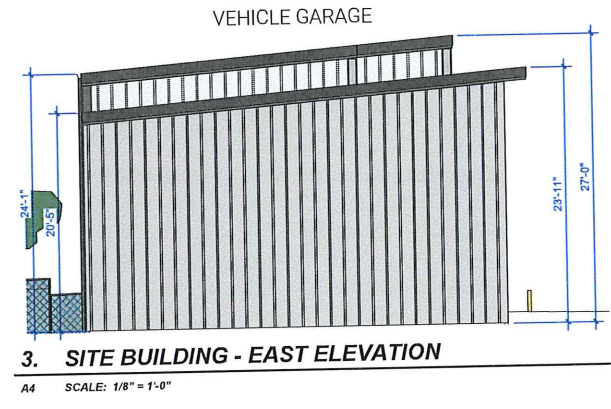
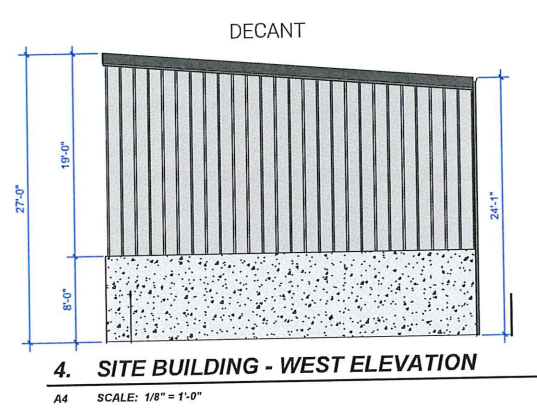
A4

KEYNOTES

- 1 CONTINUOUS GUTTER ON FASCIA BOARD, DARK BRONZE FINISH, TYP.
- 2 DOWNSPOUT, DARK BRONZE, TYP.

LEGEND

-  SMOOTH, FLAT METAL PANEL SYSTEM, PRE-FINISHED, WARM BEIGE- LIGHT WARM BEIGE AT SITE BUILDING INTERIOR
-  CORRUGATED METAL PANEL SYSTEM- HORIZONTAL, PRE-FINISHED, MEDIUM BRONZE/ GRAY
-  GLAZING, STOREFRONT WINDOW FRAMES, DARK BRONZE
-  STANDING SEAM METAL ROOFING, DARK BRONZE
-  SMOOTH CONCRETE WALL



Scale: 1/8" = 1'-0"

NOT FOR CONSTRUCTION




BUILDING ELEVATIONS

A3

KEYNOTES

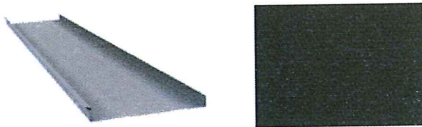
- 1 DOWNSPOUT, DARK BRONZE, TYP.
- 2 CONTINUOUS GUTTER ON FASCIA BOARD, DARK BRONZE FINISH, TYP.
- 3 OVERHEAD COILING DOOR, 14' X 16', PAINTED.
- 4 STOREFRONT WINDOW FRAMES, DARK BRONZE
- 5 ADDRESS NUMBERS, 12" HIGH LETTERING, BRUSHED ALUMINUM FINISH, MOUNTED TO WALL. (ACTUAL NUMBERS TRD.)

LEGEND

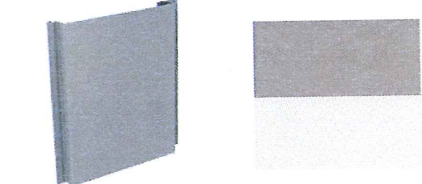
-  SMOOTH, FLAT METAL PANEL SYSTEM, PRE-FINISHED, WARM BEIGE- LIGHT WARM BEIGE AT SITE BUILDING INTERIOR
-  CORRUGATED METAL PANEL SYSTEM- HORIZONTAL, PRE-FINISHED, MEDIUM BRONZE/ GRAY
-  GLAZING, STOREFRONT WINDOW FRAMES, DARK BRONZE
-  STANDING SEAM METAL ROOFING, DARK BRONZE
-  SMOOTH CONCRETE WALL

MATERIALS

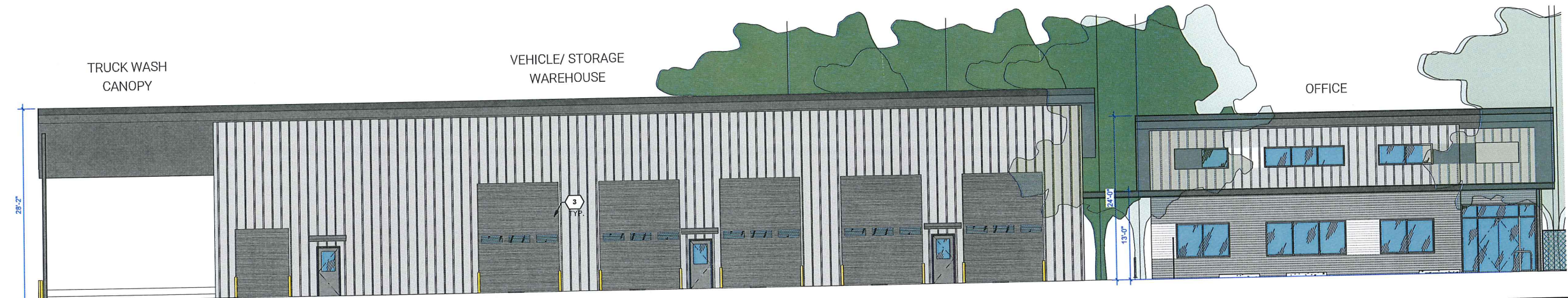
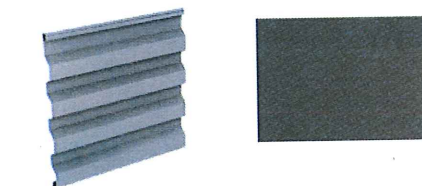
STANDING SEAM METAL ROOF



SMOOTH PANEL SYSTEM

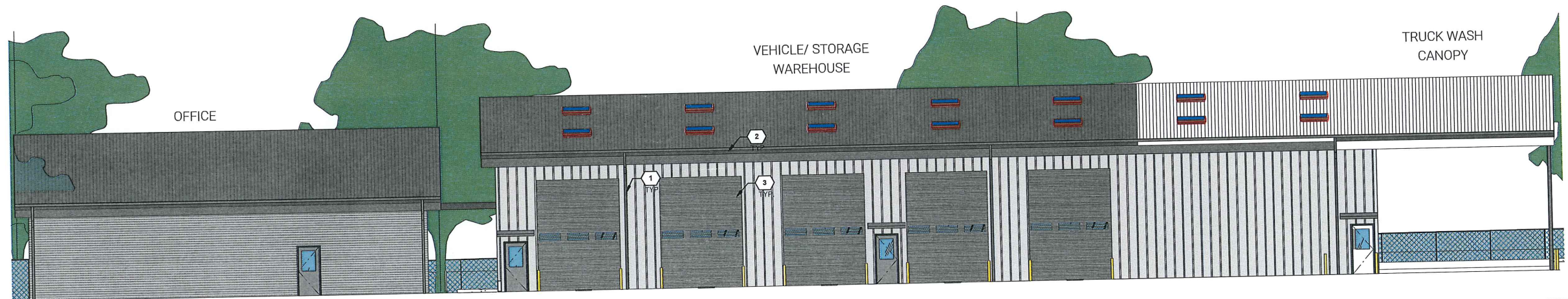


CORRUGATED PANEL SYSTEM



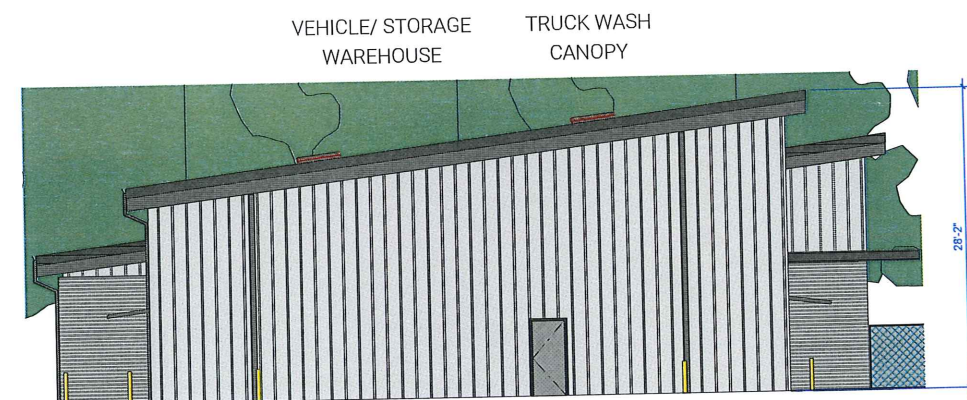
NORTH ELEVATION

A3 SCALE: 1/8" = 1'-0"



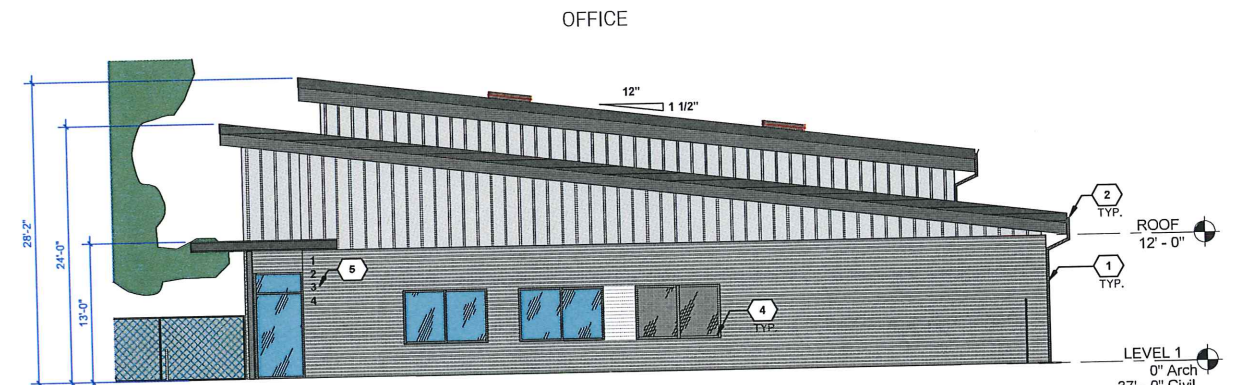
SOUTH ELEVATION

A3 SCALE: 1/8" = 1'-0"



EAST ELEVATION

A3 SCALE: 1/8" = 1'-0"



WEST ELEVATION

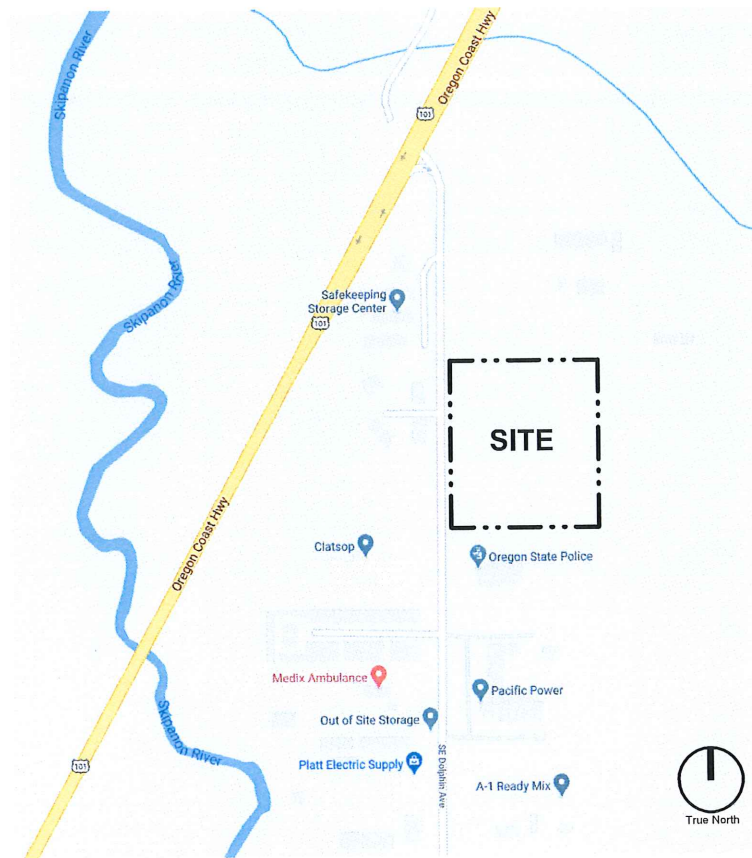
A3 SCALE: 1/8" = 1'-0"

Scale: 1/8" = 1'-0"

NOT FOR CONSTRUCTION

NWN WARRENTON RESOURCE CENTER

SITE PLAN DESIGN REVIEW APPLICATION SET - JULY 02, 2020



SHEET INDEX

CS	COVER SHEET
C1	SURVEY
C2	GRADING/ DRAINAGE PLAN
C3	UTILITY PLAN
C4	PAVING PLAN
C5	CIVIL DETAILS
C6	CIVIL DETAILS
L1	SITE PLAN
L2	LANDSCAPE PLAN
A1	PROPOSED SITE PLAN
A2	SITE BIRD'S EYE 3D VIEW
A3	OFFICE & WAREHOUSE - PROGRAMMATIC FLOOR PLAN
A4	OFFICE & WAREHOUSE - ELEVATIONS
A5	SITE MODEL

PROJECT OVERVIEW

ZONE

I-1 GENERAL INDUSTRIAL

ADDRESS

SE DOLPHIN AVE,
WARRENTON, OR

MAP TAXLOT

2300

PROJECT DESCRIPTION

THE SCOPE INCLUDES A NEW 1 STORY 4,408 SF OFFICE BUILDING DESIGNED TO SEISMIC CATEGORY IV. INCLUDING A NEW 7,740 SF VEHICLE STORAGE & WAREHOUSE, DESIGNED TO CATEGORY II, WITH DRIVE-THRU SHOP BAYS AND TRUCK WASHING EQUIPMENT (LANDA) ROOM. (A 1260 SF DRIVE BAY IS ALSO SHOWN ON PLANS AS AN ADD. ALTERNATE)

SITE WORK SCOPE INCLUDES VARIOUS OTHER SITE STRUCTURES, A COVERED TRUCK WASHING PAD, A COVERED TRASH ENCLOSURE, EMERGENCY GENERATOR, AND STAFF AND FLEET PARKING SPACES. SITE STRUCTURES (ENCLOSED VEHICLE STORAGE GARAGES, COVERED PIPE STORAGE METAL SHED, COVERED SPOIL BIN AREAS, FUELING TANK CANOPY) WILL BE CONSTRUCTED WITH PRE-ENGINEERED METAL BUILDING STRUCTURES.

SITE SUMMARY

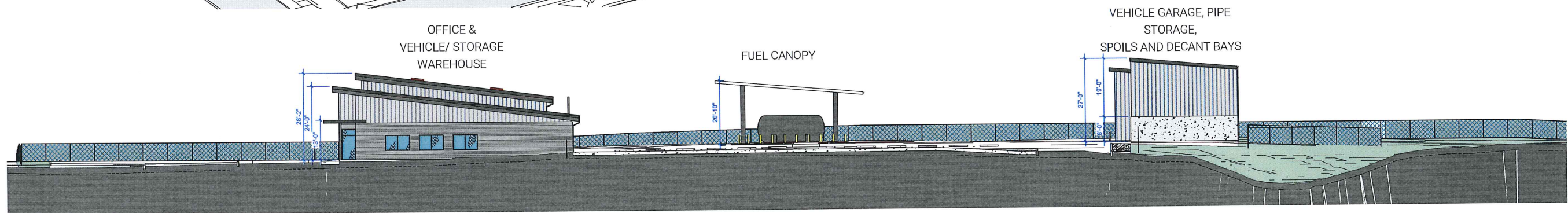
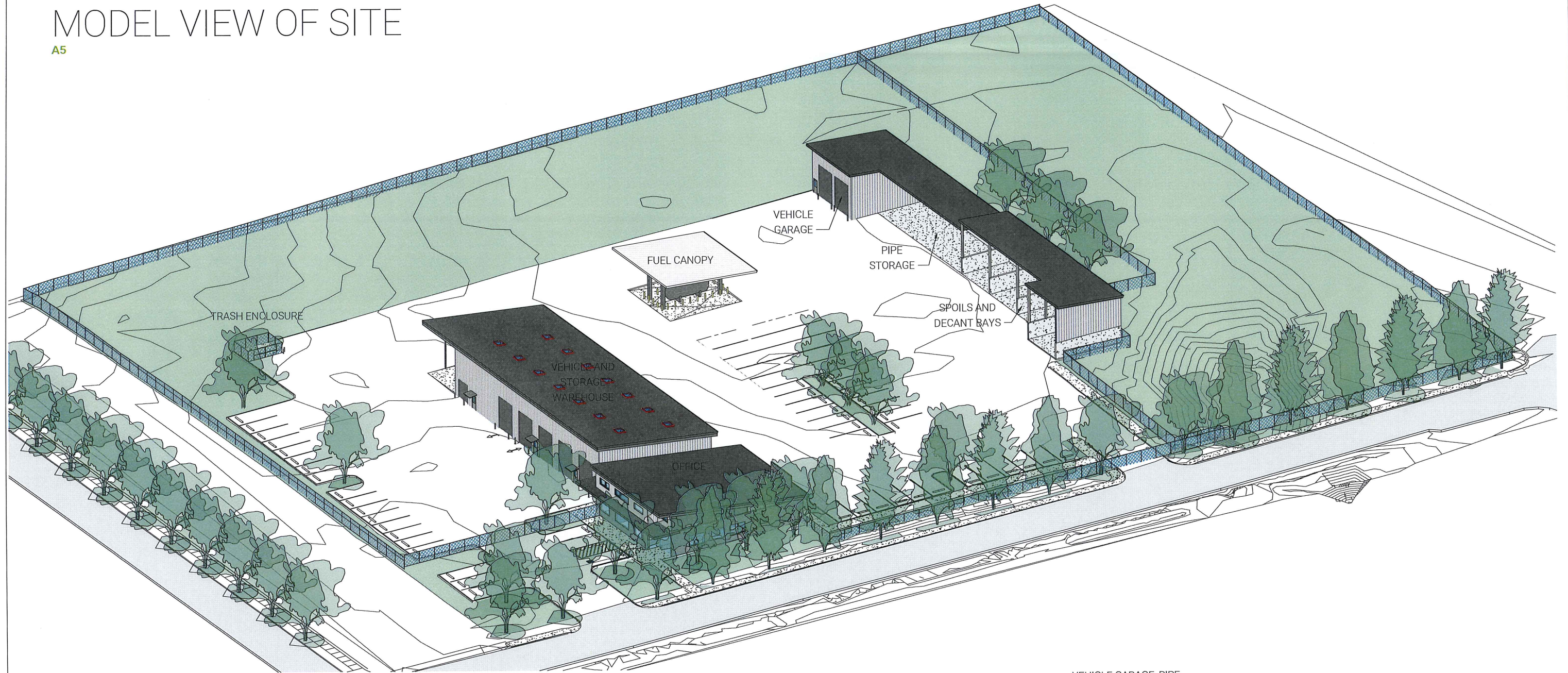
LOT COVERAGE	TOTALS
SITE	231,847.64 SF (5.32 AC)
TOTAL IMPERMEABLE SURFACES	109,278.4 SF
PAVEMENT & DRIVES	82,351.4 SF
BUILDING TOTAL	22,434 SF
OFFICE	4,408 SF
WAREHOUSE (W/ 5 COVERED TRUCK BAYS)	7,740 SF
LANDA CANOPY	1,500 SF
PIPE STORAGE	1,950 SF
SPOILS AND DECANT BAYS	2,842 SF
ENCLOSED VEHICLE STORAGE GARAGE	1,399 SF
TRASH ENCLOSURE	243 SF
FUELING CANOPY	2,352 SF
TOTAL LANDSCAPE	122,569.24 SF
DESIGNED LANDSCAPE AREAS	32,794 SF
GRAVEL AREA	31,000 SF
BIOSWALE TOTAL	7,500 SF
UNDEVELOPED OPEN SPACE	70,331 SF
PARKING	
FRONT AREA	6
STAFF	16
FLEET	26
TRUCK BAYS (NOT COVERED)	5
TRUCK BAYS (COVERED)	5



NW Natural®

MODEL VIEW OF SITE

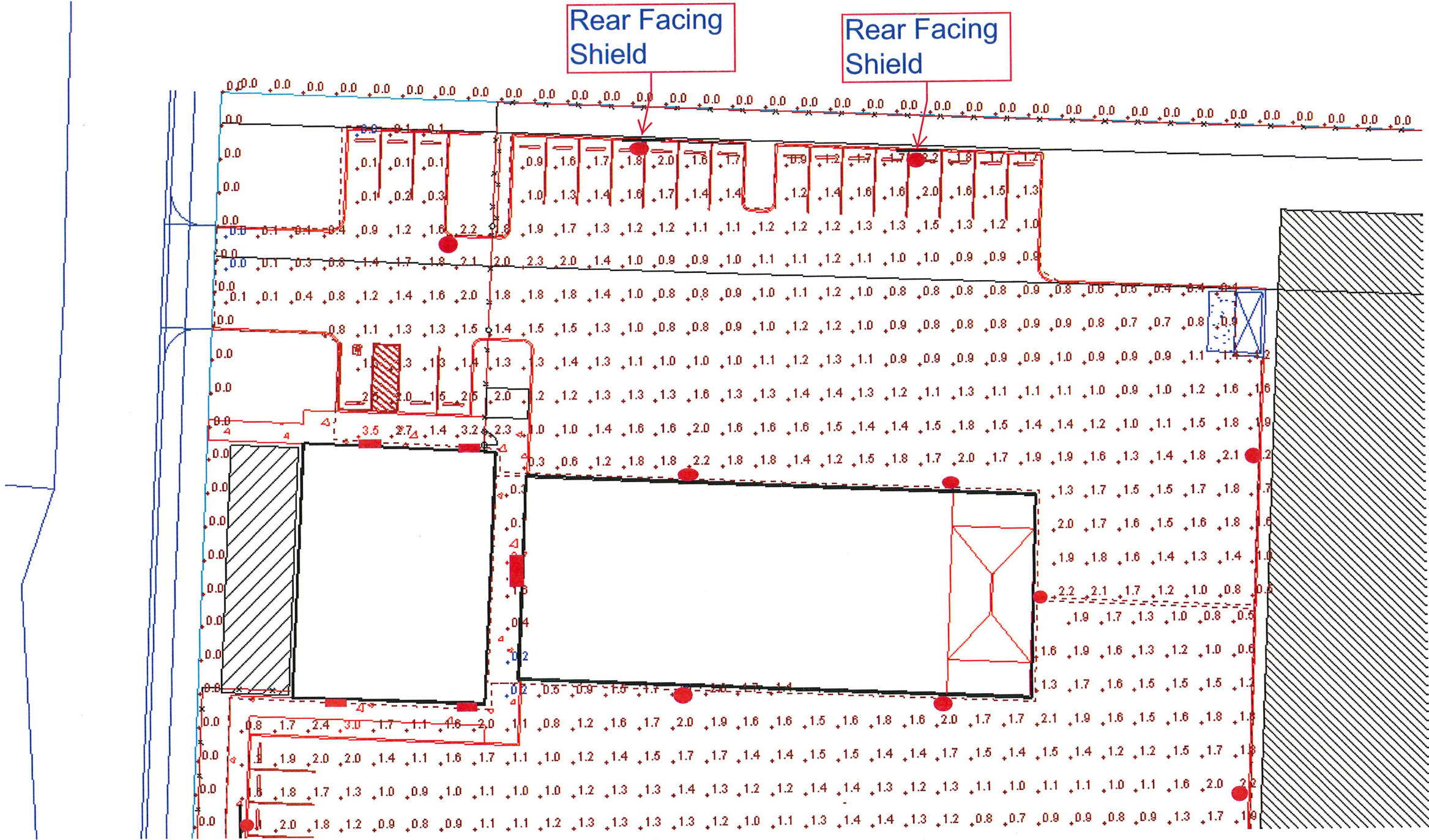
A5



SITE ELEVATION- WEST

Scale: 1/16" = 1'-0"

NOT FOR CONSTRUCTION



Current View

Symbol	Label	Quantity	Manufacturer	Catalog Number	Description	Lamp	Number Lamps	Filename	Lumens Per Lamp	Light Loss Factor	Wattage
	A	20	COOPER LIGHTING SOLUTIONS - MCGRAW-EDISON (FORMERLY EATON)	GLEON-AF-01-LED-E1-SL4-7050	GALLEON AREA AND ROADWAY LUMINAIRE (1) 70 CRI, 5000K, 1050mA LIGHTSQUARE WITH 16 LEDS EACH AND TYPE IV SPILL LIGHT ELIMINATOR OPTICS		16	GLEON-AF-01-LED-E1-SL4-7050.ies	370	1	59
	B	5	Lithonia Lighting	TWR1 LED 1 50K MVOLT	2100lm TWR1 LED WALLPACK	LED	10	TWR1_LED_1_50K_MVOLT.ies	211	1	34.4

Statistics						
Description	Symbol	Avg	Max	Min	Max/Min	Avg/Min
East PL	+	0.0 fc	0.0 fc	0.0 fc	N/A	N/A
Interior Walkway	+	1.0 fc	2.7 fc	0.2 fc	13.5:1	5.0:1
Middle Parking	+	1.3 fc	3.0 fc	0.2 fc	15.0:1	6.5:1
North Parking Area	+	1.3 fc	3.5 fc	0.0 fc	N/A	N/A
North PL	+	0.0 fc	0.0 fc	0.0 fc	N/A	N/A
South Parking Area	+	1.2 fc	2.5 fc	0.0 fc	N/A	N/A
South PL	+	0.0 fc	0.0 fc	0.0 fc	N/A	N/A
West PL	+	0.0 fc	0.0 fc	0.0 fc	N/A	N/A

Designer

Date

6/15/2020



Scale

Not to Scale

Drawing No.

Summary



Schedule											
Symbol	Label	Quantity	Manufacturer	Catalog Number	Description	Lamp	Number Lamps	Filename	Lumens Per Lamp	Light Loss Factor	Wattage
	A	20	COOPER LIGHTING SOLUTIONS - MCGRAW-EDISON (FORMERLY EATON)	GLEON-AF-01-LED-E1-SL4-7050	GALLEON AREA AND ROADWAY LUMINAIRE (1) 70 CRI, 5000K, 1050mA LIGHTSQUARE WITH 16 LEDS EACH AND TYPE IV SPILL LIGHT ELIMINATOR OPTICS		16	GLEON-AF-01-LED-E1-SL4-7050.ies	370	1	59
	B	4	Lithonia Lighting	TWR1 LED 1 50K MVOLT	2100lm TWR1 LED WALLPACK	LED	10	TWR1_LED_1_50K_MVOLT.ies	211	1	34.4

Statistics						
Description	Symbol	Avg	Max	Min	Max/Min	Avg/Min
East PL	+	0.0 fc	0.0 fc	0.0 fc	N/A	N/A
Middle Parking	+	1.3 fc	3.0 fc	0.2 fc	15.0:1	6.5:1
North Parking Area	+	1.3 fc	3.5 fc	0.0 fc	N/A	N/A
North PL	+	0.0 fc	0.0 fc	0.0 fc	N/A	N/A
South Parking Area	+	1.2 fc	2.5 fc	0.0 fc	N/A	N/A
South PL	+	0.0 fc	0.0 fc	0.0 fc	N/A	N/A
West PL	+	0.0 fc	0.0 fc	0.0 fc	N/A	N/A

Designer

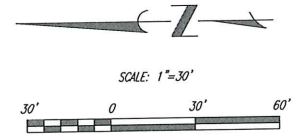
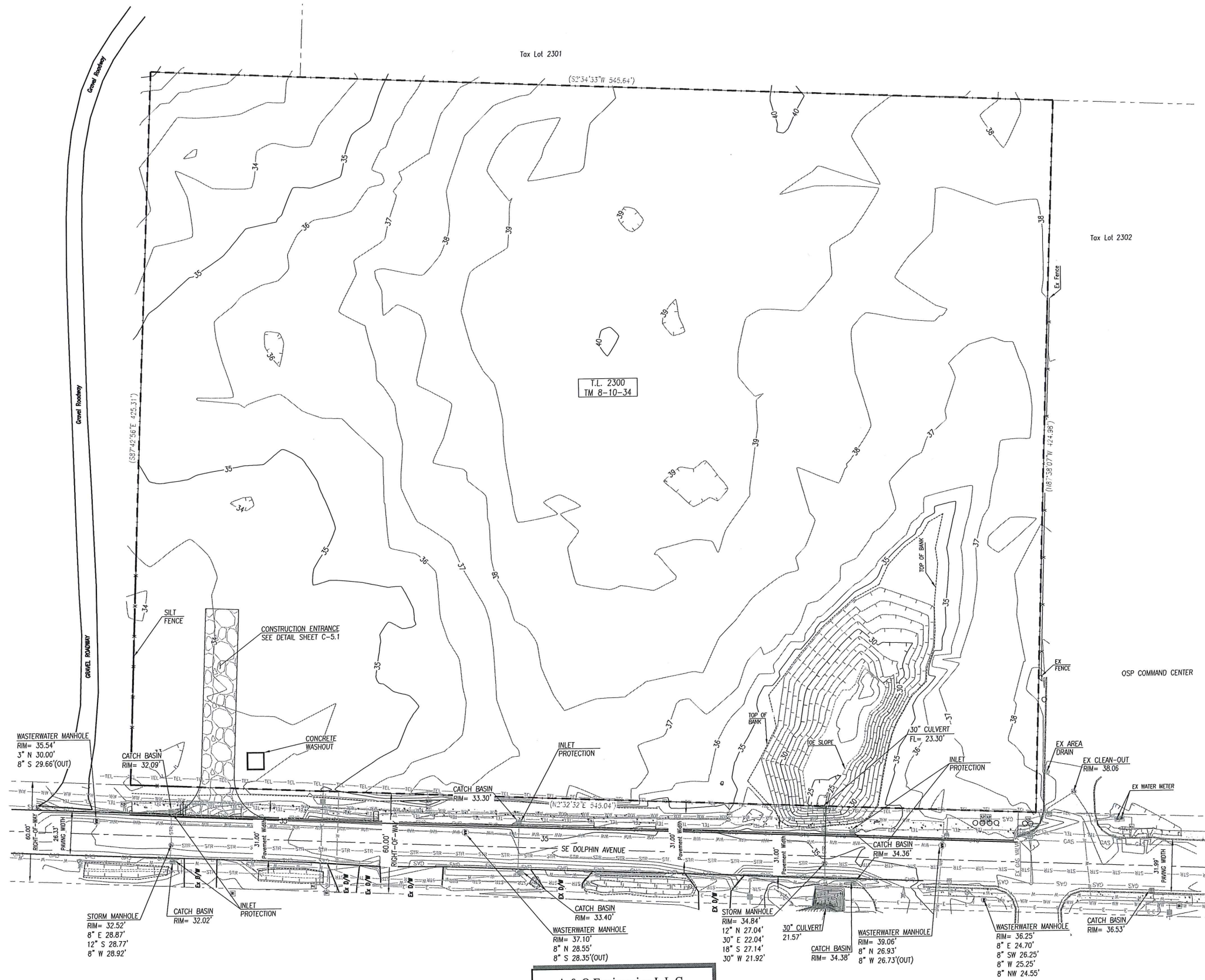
Date
6/15/2020

Scale
Not to Scale
Drawing No.

Summary

EXISTING CONDITIONS PLAN/EROSION CONTROL/SE DOLPHIN AVENUE

C-1.0



GENERAL NOTES:

1. THIS IS NOT A BOUNDARY SURVEY. SEE RECORDED DOCUMENTS FOR OFFICIAL PROPERTY BOUNDARY INFORMATION.
2. EXISTING CONDITIONS REFLECT CONDITION OF PROPERTY ON JUNE 22, 2018.
3. NO SIGNIFICANT VEGETATION ON SITE.

LEGEND

---	BOUNDARY LINE
---	ADJACENT LOT LINE
---	EXISTING 1' CONTOUR LINE
W	EXISTING WATER METER AND PIPE
⊙	EXISTING FIRE HYDRANT
⊗	EXISTING WATER VALVE
ST	EXISTING STORM DRAINAGE SYSTEM
GAS	EXISTING GAS LINE
■	EXISTING CATCH BASIN
⌋	EXISTING STORMWATER CULVERT
WW	EXISTING WASTEWATER SYSTEM
E	EXISTING OVERHEAD ELECTRIC
T	EXISTING TELECOMMUNICATION LINE
⊙	EXISTING STREET LIGHT
⊠	EXISTING ELECTRIC TRANSFORMER
⊙	EXISTING UTILITY POLE
→	EXISTING GUY ANCHOR

EROSION CONTROL NOTES:

1. DEVELOPER TO OBTAIN DEQ 1200-C PERMIT PRIOR TO START OF CONSTRUCTION.
2. EROSION CONTROL MEASURES TO BE MAINTAINED UNTIL FINAL SITE STABILIZATION IS COMPLETE.
3. INTERNAL CATCH BASIN TO BE PROTECTED AFTER INSTALLATION.



218113 | NWN Warrenton - Main - Site | SCHEMATIC DESIGN 07/02/20

A & O Engineering L.L.C.
 CONSULTING ENGINEERS & SITE DEVELOPMENT PLANNING
 380 Q ST. SUITE 200
 SPRINGFIELD, OR, 97477
 PHONE: (541) 302-9790
 info@ao-engr.com



NOT FOR CONSTRUCTION

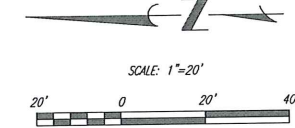


GRADING/DRAINAGE PLAN/SE DOLPHIN AVENUE

C-2.0

NOTES:

- ELEVATIONS SHOWN ON THESE PLANS ARE BASED ON TOPO SURVEY DONE BY OLSON & MORRIS.
- GRADING INCLUDES 1" DROP FROM BUILDING FINISH FLOOR ELEVATION TO BACK OF SIDEWALK.
- CONTOURS SHOWN ON THIS SHEET ARE EXISTING. PROPOSED FINISHED GRADES ARE CALLED OUT ON PLAN SHEET.
- ALL ADA FACILITIES SHALL MEET REQUIREMENTS OUTLINED IN AMERICANS WITH DISABILITY ACT, CURRENT EDITION.
- DRAINAGE BASIN Q1 TO BE TIED INTO EXISTING CATCH BASIN IN DOLPHIN RIGHT-OF-WAY.
- FINAL PIPE ROUTING TO BE DETERMINED DURING BUILDING PERMIT PROCESS.
- TRASH ENCLOSURE DETAIL SHOWN ON ARCHITECTURAL PLANS.
- DETENTION POND AND FLOW CONTROL STRUCTURE DESIGNED TO PROCESS THE 100 YEAR STORM EVENT.
- FLOW CONTROL STRUCTURE DESIGNED TO KEEP POST-CONSTRUCTION PERK FLOW RATE LEAVING THE SITE TO BE CONSISTENT WITH PRE-DEVELOPMENT PERK FLOW RATE.
- DETENTION POND DETAILS SHOWN ON SHEET C-5.0.
- FINAL GRADING TO BE DETERMINED DURING BUILDING PERMIT PROCESS.
- RIP-RAP ARMORING TO BE DESIGNED DURING BUILDING PERMIT PROCESS.



Tax Lot 2302

ESTIMATED CUT/FILL VOLUMES

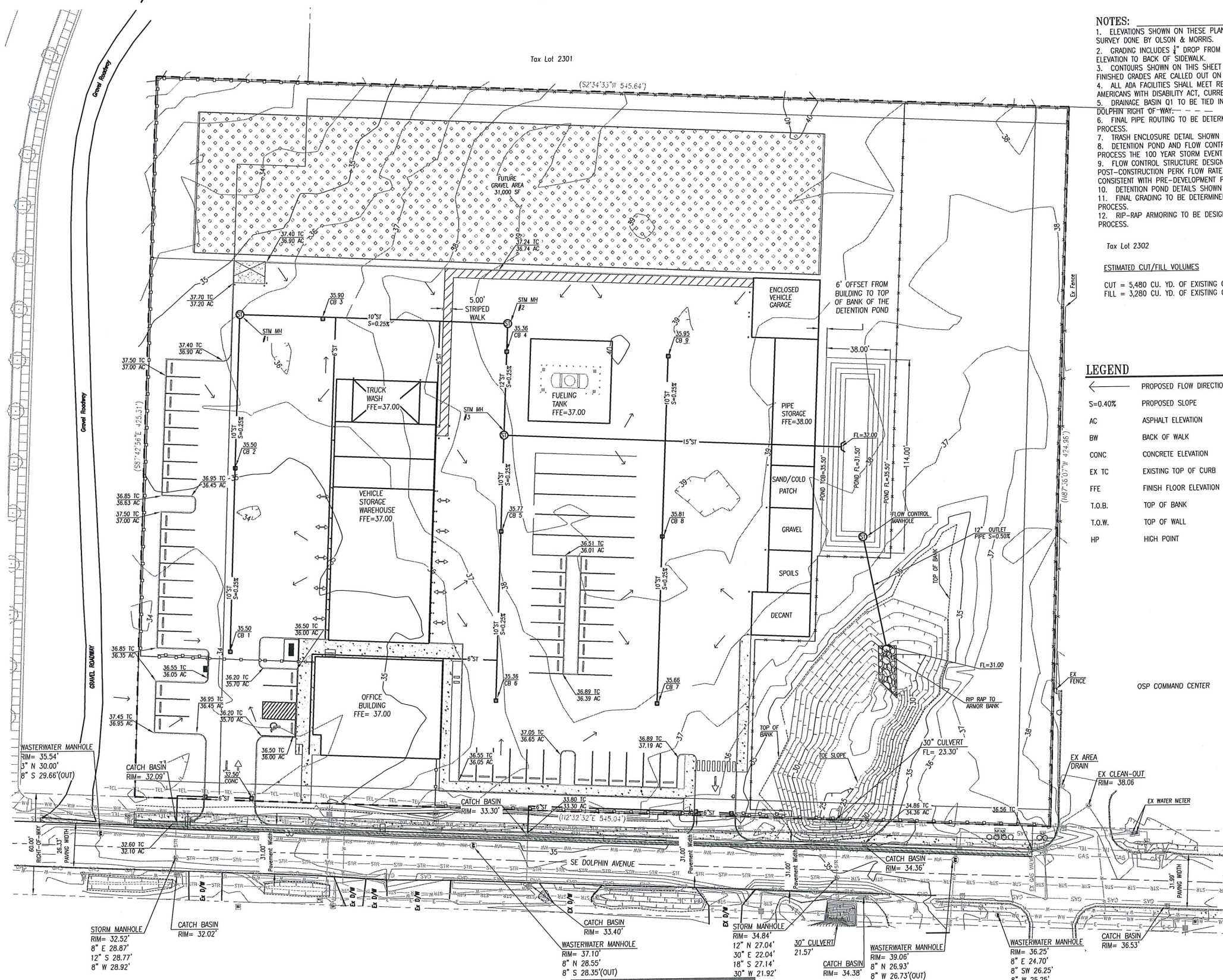
CUT = 5,480 CU. YD. OF EXISTING ONSITE MATERIAL
 FILL = 3,280 CU. YD. OF EXISTING ONSITE MATERIAL

LEGEND

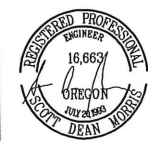
- ← PROPOSED FLOW DIRECTION
- S=0.40% PROPOSED SLOPE
- AC ASPHALT ELEVATION
- BW BACK OF WALK
- CONC CONCRETE ELEVATION
- EX TC EXISTING TOP OF CURB
- FFE FINISH FLOOR ELEVATION
- T.O.B. TOP OF BANK
- T.O.W. TOP OF WALL
- HP HIGH POINT

LEGEND

- 484 EXISTING CONTOURS
- EXISTING BOUNDARY
- ADJACENT PROPERTIES
- EXISTING CURB LINE
- EXISTING FENCE
- 8"W EXISTING WATER MAIN
- W EXISTING WATER METER
- ⊙ EXISTING WATER VALVE
- ⊙ EXISTING FIRE HYDRANT
- WW 8"WW EXISTING WASTEWATER SYSTEM
- ⊙ EXISTING CLEANOUT
- ST 24"SD EXISTING STORM DRAINAGE SYSTEM
- ⊙ EXISTING CATCH BASIN
- ⊙ EXISTING CURB INLET
- ⊙ EXISTING STREET LIGHT
- UGE EXISTING UNDERGROUND ELECTRIC
- ⊙ EXISTING TRANSFORMER
- ⊙ EXISTING TELEPHONE PEDESTAL
- PHN EXISTING TELEPHONE LINE
- 2"GC EXISTING GAS MAIN
- ⊙ EXISTING GAS VALVE
- ⊙ PROPOSED WATER METER
- 8"WW PROPOSED WASTEWATER LINE
- ⊙ PROPOSED WASTEWATER MANHOLE
- ⊙ PROPOSED WASTEWATER CLEANOUT
- 12"ST PROPOSED STORM LINE
- ⊙ PROPOSED STORM MANHOLE
- ⊙ PROPOSED STORM CLEANOUT
- ⊙ PROPOSED CURB INLET
- ⊙ EXISTING UTILITY POLE
- ⊙ EXISTING ELECTRIC TRANSFORMER
- ⊙ EXISTING GUY ANCHOR
- T EXISTING TELECOMMUNICATION LINE
- E EXISTING OVERHEAD ELECTRIC
- ⊙ EXISTING STORMWATER CULVERT

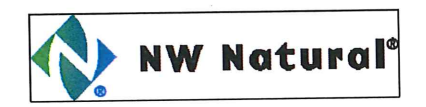


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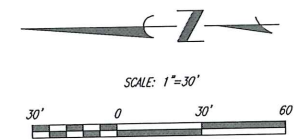
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UTILITY PLAN/SE DOLPHIN AVENUE

C-3.0



LEGEND

- BOUNDARY LINE
- ADJACENT LOT LINE
- EXISTING 1' CONTOUR LINE
- EXISTING WATER METER AND PIPE
- EXISTING FIRE HYDRANT
- EXISTING WATER VALVE
- ST --- EXISTING STORM DRAINAGE SYSTEM
- GAS --- EXISTING GAS LINE
- EXISTING CATCH BASIN
- EXISTING STORMWATER CULVERT
- WW --- EXISTING WASTEWATER SYSTEM
- E --- EXISTING OVERHEAD ELECTRIC
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- EXISTING ELECTRIC TRANSFORMER
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- EXISTING CUY ANCHOR

LIGHTING NOTES:

- SITE LIGHTING TO BE PROVIDED BY WALL PACKS ON STRUCTURES AND POLE MOUNTED FIXTURES 20.00' HIGH.
- WIRING TO LIGHTING TO BE COMPLETED DURING BUILDING PERMIT PROCESS.
- SITE LIGHTING DESIGNED TO BE PROVIDED NO LIGHT TRESSPASS ON ADJACENT PROPERTIES.

WASTEWATER NOTES:

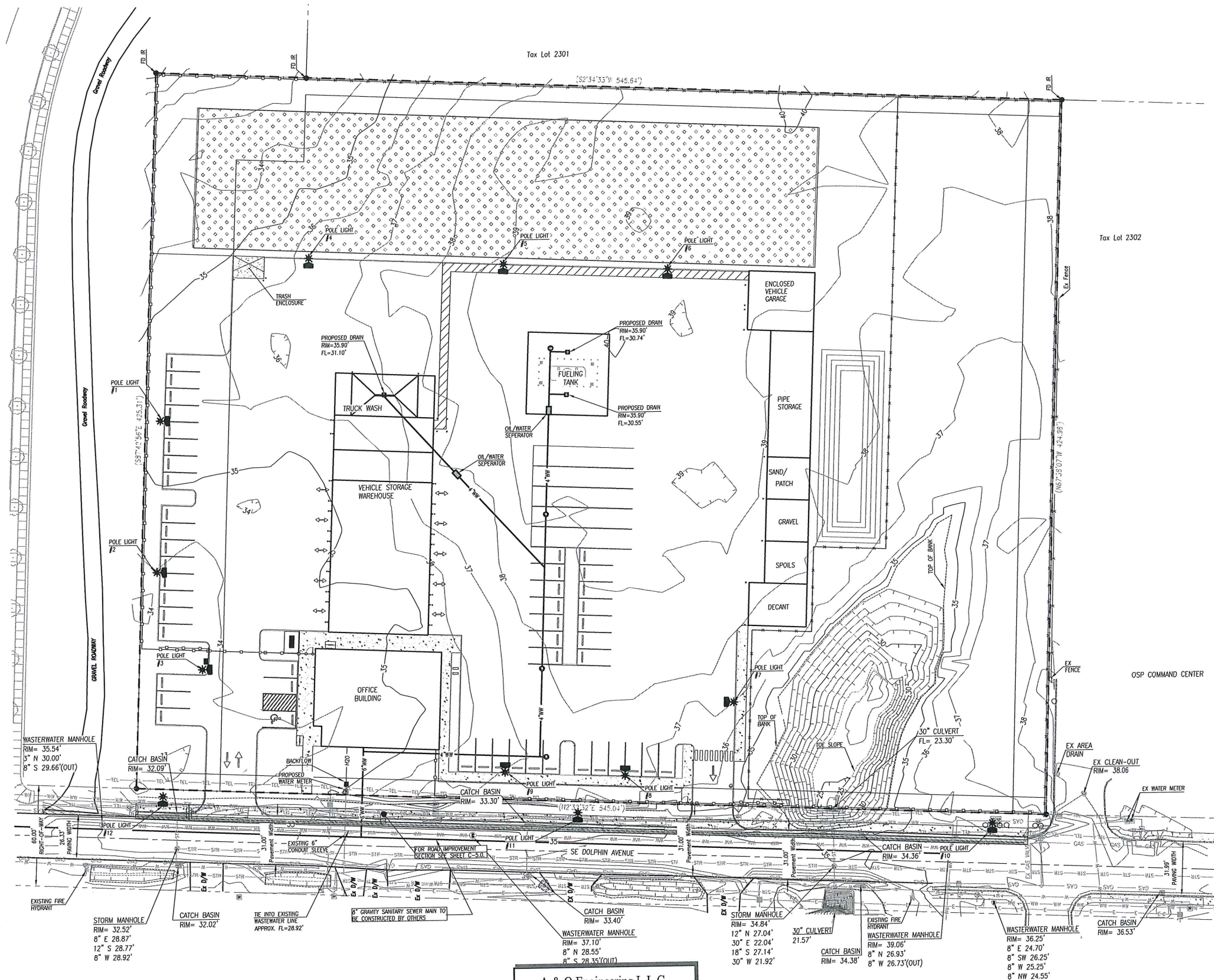
- WASTEWATER MANLINE IN DOLPHIN ROAD RIGHT OF WAY TO BE CONSTRUCTED UNDER SEPARATE PERMIT AS PART OF WARRENTON MIDDLE SCHOOL PROJECT.
- FUEL ISLAND AND TRUCK WASH RUN-OFF TO BE COLLECTED, ROUTED THROUGH OIL/WATER SEPARATORS TO THE WASTEWATER SYSTEM.
- ONSITE WASTEWATER LINES TO MEET REQUIREMENTS IN OREGON STATE PLUMBING SPECIALTY CODE, 2017.

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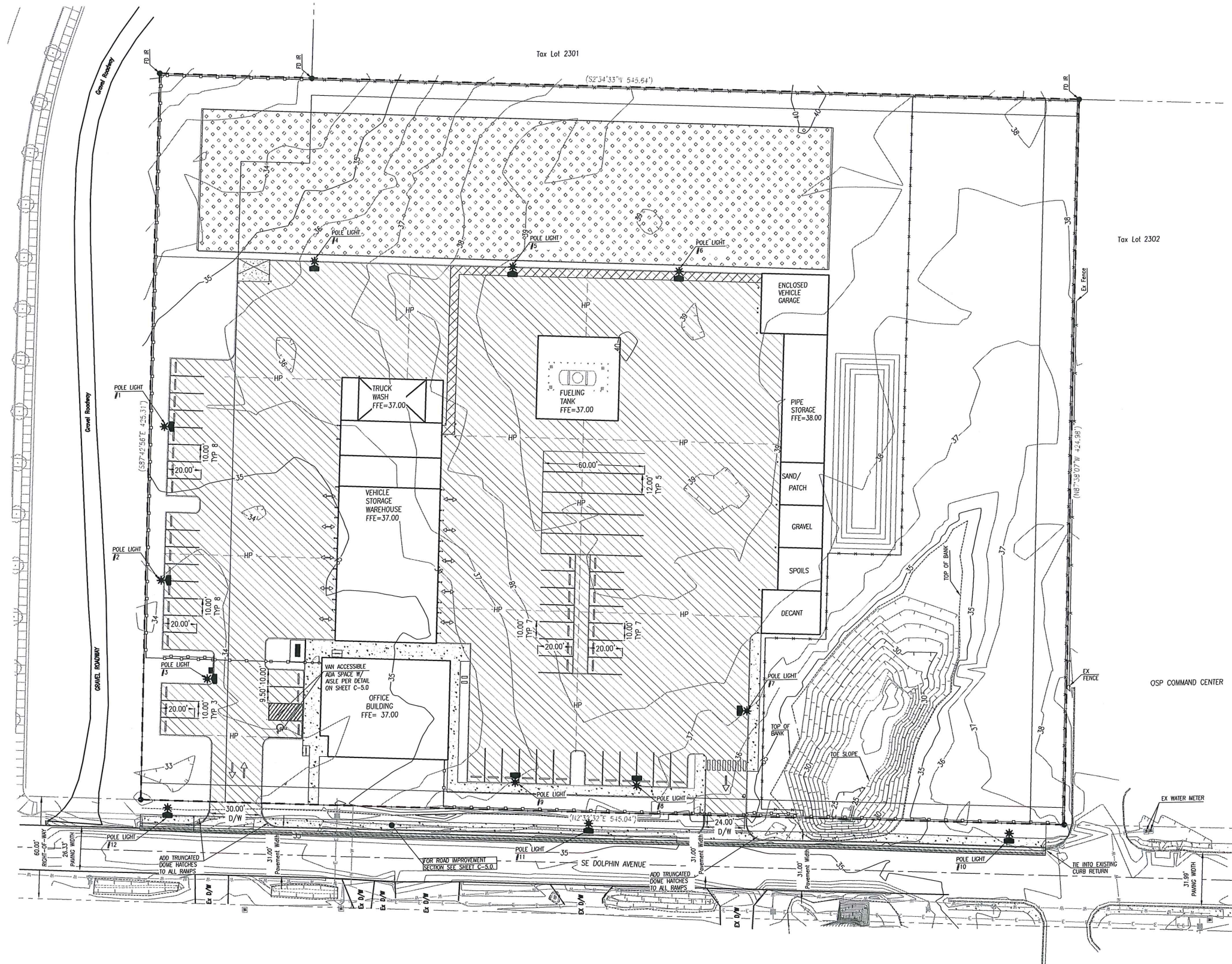
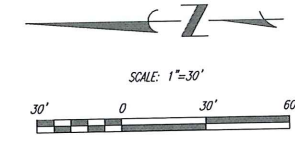
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PAVING/LIGHTING PLAN/SE DOLPHIN AVENUE

C-4.0



LEGEND

- 484- EXISTING CONTOURS
- EXISTING BOUNDARY
- ADJACENT PROPERTIES
- EXISTING CURB LINE
- EXISTING FENCE
- PROPOSED POLE MOUNTED LIGHT
- PROPOSED PAVED AREA: 3" LEVEL 2 HMAC AC OVER 12" C.R. AS PER DETAIL.
- PROPOSED CONCRETE AREA: 6" 3000 PSI REINFORCED CONCRETE OVER 2" CRUSHED ROCK.

PAVING NOTES

1. CURBING SHOWN PROPOSED TO BE EXTRUDED. FINAL DESIGN TO BE COMPLETED DURING BUILDING PERMIT PROCESS.
2. ALL WALKWAYS SHALL BE DESIGNED TO MEET ADA STANDARDS. FINAL DESIGN OF ALL ADA FACILITIES DURING BUILDING PERMIT.
3. DOLPHIN ROAD WIDENING PROPOSED TO BE PER WARRENTON DESIGN STANDARDS. SEE STREET CROSS SECTION ON SHEET C-5.0
4. AC PAVING SECTION PROPOSED TO BE 3" AC OVER 12" CRUSHED ROCK PER PAVING RECOMMENDATIONS IN GEOTECHNICAL REPORT PREPARED BY EARTH ENGINEERS INC.
5. DOLPHIN ROAD IMPROVEMENTS TO BE DESIGNED AND PERMITTED THROUGH CITY PIP PERMIT PROCESS.
6. WHEEL STOPS INSTALLED PER DETAIL SHOWN ON SHEET C-5.0.
7. ADA RAMPS TO BE DESIGNED DURING BUILDING PERMIT PROCESS.

SE DOLPHIN AVENUE LIGHTS:

1. PER WARRENTON DESIGN STANDARDS 3.1.7 STREET LIGHTS HAVE BEEN PLACED EVERY 250 FEET AS REQUIRED FOR INDUSTRIAL DEVELOPMENT.
2. PROPOSED STREET LIGHTS IN SE DOLPHIN AVENUE RIGHT OF WAY TO MEET STANDARD PACIFIC POWER FIXTURES AND POLES.
3. STREET LIGHTS IN SE DOLPHIN AVENUE RIGHT OF WAY TO BE STANDARD PACIFIC POWER FIXTURES, POLES AND MOUNTING HEIGHT.



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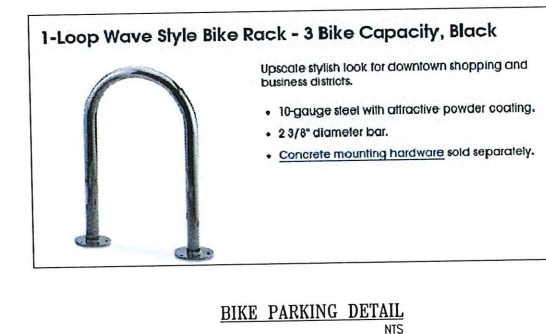
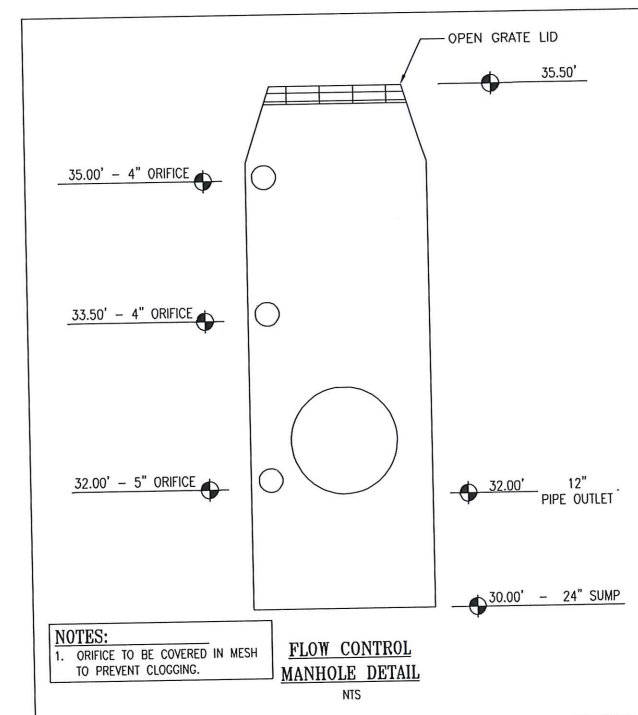
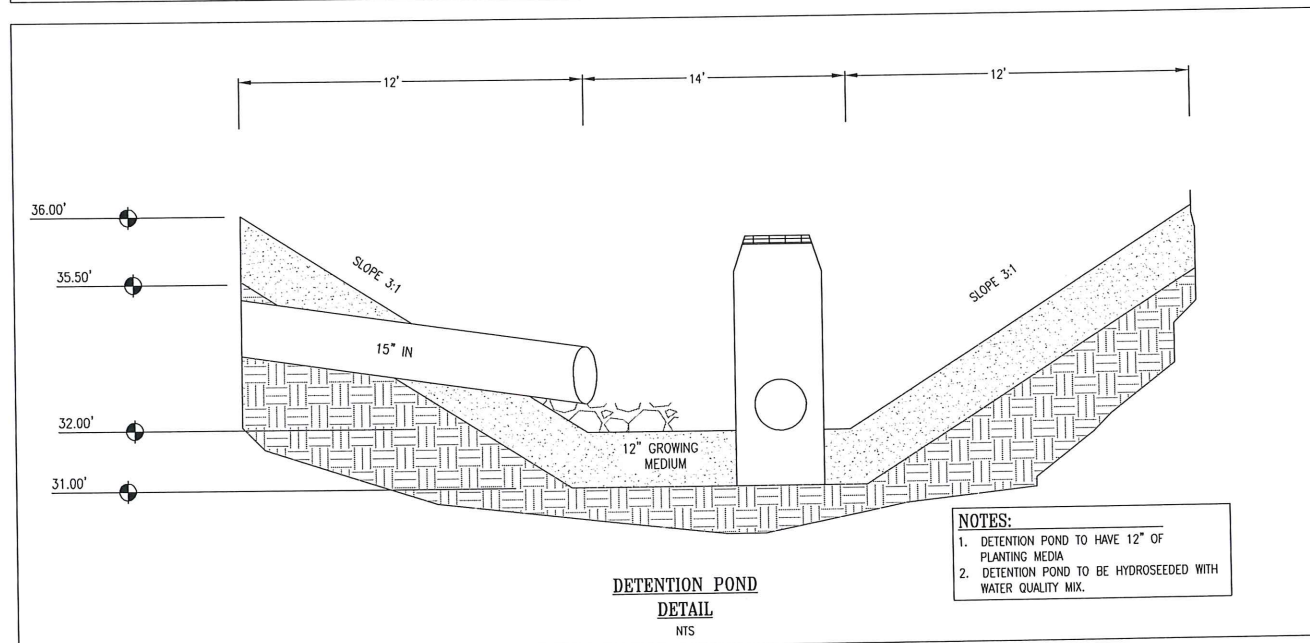
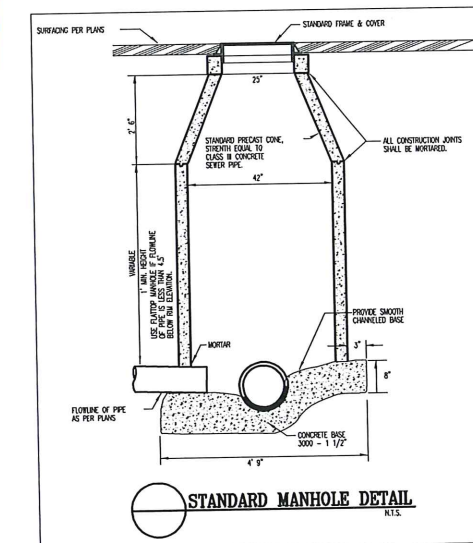
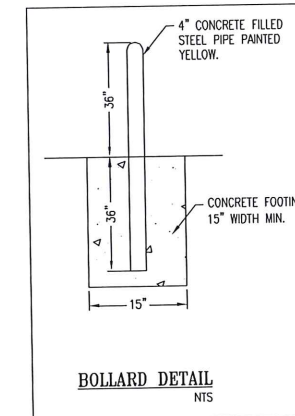
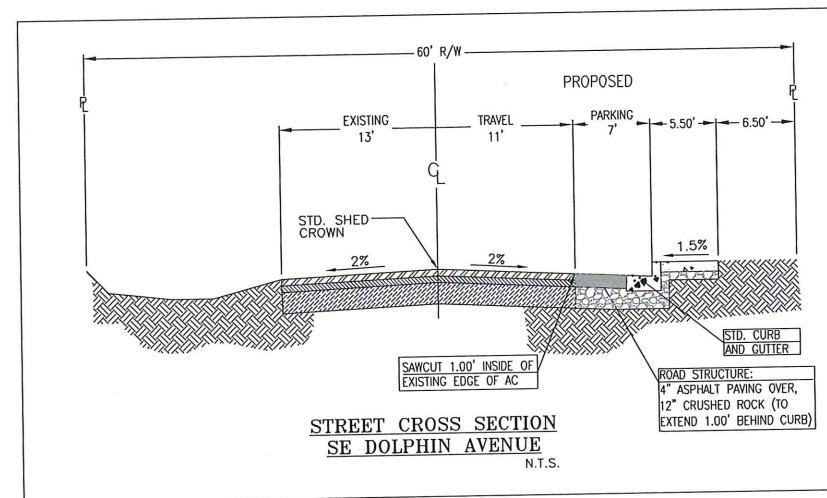
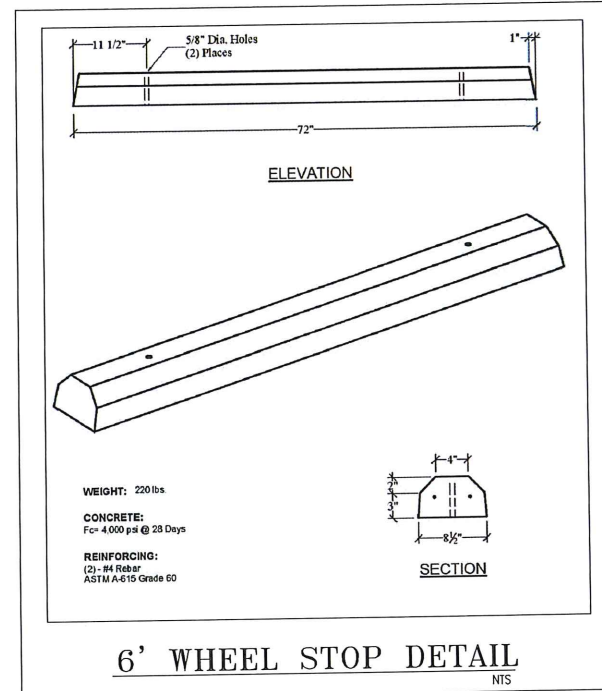
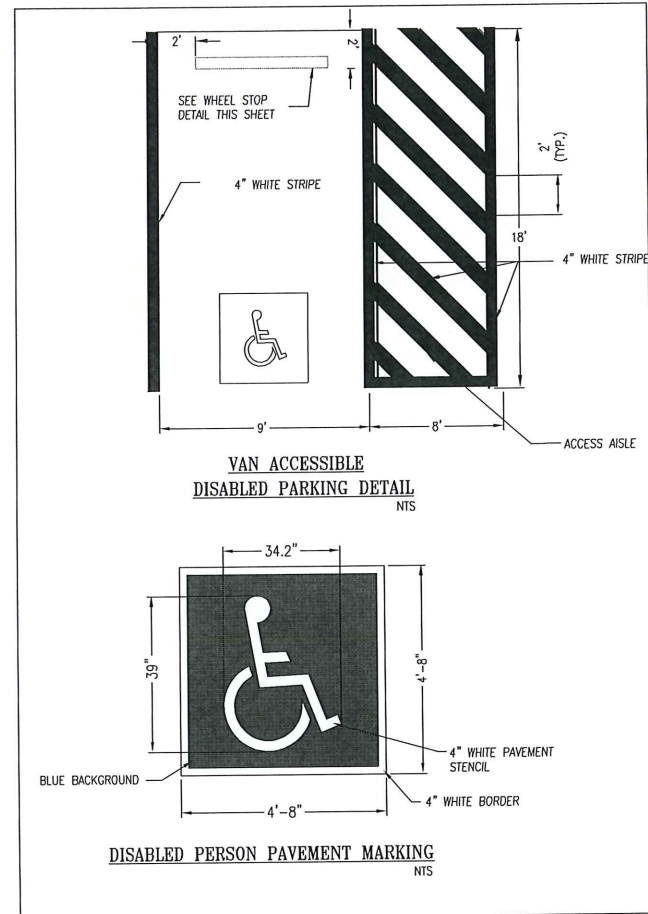


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CIVIL DETAILS/SE DOLPHIN AVENUE

C-5.0



Scale:



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SITE PLAN

L1

LEGEND

	PROPERTY LINE
	EXISTING CONTOURS
	PEDESTRIAN CONCRETE
	ASHPALT PAVING
	GRAVEL
	BIOSWALE - SEE COVERAGE TABLE
	DESIGNED LANDSCAPE AREA - SEE COVERAGE TABLE
	UNDEVELOPED OPEN SPACE - SEE COVERAGE TABLE
	BIKE RACK - 2.5' X 6' PER SPACE PER WARRENTON CODE SEC 16.128.040
	POLE LIGHT - SEE CIVIL

KEY NOTES

	CONCRETE CURB
	CONCRETE CURB AND GUTTER
	STAFF PARKING 21 SPACES AT 10' X 20' EACH EXCEPT WHERE NOTED 1 VAN ACCESSIBLE ADA SPACE AT 10'X20'
	FLEET PARKING 26 SPACES; 10' X 20' EACH
	FLEET PARKING 5 SPACES; 12' X 60' EACH
	ADD ALTERNATE: SEEDED LAWN
	CLEAR VISION TRIANGLE PER WARRENTON CODE SEC 16.132

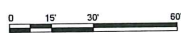
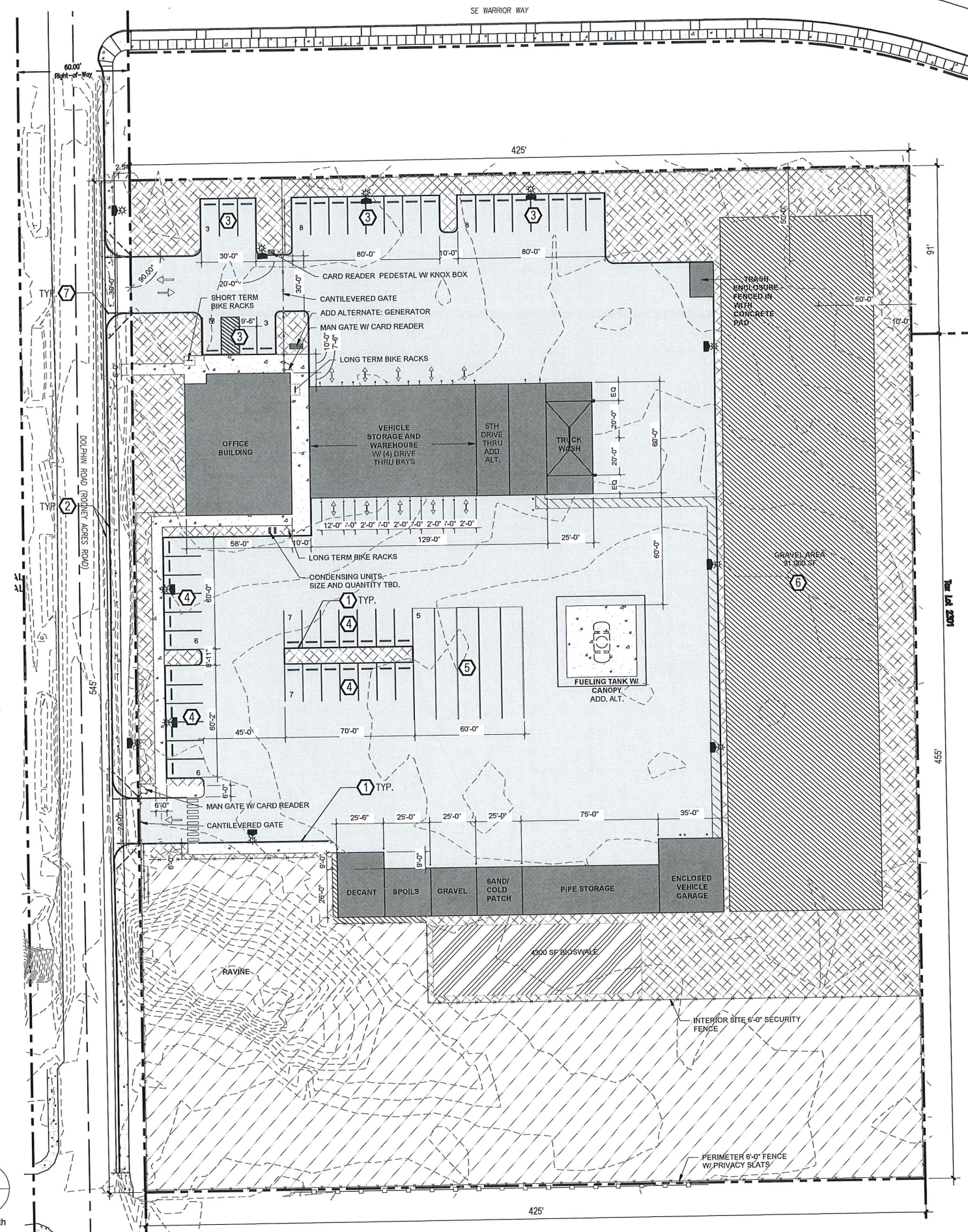
SITE SUMMARY

LOT COVERAGE	TOTALS
SITE	231,845.2 SF (5.32 AC)

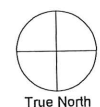
TOTAL IMPERMEABLE SURFACES	108,136 SF
PAVEMENT & DRIVES	86,442 SF
BUILDING	11,130 SF
OFFICE	4,300 SF
STOREROOM/ METER STORAGE	1,600 SF
WAREHOUSE	4,860 SF
(W/ 4 COVERED TRUCK BAYS)	
LANDA ROOM	370 SF
PIPE STORAGE	1,982 SF
SPOILS SHED	3,776 SF
TRASH ENCLOSURE	336 SF
LANDA CANOPY	1,500 SF
FUELING CANOPY	1,800 SF

SITE LANDSCAPE COVERAGE	
DESIGNED LANDSCAPE AREAS*	69,243 SF
UNDEVELOPED OPEN SPACE	51,819 SF
TOTAL LANDSCAPE AREA	121,062 SF
PERCENT LANDSCAPE COVERAGE OF TOTAL SITE	52.2%

*INCLUDES PLANT BEDS, BIOSWALE, AND GRAVEL AREAS



Scale:



NOT FOR CONSTRUCTION

LANDSCAPE PLAN

L2

LEGEND

- PROPERTY LINE
- EXISTING CONTOURS
- POLE LIGHT - SEE CIVIL
- STREET TREE

PARKING LANDSCAPE COVERAGE

VEHICULAR USE AREA	84,747 SF
INTERIOR LANDSCAPE AREA	10,270 SF
PERCENT COVERAGE OF VEHICULAR USE AREA	12.1%

PLANTING NOTES

1. ALL NEW LANDSCAPE AREAS SHALL BE WATERED WITH AN AUTOMATIC IRRIGATION SYSTEM.

PLANTING SCHEDULE

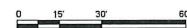
TREES	QTY	BOTANICAL NAME	COMMON NAME	SIZE & TYPE	SPACING
	8	ALNUS RUBRA	RED ALDER	2" CAL; B&B	AS SHOWN
	5	PICEA SITCHENSIS	SITKA SPRUCE	2" CAL; B&B	AS SHOWN
	8	PINUS CONTORTA VAR. CONTORTA	SHORE PINE	2" CAL; B&B	AS SHOWN
	6	TSUGA HETEROPHYLLA	WESTERN HEMLOCK	2" CAL; B&B	AS SHOWN
SHRUBS / GCVRS	QTY	BOTANICAL NAME	COMMON NAME	SIZE & TYPE	SPACING
	75	ARCTOSTAPHYLOS UVA-URSI 'VANCOUVER JADE'	VANCOUVER JADE MANZANITA	1 GAL CONT	AS SHOWN
	14	ARCTOSTAPHYLOS COLUMBIANA	HAIRY MANZANITA	5 GAL CONT	AS SHOWN
	178	CEANOTHUS GLORIOSUS	POINT REYES CREEPER	1 GAL CONT	AS SHOWN
	13	CEANOTHUS THYRSIFLORUS 'OREGON MIST'	OREGON MIST CEANOTHUS	5 GAL CONT	AS SHOWN
	13	CEANOTHUS VELUTINUS VAR. HOOKERI	HOOKER'S CEANOTHUS	5 GAL CONT	AS SHOWN
	37	ERIGERON GLAUCUS	BEACH ASTER	1 GAL CONT	AS SHOWN
	189	MAHONIA REPENS	CREEKING OREGON GRAPE	1 GAL CONT	AS SHOWN
	10	MYRICA CALIFORNICA	PACIFIC WAX MYRTL	5 GAL CONT	AS SHOWN
	2644	PENNISETUM ALOPECUROIDES 'LITTLE BUNNY'	LITTLE BUNNY FOUNTAIN GRASS	1 GAL CONT	12" O.C.
	66	SPIRAEA BETULIFOLIA 'TOR'	TOR SPIREA	3 GAL CONT	AS SHOWN
	37	VACCINIUM OVATUM	EVERGREEN HUCKLEBERRY	1 GAL CONT	AS SHOWN

STORMWATER PLANTINGS

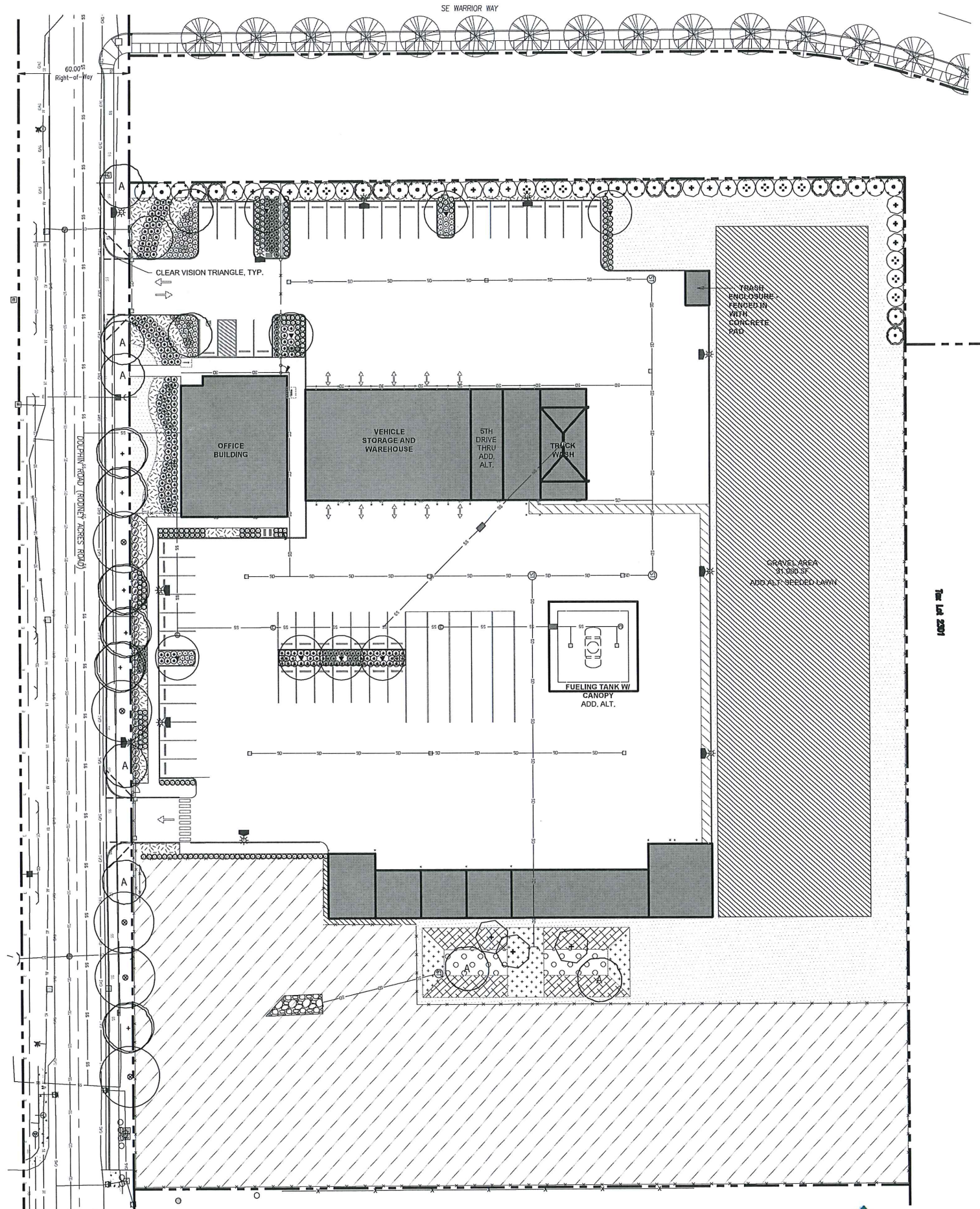
TREES	QTY	BOTANICAL NAME	COMMON NAME	SIZE & TYPE	SPACING
	3	ACER CIRCINATUM	VINE MAPLE	1" CAL; B&B	AS SHOWN
	2	ALNUS RUBRA	RED ALDER	1" CAL; B&B	AS SHOWN
SHRUBS / GCVRS	QTY	BOTANICAL NAME	COMMON NAME	SIZE & TYPE	SPACING
	1008	DESCHAMPSIA CESPITOSA	TUFTED HAIR GRASS	1 GAL CONT	18" o.c.
	503	JUNCUS TENUIS	SLENDER RUSH	1 GAL CONT	18" o.c.
	177	POLYSTICHUM MUNITUM	WESTERN SWORD FERN	1 GAL CONT	36" o.c.

LAWN AREA

SEED MIX	QTY BASE BID	QTY ADD ALT
	20,534 SF	51,534 SF



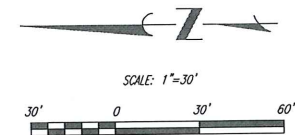
Scale:



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GRADING/DRAINAGE PLAN/SE DOLPHIN AVENUE

C-2.0



NOTES:

1. ELEVATIONS SHOWN ON THESE PLANS ARE BASED ON TOPO SURVEY DONE BY OLSON & MORRIS.
2. GRADING INCLUDES 1" DROP FROM BUILDING FINISH FLOOR ELEVATION TO BACK OF SIDEWALK.
3. CONTOURS SHOWN ON THIS SHEET ARE EXISTING. PROPOSED FINISHED GRADES ARE CALLED OUT ON PLAN SHEET.
4. ALL ADA FACILITIES SHALL MEET REQUIREMENTS OUTLINED IN AMERICANS WITH DISABILITY ACT, CURRENT EDITION.
5. DRAINAGE BASIN 01 TO BE TIED INTO EXISTING CATCH BASIN IN DOLPHIN RIGHT-OF-WAY.
6. FINAL PIPE ROUTING TO BE DETERMINED DURING BUILDING PERMIT PROCESS.
7. TRASH ENCLOSURE DETAIL SHOWN ON ARCHITECTURAL PLANS.
8. DETENTION POND AND FLOW CONTROL STRUCTURE DESIGNED TO PROCESS THE 100 YEAR STORM EVENT.
9. FLOW CONTROL STRUCTURE DESIGNED TO KEEP POST-CONSTRUCTION PERK FLOW RATE LEAVING THE SITE TO BE CONSISTENT WITH PRE-DEVELOPMENT PERK FLOW RATE.
10. DETENTION POND DETAILS SHOWN ON SHEET C-5.0.
11. FINAL GRADING TO BE DETERMINED DURING BUILDING PERMIT PROCESS.
12. RIP-RAP ARMORING TO BE DESIGNED DURING BUILDING PERMIT PROCESS.

Tax Lot 2302

ESTIMATED CUT/FILL VOLUMES

CUT = 5,480 CU. YD. OF EXISTING ONSITE MATERIAL
 FILL = 3,280 CU. YD. OF EXISTING ONSITE MATERIAL

LEGEND

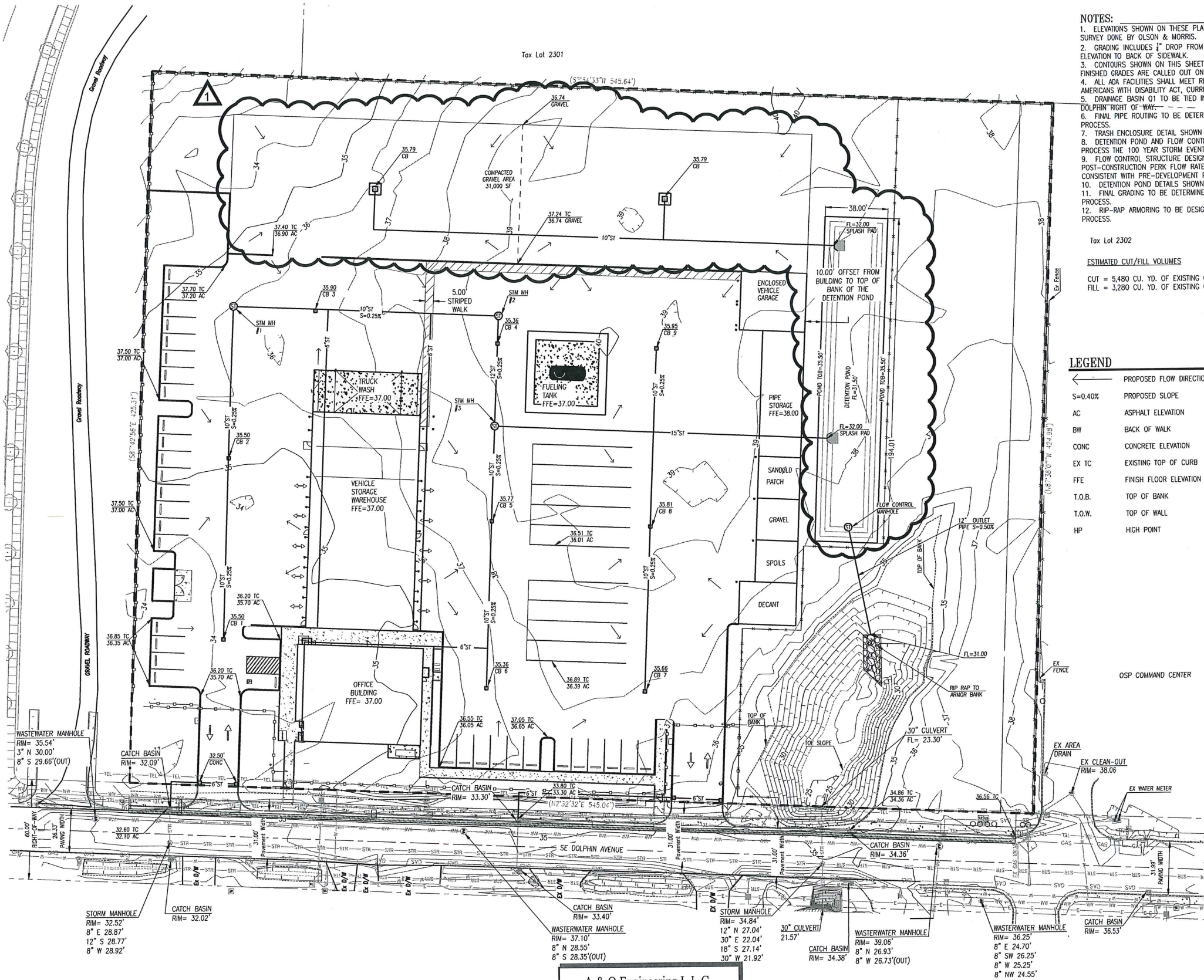
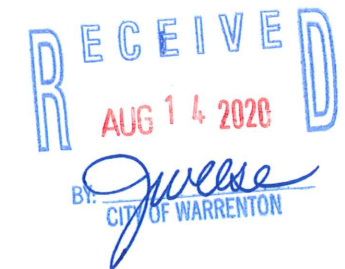
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LEGEND

- 484--- EXISTING CONTOURS
- - - - - EXISTING BOUNDARY
- - - - - ADJACENT PROPERTIES
- ===== EXISTING CURB LINE
- ===== EXISTING FENCE
- 8"W--- EXISTING WATER MAIN
- [W] EXISTING WATER METER
- [X] EXISTING WATER VALVE
- [FH] EXISTING FIRE HYDRANT
- [WW] 8"WW EXISTING WASTEWATER SYSTEM
- [C] EXISTING CLEANOUT
- [ST] 24"SD EXISTING STORM DRAINAGE SYSTEM
- [CB] EXISTING CATCH BASIN
- [CI] EXISTING CURB INLET
- [SL] EXISTING STREET LIGHT
- [UGE] EXISTING UNDERGROUND ELECTRIC
- [A] EXISTING TRANSFORMER
- [P] EXISTING TELEPHONE PEDESTAL
- [PHN] EXISTING TELEPHONE LINE
- 2"G--- EXISTING GAS MAIN
- [GV] EXISTING GAS VALVE
- [WM] PROPOSED WATER METER
- 8"WW PROPOSED WASTEWATER LINE
- PROPOSED WASTEWATER MANHOLE
- PROPOSED WASTEWATER CLEANOUT
- 12"ST PROPOSED STORM LINE
- [SM] PROPOSED STORM MANHOLE
- [SC] PROPOSED STORM CLEANOUT
- [CI] PROPOSED CURB INLET
- [UP] EXISTING UTILITY POLE
- [ET] EXISTING ELECTRIC TRANSFORMER
- [GA] EXISTING GUY ANCHOR
- [T] EXISTING TELECOMMUNICATION LINE
- [E] EXISTING OVERHEAD ELECTRIC
- [CUL] EXISTING STORMWATER CULVERT

REVISIONS

- 1 LUR REVISION 1 AUGUST 12, 2020



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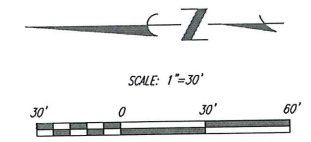
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UTILITY PLAN/SE DOLPHIN AVENUE

C-3.0



LEGEND

- BOUNDARY LINE
- - - ADJACENT LOT LINE
- EXISTING 1' CONTOUR LINE
- W --- EXISTING WATER METER AND PIPE
- F --- EXISTING FIRE HYDRANT
- V --- EXISTING WATER VALVE
- ST --- EXISTING STORM DRAINAGE SYSTEM
- GAS --- EXISTING GAS LINE
- CB --- EXISTING CATCH BASIN
- SC --- EXISTING STORMWATER CULVERT
- WW --- EXISTING WASTEWATER SYSTEM
- E --- EXISTING OVERHEAD ELECTRIC
- T --- EXISTING TELECOMMUNICATION LINE
- SL --- EXISTING STREET LIGHT
- ET --- EXISTING ELECTRIC TRANSFORMER
- UP --- EXISTING UTILITY POLE
- GA --- EXISTING GUY ANCHOR

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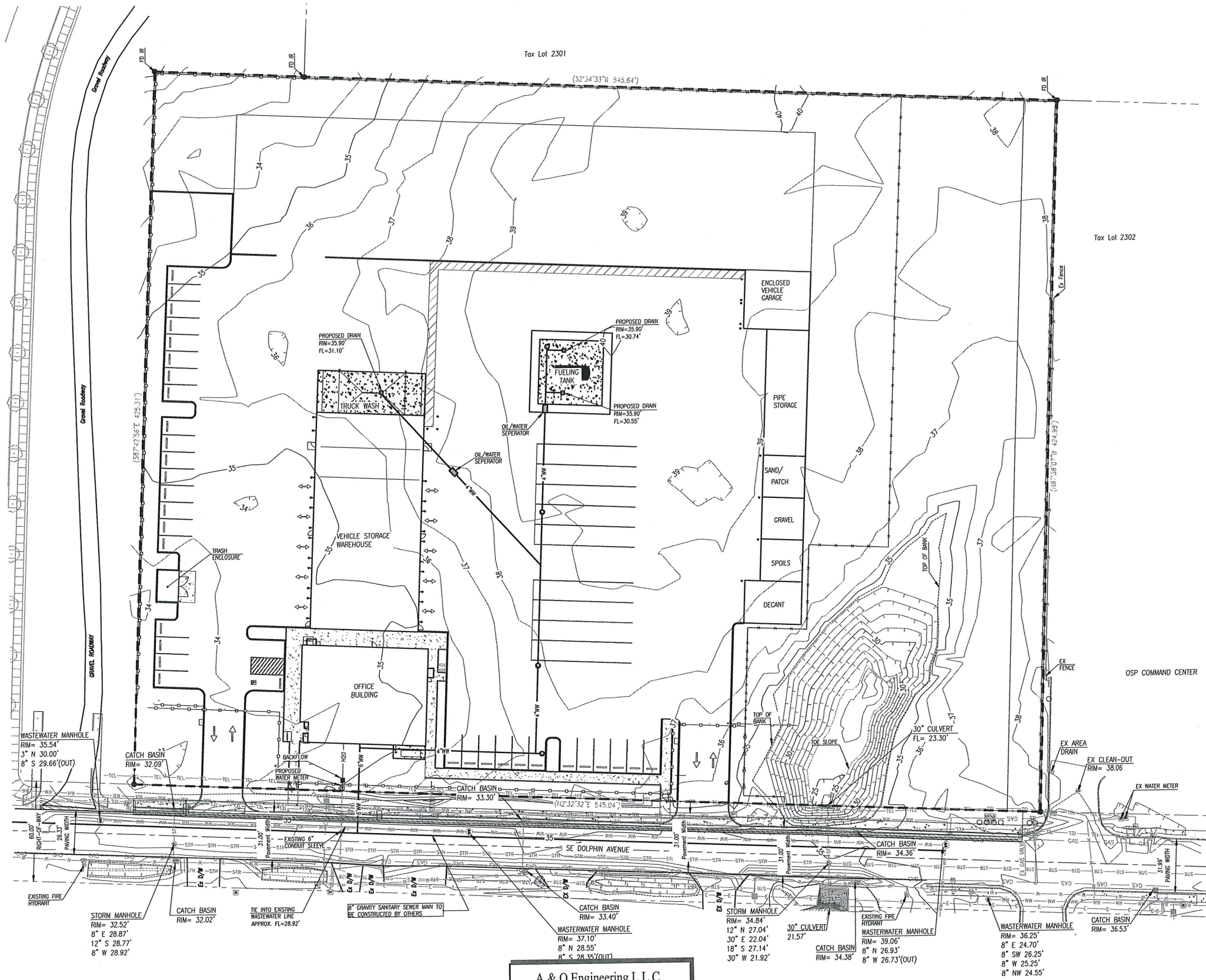
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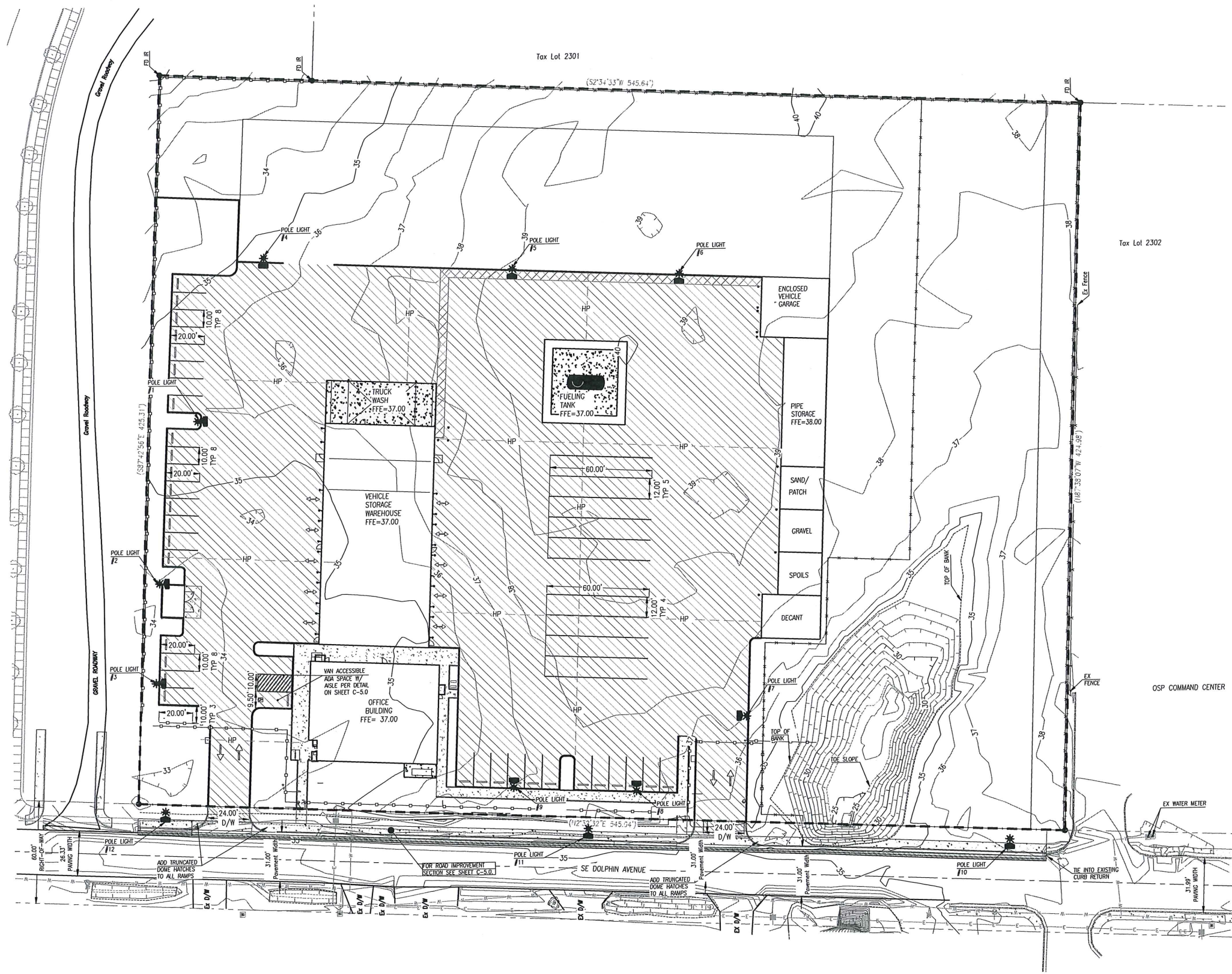
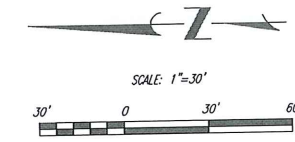
218113 | NWN Warrenton - Main - Site | SCHEMATIC DESIGN

NOT FOR CONSTRUCTION



PAVING/LIGHTING PLAN/SE DOLPHIN AVENUE

C-4.0



LEGEND

- EXISTING CONTOURS
- EXISTING BOUNDARY
- ADJACENT PROPERTIES
- EXISTING CURB LINE
- EXISTING FENCE
- PROPOSED POLE MOUNTED LIGHT
- PROPOSED PAVED AREA: 3" LEVEL 2 HMAC AC OVER 12" C.R. AS PER DETAIL.
- PROPOSED CONCRETE AREA: 6" 3000 PSI REINFORCED CONCRETE OVER 2" CRUSHED ROCK.

PAVING NOTES

1. CURBING SHOWN PROPOSED TO BE EXTRUDED. FINAL DESIGN TO BE COMPLETED DURING BUILDING PERMIT PROCESS.
2. ALL WALKWAYS SHALL BE DESIGNED TO MEET ADA STANDARDS. FINAL DESIGN OF ALL ADA FACILITIES DURING BUILDING PERMIT.
3. DOLPHIN ROAD WIDENING PROPOSED TO BE PER WARRENTON DESIGN STANDARDS. SEE STREET CROSS SECTION ON SHEET C-5.0
4. AC PAVING SECTION PROPOSED TO BE 3" AC OVER 12" CRUSHED ROCK PER PAVING RECOMMENDATIONS IN GEOTECHNICAL REPORT PREPARED BY EARTH ENGINEERS INC.
5. DOLPHIN ROAD IMPROVEMENTS TO BE DESIGNED AND PERMITTED THROUGH CITY PIP PERMIT PROCESS.
6. WHEEL STOPS INSTALLED PER DETAIL SHOWN ON SHEET C-5.0.
7. ADA RAMPS TO BE DESIGNED DURING BUILDING PERMIT PROCESS.

SE DOLPHIN AVENUE LIGHTS:

1. PER WARRENTON DESIGN STANDARDS 3.1.7 STREET LIGHTS HAVE BEEN PLACED EVERY 250 FEET AS REQUIRED FOR INDUSTRIAL DEVELOPMENT.
2. PROPOSED STREET LIGHTS IN SE DOLPHIN AVENUE RIGHT OF WAY TO MEET STANDARD PACIFIC POWER FIXTURES AND POLES.
3. STREET LIGHTS IN SE DOLPHIN AVENUE RIGHT OF WAY TO BE STANDARD PACIFIC POWER FIXTURES, POLES AND MOUNTING HEIGHT.

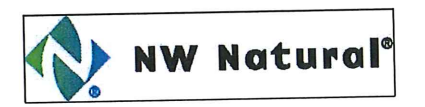


218113 | NWN Warrenton - Main - Site | SCHEMATIC DESIGN 05/29/20

A & O Engineering L.L.C.
 CONSULTING ENGINEERS & SITE DEVELOPMENT PLANNING
 380 Q ST. SUITE 200
 SPRINGFIELD, OR. 97477
 PHONE: (541) 302-9790
 info@aao-engr.com



NOT FOR CONSTRUCTION



SITE PLAN

L1

LEGEND

	PROPERTY LINE
	EXISTING CONTOUR
	PEDESTRIAN CONCRETE
	ASHPALT PAVING
	GRAVEL
	BIOSWALE - SEE COVERAGE TABLE
	DESIGNED LANDSCAPE AREA - SEE COVERAGE TABLE
	UNDEVELOPED OPEN SPACE - SEE COVERAGE TABLE
	BIKE RACK - 2.5' X 6' PER SPACE PER WARRENTON CODE SEC 16.128.040
	POLE LIGHT - SEE CIVIL

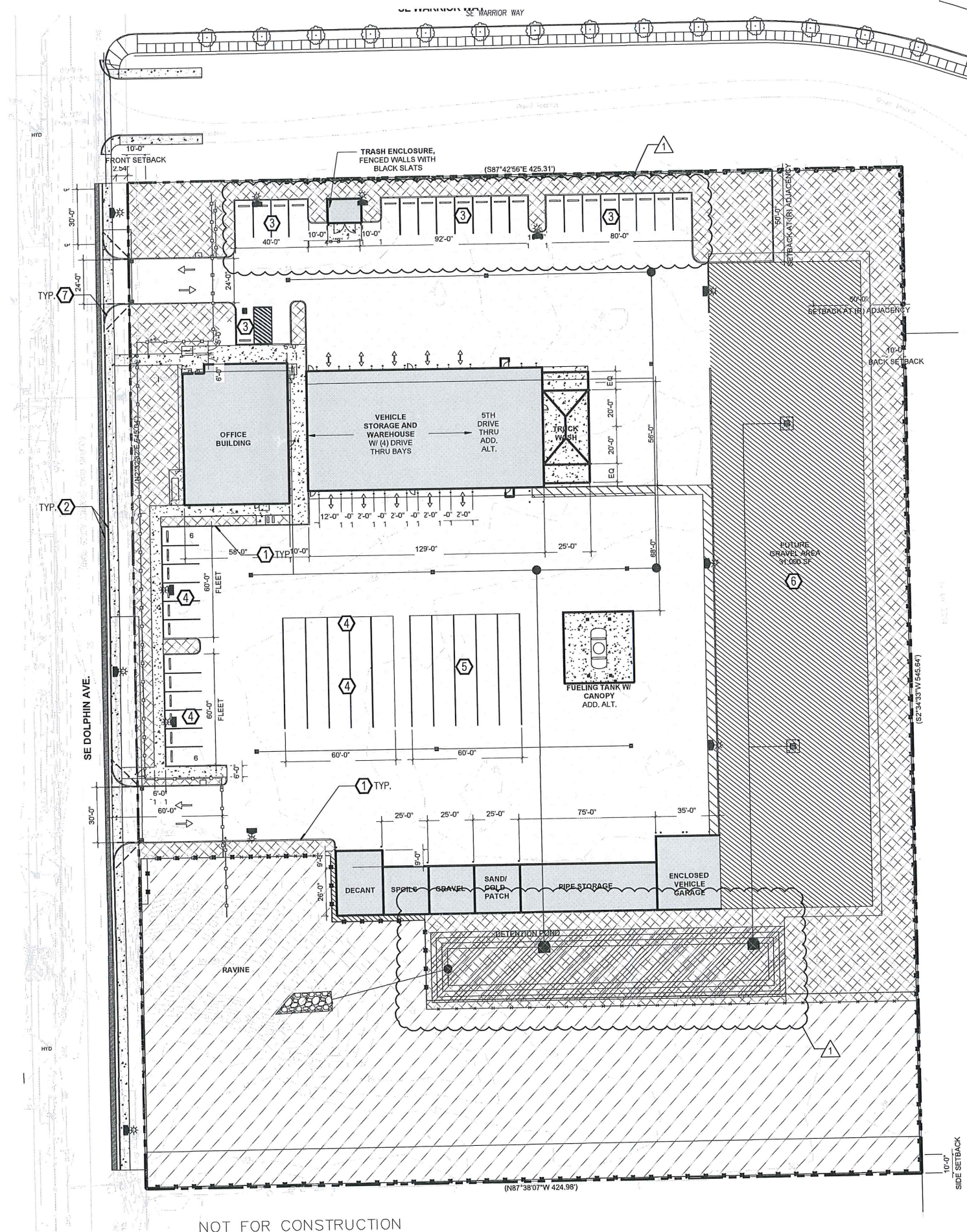
KEY NOTES

①	CONCRETE CURB
②	CONCRETE CURB AND GUTTER
③	STAFF PARKING 21 SPACES AT 10' X 20' EACH EXCEPT WHERE NOTED 1 VAN ACCESSIBLE ADA SPACE AT 10'X20'
④	FLEET PARKING 26 SPACES; 10' X 20' EACH
⑤	FLEET PARKING 5 SPACES; 12' X 60' EACH
⑥	ADD ALTERNATE: SEEDED LAWN
⑦	CLEAR VISION TRIANGLE PER WARRENTON CODE SEC 16.132

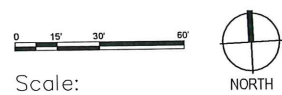
SITE LANDSCAPE COVERAGE

DESIGNED LANDSCAPE AREAS*	69,243 SF
UNDEVELOPED OPEN SPACE	51,819 SF
TOTAL LANDSCAPE AREA	121,062 SF
PERCENT LANDSCAPE COVERAGE OF TOTAL SITE	52.2%

*INCLUDES PLANT BEDS, BIOSWALE, AND GRAVEL AREAS



NOT FOR CONSTRUCTION



Scale:

NORTH

REVISIONS

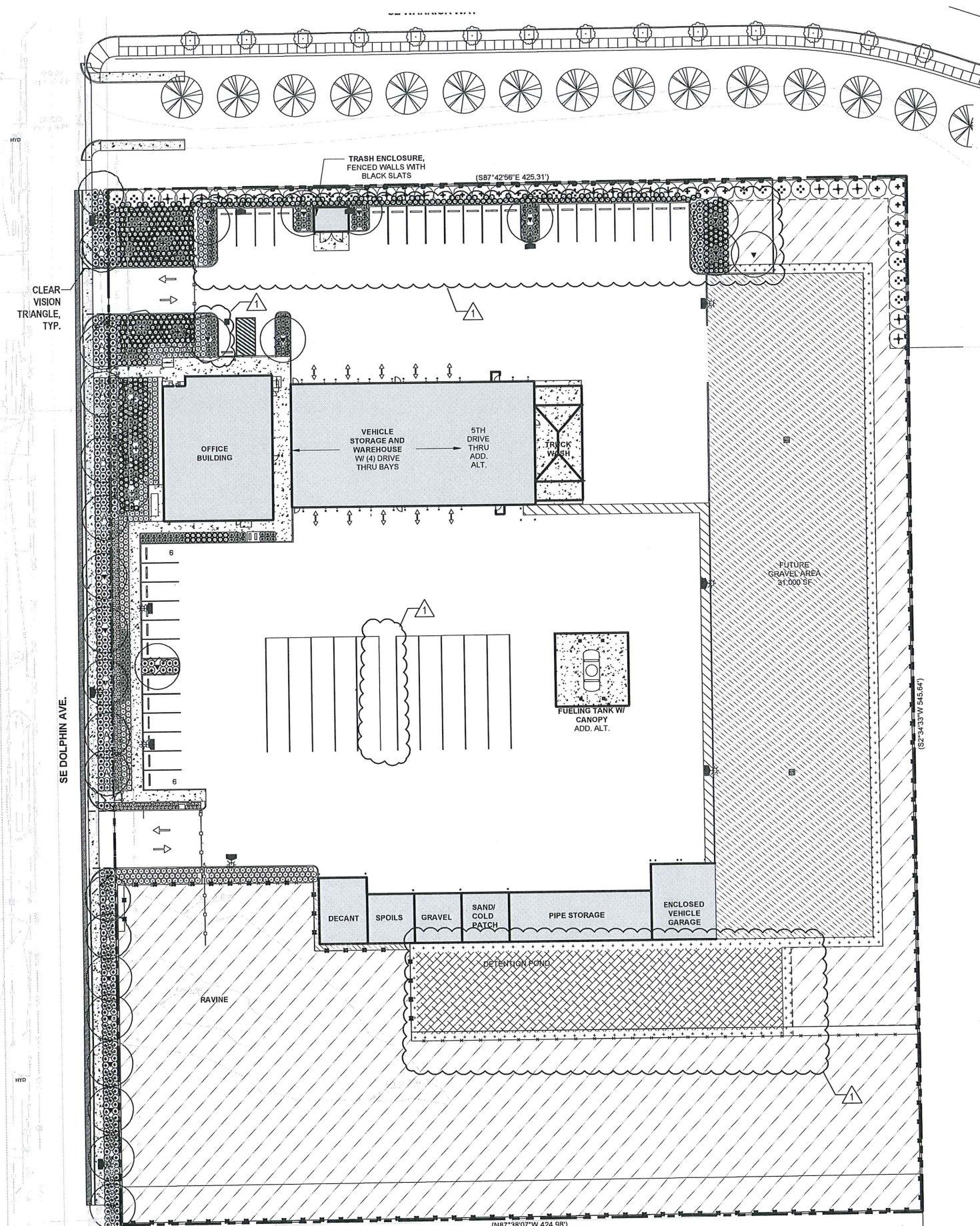
①	LUR Revision 1	August 14, 2020
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SITE PLAN

L2

LEGEND

	PROPERTY LINE
	GRAVEL
	UNDEVELOPED AREA
	STREET TREE - NIC
	LINEAR ROOT BARRIER - 24" DEPTH



PLANTING SCHEDULE

TREES	QTY	BOTANICAL NAME	COMMON NAME	SIZE & TYPE	SPACING
	10	ALNUS RUBRA	RED ALDER	2" CAL; B&B	AS SHOWN
	8	FRANGULA PURSHIANA	CASCARA	1.5" CAL; B&B	AS SHOWN
	19	PINUS CONTORTA VAR. CONTORTA	SHORE PINE	2" CAL; B&B	AS SHOWN
SHRUBS / GCVRs	QTY	BOTANICAL NAME	COMMON NAME	SIZE & TYPE	SPACING
	14	ARCTOSTAPHYLOS COLUMBIANA	HAIRY MANZANITA	5 GAL CONT	AS SHOWN
		CALAMAGROSTIS ACUTIFLOR 'KARL FOERSTER'	FEATHER REED GRASS	3 GAL CONT	AS SHOWN
	178	CEANOTHUS X 'CENTENNIAL'	CENTENNIAL CEANOTHUS	1 GAL CONT	AS SHOWN
	13	CEANOTHUS VELUTINUS VAR. HOOKERI	HOOKER'S CEANOTHUS	5 GAL CONT	AS SHOWN
	189	MAHONIA REPENS	CREEKPING OREGON GRAPE	1 GAL CONT	AS SHOWN
	10	MYRICA CALIFORNICA	PACIFIC WAX MYRTL	5 GAL CONT	AS SHOWN
	2644	POLYSTICHUM MUNITUM	SWORD FERN	3 GAL CONT	12' O.C.
	66	SPIRAEA BETULIFOLIA 'TOR'	TOR SPIREA	3 GAL CONT	AS SHOWN

STORMWATER PLANTING

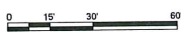
SEEDING: SUNMARK NATIVE WATER QUALITY MIX

SEED	QTY	BOTANICAL NAME	COMMON NAME	RATE
	8,300 SF	BECHMANIA SYZIGACHNE	AMERICAN SLOUGHGRASS	1 LB/1000 SF
		DESCHAMPSIA CESPITOSA	TUFTED HAIR GRASS	
		ELYMUS GLAUCUS	BLUE WILDRYE	
		FESTUCA RUBRA RUBRA	NATIVE RED FESCUE	
		GLYCERIA OCCIDENTALLIS	WESTERN MANNAGRASS	

LAWN AREA

SEEDING: SUNMARK NATIVE ECO TURF MIX

SEED	QTY	BOTANICAL NAME	COMMON NAME	RATE
	4,230 SF	BOUTELOUA GRACILIS	BLUE GRAMA	1 LB/1000 SF
		BUCHOLE DACTYLOIDES	BUFFALOGRASS	
		FESTUCA RUBRA RUBRA	NATIVE RED FESCUE	
		KOELERIA MACRANTHA	PRAIRIE JUNEGRASS	
		TRIFOLIUM FRAGIFERUM	STRAWBERRY COVER	



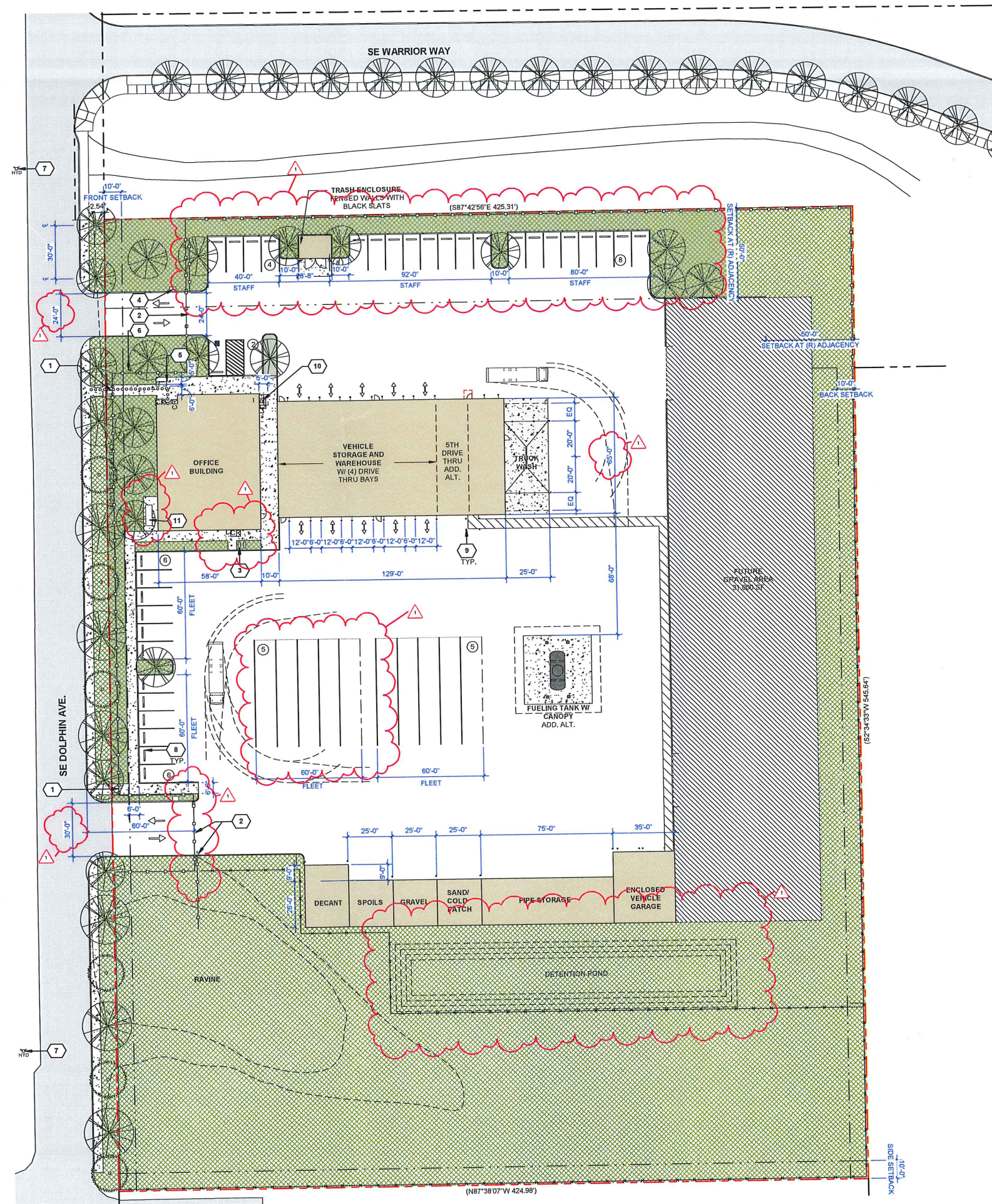
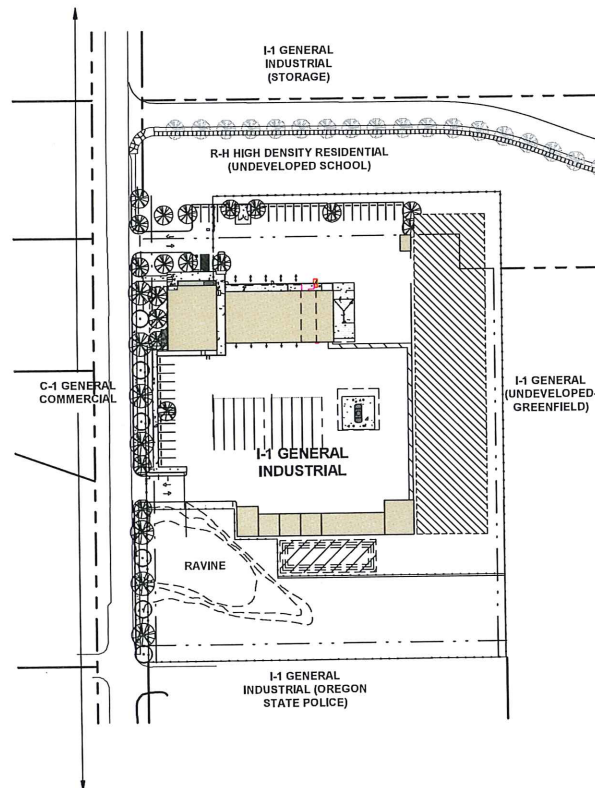
Scale:

REVISIONS

	LUR Revision 1	August 14, 2020
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SITE PLAN

A1



KEYNOTES

- 1 MAN GATE WITH CARD READER
- 2 CANTILEVERED GATE
- 3 CONDENSING UNITS, SIZE AND QUANTITY TO BE DETERMINED
- 4 PEDASTAL MOUNTED CARD READER AND KNOX BOX
- 5 SHORT TERM BICYCLE RACKS
- 6 FLAG POLE
- 7 FIRE HYDRANT
- 8 WHEEL STOPS
- 9 CONCRETE FILLED BOLLARDS WITH YELLOW PLASTIC SLEEVE
- 10 LONG TERM BICYCLE RACKS
- 11 ADD ALTERNATE: GENERATOR

LEGEND

- LANDSCAPED REGIONS
- UNDEVELOPED LAND
- SIDEWALK
- GRAVEL
- PROPOSED BUILDING
- PROPERTY LINE
- SETBACK LINE
- 7'-0" FENCE WITH BLACK PRIVACY SLATS
- 7'-0" FENCE
- EXISTING CONCRETE SIDEWALK
- SITE EGRESS PATH TO RIGHT OF WAY
- ENTRY/EXIT ACCESS TO ACCESSIBLE MEANS OF EGRESS
- PUBLIC ENTRY ON ACCESSIBLE ROUTE
- SITE POLE LIGHTING
- NO. OF PARKING SPACES IN GROUP
- TRAFFIC ARROW
- FIRE HYDRANT
- EXISTING FIRE HYDRANT
- FIRE DEPARTMENT CONNECTION
- 123'-0" SPOT GRADE ELEVATION
- F.F. 123' FINISH FLOOR ELEVATION

LOT COVERAGE	TOTALS
SITE	231,847.64 SF (5.32 AC)
TOTAL IMPERMEABLE SURFACES	109,278.4 SF
PAVEMENT & DRIVES	82,351.4 SF
BUILDING TOTAL	22,434 SF
OFFICE	4,408 SF
WAREHOUSE (W/ 5 COVERED TRUCK BAYS)	7,740 SF
LANDSCAPE CANOPY	1,500 SF
PIPE STORAGE	1,950 SF
SPOILS AND DECANT BAYS	2,842 SF
ENCLOSED VEHICLE STORAGE GARAGE	1,399 SF
TRASH ENCLOSURE	243 SF
FUELING CANOPY	2,352 SF
TOTAL LANDSCAPE	122,869.24 SF
DESIGNED LANDSCAPE AREAS	32,794 SF
GRAVEL AREA	31,000 SF
BIOSWALE TOTAL	7,500 SF
UNDEVELOPED OPEN SPACE	70,331 SF
PARKING	
FRONT AREA	6
STAFF	16
FLEET	28
TRUCK BAYS (NOT COVERED)	5
TRUCK BAYS (COVERED)	5

REVISIONS

- LUR Revision 1 August 14, 2020

Scale: As indicated

NOT FOR CONSTRUCTION

LRS ARCHITECTS 218113 | NWN Warrenton Resource Center | 50% DESIGN DEVELOPMENT 08/13/20





P.O. Box 973
Seaside, Oregon 97138
(503) 468-8600
adam@amengnr.com

August 25, 2020

Mark Barnes
Community Development Director, Interim
City of Warrenton
225 S Main Ave
Warrenton, OR 97146

RE: NW Natural Warrenton Resource Center – Site Design Review (WMC 16.212.040)

Dear Mr. Barnes,

The City of Warrenton has engaged A.M. Engineering (AME) to provide site design review support consulting services.

The planning review submittal package has been provided to AME. The drawing set, Impact Study, Pre Stormwater Report, and Pre-App Meeting notes have been reviewed. This letter summarizes my review comments of this information provided to me, by LRS Architects, Mary Fierros Bower on 8/10/20 and revised sheets provided 8/17/20. The site is located on S.E. Dolphin Avenue southeast of Highway 101.

As submitted, the development consists of an office building with several outbuildings, associated utilities, parking lot, and half street improvements on SE Dolphin Avenue.

The submitted information has been reviewed for compliance with the City of Warrenton Municipal Code Site Design Review, Section 16.212.040 and additional referenced sections.

General Notes:

- Construction Documents shall meet all requirements of federal, state, and local standards, codes, ordinances, guidelines and/or other legal requirements.
- The developer is required to follow the City of Warrenton Development Standards. These standards can be found in Title 16 of the Warrenton Municipal Code. Please provide documentation showing how this development will meet the standards set forth in the development code.
- The developer must follow the City's Water and Sewer Regulations. These regulations are included under Title 13 of the Warrenton Municipal Code.
- The developer is required to follow the Engineering Standards & Design Criteria Manual. Please provide documentation showing how the development meets the standards set forth in this manual.
- Grading and Erosion Control activities shall adhere to the requirements of the Approved Oregon Department of Environmental Quality General Permit for Stormwater Discharge, 1200-C when applicable.
- Grading activities shall adhere to the recommendations provided in the project final Geotechnical report when applicable.



P.O. Box 973
Seaside, Oregon 97138
(503) 468-8600
adam@amengnr.com

Comments:

- 1 Please provide the sidewalk dimension on the plan.
- 2 Provide entire tax lot and the surrounding property to a distance sufficient to determine the location of the development in the City, and the relationship between the proposed development site and adjacent property and development. The property boundaries, dimensions and gross area shall be identified. Please provide an additional map showing the site in relation to the rest of the City.
- 3 Provide identification of slopes greater than 10%.
- 4 Please confirm there are no existing easements on the site. There appears to be utilities along Dolphin that are located on private property.
- 5 Provide name and address of applicant, project designer, engineer, architect, surveyor, and/or planner, if applicable on the cover sheet.
- 6 Locate proposed water meter in the right of way or provide an easement.
- 7 Provide driveway entrance winged configuration per code.
- 8 The City of Warrenton utilizes ODOT standard drawings and details for public facilities construction. Coordinate, and modify as appropriate, any ODOT drawings with the City's Engineering Design Standards Adopted April 2020.
- 9 The impact study, wastewater section, states the adjacent pump station was constructed to accommodate the full buildout of an industrial area. Provide adequate documentation confirming the statement or an analysis of the existing conditions that demonstrates available capacity.
- 10 Provide a wastewater demand for the office/warehouse.
- 11 Provide a wastewater demand for the decant bin.
- 12 The "Landa" pre-treatment appears to be an alternate to the base scope. Provide wastewater demand for both scenarios.
- 13 Provide additional information on the recycled wash water facilities and process. Where is the recycled water stored; how is it reincorporated into the washing system and isolated from the domestic water service and wastewater drain? A narrative explanation should be sufficient.
- 14 The wastewater adverse effect statement should be based on the analysis of available capacity and the proposed demand if a statement cannot be provided to confirm the station was constructed to accommodate the full buildout of an industrial area.
- 15 Please provide a basin map for the stormwater report.
- 16 Fire hydrants are not shown in the plans. Coordinate with the Fire chief for hydrant spacing and ensure compliance with the City code.
- 17 Sheet C2.0, update excavation and fill quantities,
- 18 Sheet C4.0, update driveway width dimensions.
- 19 The revised stormwater report calculations now include an exfiltration component in the detention basin. The body of the report does not mention exfiltration. Provide references to support the inclusion of exfiltration.
- 20 The refuse location/layout doesn't meet City standards.
- 21 Both fuel area and wash area will need to be completely covered and all water entering the sewer system will need to be treated before entering the public sewer system. The developer will be required to apply for a discharge permit for each location. The discharge permit will include discharge limits and required testing.



P.O. Box 973
Seaside, Oregon 97138
(503) 468-8600
adam@amengnr.com

If there are any questions, please don't hesitate to call me.

Sincerely,

A handwritten signature in blue ink that reads "Adam Dailey". The signature is written in a cursive, flowing style.

Adam Dailey, P.E.
President

Enclosures: None

Cc: Client, File

Janice Weese

From: Mary Fierros-Bower <mfierrosbower@lrsarchitects.com>
Sent: Friday, August 14, 2020 11:32 AM
To: Mark Barnes; Adam; Janice Weese
Cc: Skye Woodhouse; 218113 NWN Warrenton Resource Center
Subject: NWN Warrenton - Parking/landscape LUR Revision 1
Attachments: NWN Warrenton - LUR Revision 1 Rev Sheets Only.pdf

Hi Mark and Adam,

Please see attached for revised sheets only to the LUR drawing set. This includes parking landscape revisions to the site including a revision to the detention pond incorporating the compacted gravel area, see updated civil and landscape plans. Also included are other misc. revisions to the site plan, see Sheet A1 showing changes clouded.

We will be pursuing Option (c) as noted in your email below, putting in extra landscape islands and trees in the part of the parking lot dedicated to normal-sized vehicles (staff parking), arguing that the standard is met when averaged over the entire site.

Adam, the civil engineer, Scott Morris plans to update the stormwater report to reflect the change to the detention pond over the weekend and will provide it separately on Monday as supplemental information.

Please let me know if you would like a compiled LUR Revision 1 drawing set versus just the revised sheets.

Thanks! Mary

Mary Fierros Bower | Architect, Associate, Project Manager, LEED® AP, NCARB | LRS Architects | Portland | Shanghai
503.265.1572 direct · 720 NW Davis Street Suite 300 · Portland, OR 97209 · www.lrsarchitects.com · [vCard](#)

From: Mark Barnes <cityplanner@ci.warrenton.or.us>
Sent: Friday, August 7, 2020 4:12 PM
To: Mary Fierros-Bower <mfierrosbower@lrsarchitects.com>
Cc: Skye Woodhouse <swoodhouse@lrsarchitects.com>; 218113 NWN Warrenton Resource Center <218113@lrsarchitects.com>
Subject: Re: NWN Warrenton - Parking/landscape clarification and Perimeter Fence Height

Hi Mary;

1. Question listed in Skye's email below. NW Natural prefers to keep the fleet parking area clear of landscape islands / trees to provide for safe truck turning radius'. Please confirm.

I think the applicable standard here is this language from 16.124.070.E.2: "All parking areas with more than 20 spaces shall include landscape islands with trees to break up the parking area into rows of not more than 12 contiguous parking spaces."

If Kevin Cronin gave you guidance on this point, then run with that; however, I can't guarantee that the Planning Commission will follow that interpretation. As I see it, you have several options: (a) apply for a variance to this standard; (b) bite the bullet and follow this standard, but make your aisles and turn radii big enough to accommodate your oversized trucks and trailers; or (c) put in extra landscape islands and trees in the part of the parking lot dedicated to normal-sized vehicles, arguing that the standard is met when averaged over the entire site.

2. NW Natural typically provides a 7' high chain link fence around the perimeter of their Resource Centers for security reasons. However, the zoning code states a maximum of 6' high is allowed, thus our LUR drawings show 6' high for this reason. We would like approval to deviate from the 6' height requirement to 7' high. Please confirm.

I can't give you administrative approval to deviate from this height. The applicable standard is in 16.124.050.B.2: "Fences in the General Industrial (I-1) Zone may exceed six feet if necessary to protect the welfare of the general public (i.e., airport runway safety, military, coast guard, or homeland security defense facilities, etc.) but not for protection of private property (i.e., auto repair lots, equipment yards, woodworking shops, etc.)."

You can seek a variance to this standard, but your case for a variance will have to be strong. I don't know what the Planning Commission has done in the past with this kind of variance. Take a look at the variance criteria in 16.272.020.

If you want to pursue a variance for either or both of these elements, I'll need that application material from you very soon. I will re-advertise this no later than 8/21 for the 9/10 hearing, and I will have to write an expanded staff report. Also, I'll need a revised plan-set with the changes clouded for engineering review. I'm not working in City Hall Monday or Tuesday, but if you would like to talk about this, call me on my mobile phone: 503 338 9493.

Mark Barnes
Interim City Planner
503 861 0920

From: Mary Fierros-Bower <mfierrosbower@lrsarchitects.com>
Sent: Friday, August 7, 2020 12:49 PM
To: Mark Barnes <cityplanner@ci.warrenton.or.us>
Cc: Skye Woodhouse <swoodhouse@lrsarchitects.com>; 218113 NWN Warrenton Resource Center <218113@lrsarchitects.com>
Subject: NWN Warrenton - Parking/landscape clarification and Perimeter Fence Height

Hi Mark,

Have you had a chance to review the question below? Thanks! Mary

Mary Fierros Bower | Architect, Associate, Project Manager, LEED® AP, NCARB | LRS Architects | Portland |

Shanghai

503.265.1572 direct · 720 NW Davis Street Suite 300 · Portland, OR 97209 · www.lrsarchitects.com · vCard

From: Mary Fierros-Bower

Sent: Wednesday, August 5, 2020 9:52 AM

To: Mark Barnes <cityplanner@ci.warrenton.or.us>

Cc: 'Janice Weese' <jweese@ci.warrenton.or.us>; Skye Woodhouse <swoodhouse@lrsarchitects.com>; 218113 NWN

Warrenton Resource Center <218113@lrsarchitects.com>

Subject: NWN Warrenton - Parking/landscape clarification and Perimeter Fence Height

Hi Mark,

We have a couple of zoning questions.

1. Question listed in Skye's email below. NW Natural prefers to keep the fleet parking area clear of landscape islands / trees to provide for safe truck turning radius'. Please confirm.
2. NW Natural typically provides a 7' high chain link fence around the perimeter of their Resource Centers for security reasons. However, the zoning code states a maximum of 6' high is allowed, thus our LUR drawings show 6' high for this reason. We would like approval to deviate from the 6' height requirement to 7' high. Please confirm.

Thank you! Mary

Mary Fierros Bower | Architect, Associate, Project Manager, LEED® AP, NCARB | LRS Architects | Portland |

Shanghai

503.265.1572 direct · 720 NW Davis Street Suite 300 · Portland, OR 97209 · www.lrsarchitects.com · vCard

From: Skye Woodhouse

Sent: Monday, July 20, 2020 4:30 PM

To: 'cityplanner@ci.warrenton.or.us' <cityplanner@ci.warrenton.or.us>

Cc: 218113 NWN Warrenton Resource Center <218113@lrsarchitects.com>

Subject: NWN Warrenton - Parking/landscape clarification

Good afternoon Mark,

We had been working with Kevin Cronin on a parking clarification for the NWN Warrenton Project and we still have some questions, just to make certain we understand the code and are in accordance moving forward. The red text at the bottom is the main question.

For background, this was Kevin's original comment:

- **Parking:** The off street parking standard for this proposal is: *1 space per 2 employees on the largest shift, plus 1 space per company vehicle; a minimum of 2 spaces is required. There is no maximum required. There are also reduction standards to allocate land for other purposes. Fleet only parking can be designated separately from day to day employee parking.* Bike parking is required. Please refer to specific design standards.

Does this mean there is a difference in how you define staff and fleet parking? Can the number of fleet vehicle spaces be counted independently from the minimum off-street staff parking requirement totals? Can fleet be treated as storage yard area?

We currently have 22 staff parking space allocated on the site and a total of 26 fleet vehicles parking spaces + 5 long truck / trailer parking spaces.

1. There will be a total of 9 employees at this site:

1 parking space per 2 employees + 1 parking space per 1 company vehicle (each employee is assumed to have 1 company vehicle)

$(1 \times 5) + (1 \times 9) = (14)$ staff parking spaces are required.

THE MAIN QUESTION IS: If the 26 fleet parking spaces are treated separately from the staff parking, do we need to apply the landscape standards to the areas? Is there a chance we can remove that planter at the center to provide better circulation through the site. See attached, highlighted.

Skye Woodhouse | LRS Architects | Portland | Shanghai

971.242.8177 direct · 720 NW Davis Street Suite 300 · Portland, OR 97209 · www.lrsarchitects.com · vCard



CITY OF WARRENTON
Pre-application Conference
Warrenton Development Code - Chapter 16.208.070
Wednesdays @ 11 am

FEE \$150.00
 File# _____
 Date Received _____
 Receipt# _____

Subject Property Site Address: SE Dolphin Avenue, Warrenton, Oregon

Township 8 range 10, section 34, Tax Lot (s) 2300

Lot Square Footage/Acres 231,739 SF Zone I1, General Industrial

Property Owner Wayne Pipes, NW Natural Phone/text 503.226.4211 X2496

Mailing Address 250 SW Taylor Street, Portland, OR 97204-3038

Email Wayne.Pipes@nwnatural.com

Applicant (if not owner) Mary Fierros Bower, LRS Architects Phone/text 503.265.1572

Mailing Address 720 NW Davis Street, Suite 300, Portland, OR 97209

Email mfierrosbower@lrsarchitects.com

DESCRIBE PROPOSAL AS COMPLETELY AS POSSIBLE: _____

See below for proposal description.

1. **Participants.** When a pre-application conference is required, the applicant shall meet with the Community Development Director or his/her designee(s).
2. **Information Provided.** At such conference, the Community Development Director shall:
 - a. Cite the Comprehensive plan policies and map designations applicable to the proposal;
 - b. Cite the ordinance provisions, including the substantive and procedural requirements applicable to the proposal;
 - c. Provide available technical data and assistance, which will aid the applicant;
 - d. Identify other governmental policies and regulations that relate to the application; and
 - e. Reasonably identify other opportunities or constraints concerning the application.
3. **Disclaimer.** Failure of the Community Development Director or his/her designee to provide any of the Information required by this subsection C shall not constitute a waiver of any of the standards, criteria or

[Type text]

[Type text]



requirements for the application.

4. **Changes in the Law.** Due to possible changes in federal, state, regional, and local law, the applicant is responsible for ensuring that the application complies with all applicable laws on the day the application is deemed complete.
5. **Pre-application Checklist: Please submit the following items for review at least 10 days prior to a scheduled pre-application conference.**
 - Preliminary site plan or preliminary plat
 - Preliminary utility plan
 - Preliminary elevations (mixed use, commercial and multi-family)
 - Optional: List of questions and concerns **See attached for applicant list of questions.**

PROJECT DESCRIPTION:

NW Natural is proposing to build a new Resource Center that is not open to visitors on greenfield land. All proposed buildings will be constructed with pre-engineered metal building (PEMB) structures.

The scope includes a new 1 story 4,300 SF office building designed to seismic category IV. Including a new 6,830 SF vehicle storage & warehouse building designed to category II, with drive-thru bays and truck washing equipment (Landa) room. A 1,080 SF drive thru bay is also shown on the plans as an add alternate.

Site work scope includes, a covered truck washing pad, a covered trash enclosure, emergency generator, and staff and fleet parking spaces. Site structures include an enclosed vehicle storage garage, covered pipe storage metal shed, covered spoil bin areas and fueling tank canopy.

[Type text]

[Type text]

Pre-application
November 2019

Bob Johnston

From: Richard Stelzig
Sent: Wednesday, May 13, 2020 7:43 AM
To: 'Mary Fierros-Bower'; Janice Weese; Kevin Cronin; Bob Johnston; Fire Chief; Pipes, Wayne K.; Larry Atchison; Tom Maki; Scott Morris; Kyle Morris; Skye Woodhouse
Subject: RE: NW Natural Warrenton Resource Center Pre-App Meeting

To All,

Engineering Standards are located here: <https://www.ci.warrenton.or.us/publicworks/page/engineering-specifications-design-guide>

Answer to Applicants Engineering Questions:

1. There is no water sleeve crossing SE Dolphin Avenue
2. A stormwater report will be required as part of this project. The outfall may be riprap, but it must be designed for the calculated flow and velocity to prevent erosion.
3. Sewer laterals will be constructed as part of the wastewater improvements on SE Dolphin Avenue. The developer should look into the City's advanced financing of public improvements in our municipal code. This code section can be found here: http://qcode.us/codes/warrenton/view.php?topic=3-3_16&showAll=1&frames=on . This code section provides a way for the developer to be reimbursed as other properties connect to the new sewer system.

We also suggest that the developer coordinate with the Warrenton School District as they are in the process of designing this same sewer line. This would be a good time to discuss cost sharing for this sewer main with the school.

4. Half street improvements are necessary on your side of the SE Dolphin Avenue. The improvements will need to line up with the three existing catch basins and will include street lights.
5. A stormwater report is necessary for this development. Stormwater detention may not be necessary if downstream infrastructure can safely pass 100-year flows. ➡
6. It is not clear what this item is asking.
7. Fire hydrant locations will be determined by the fire department.

Additional Comments:

- a. Both fuel area and wash area will need to be completely covered and all water entering the sewer system will need to be treated before entering the public sewer system. In addition, the property owner will be required to apply for a discharge permit for each location. The discharge permit will include discharge limits and required testing.
- b. Additional comments will be forwarded to the design team after the pre-application meeting.

Thank you,

Collin Stelzig, PE | Public Works Director | City of Warrenton
45 SW 2nd St. | P.O. Box 250 | Warrenton, OR 97146
Office: (503) 861-0912 | Fax: (503) 861-9661
rstelzig@ci.warrenton.or.us | www.ci.warrenton.or.us

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-----Original Appointment-----

From: Mary Fierros-Bower <mfierrosbower@lrsarchitects.com>

Sent: Tuesday, May 12, 2020 4:45 PM

To: Mary Fierros-Bower; Janice Weese; Kevin Cronin; Bob Johnston; Fire Chief; Richard Stelzig; Pipes, Wayne K.; Larry Atchison; Tom Maki; Scott Morris; Kyle Morris; Skye Woodhouse

Subject: NW Natural Warrenton Resource Center Pre-App Meeting

When: Wednesday, May 13, 2020 11:00 AM-12:00 PM (UTC-08:00) Pacific Time (US & Canada).

Where: LRS Architects Zoom Meeting

Mary Bower is inviting you to a scheduled Zoom meeting.

Join Zoom Meeting

<https://lrsarchitects.zoom.us/j/97119071601?pwd=R2Z2bTZYdG5zM3g1eTNhSG4va2M5UT09>

Meeting ID: 971 1907 1601

Password: 716939

One tap mobile

+12532158782,,97119071601# US (Tacoma)

+13462487799,,97119071601# US (Houston)

Dial by your location

+1 253 215 8782 US (Tacoma)

+1 346 248 7799 US (Houston)

+1 669 900 6833 US (San Jose)

+1 301 715 8592 US (Germantown)

+1 312 626 6799 US (Chicago)

+1 929 205 6099 US (New York)

Meeting ID: 971 1907 1601

Find your local number: <https://lrsarchitects.zoom.us/j/97119071601?pwd=R2Z2bTZYdG5zM3g1eTNhSG4va2M5UT09>

CITY OF WARRENTON
PLANNING AND BUILDING DEPARTMENT
Telephone: 503-861-0920

VARIANCE APPLICATION

To be accompanied by a Site Plan, copy of property deed and Letter of Authorization, if applicable.

OFFICE USE ONLY	
FILE # <u>V-20-4</u>	FEE \$ <u>500⁰⁰</u>
ZONING DISTRICT _____	
RECEIPT # <u>8832845</u>	

The Variance application process is a method for assuring compliance with the City of Warrenton Comprehensive Plan and Development Code, and to ensure wise utilization of natural resources, and the proper integration of land uses utilizing appropriate landscaping or screening measures. Please answer the questions as completely as possible.

Legal Description of the Subject Property: Township 8, Range 10, Section(s) 34
Tax Lot(s) 2300

Property street address: SE Dolphin Avenue, Warrenton, Oregon 97146

I/we, the undersigned applicant(s) or authorized agent, affirm by my/our signature(s) that the information contained in the foregoing application and associated submissions is true and correct.

APPLICANT:

Printed Name: Mary Fierros Bower, LRS Architects mfierrosbower@lrsarchitects.com

Signature: *Mary Fierros Bower* Date: 8/12/20

Address: 720 NW Davis Street, Suite 300 Phone: 503.265.1572

City/State/Zip: Portland, OR 97209 Fax: _____

PROPERTY OWNER (if different from Applicant):

Printed Name: Wayne Pries, NW Natural

Signature: *Wayne Pries* Date: 8/12/20

Address: 250 SW Taylor Street Phone: 503.226.4211 X2496

City/State/Zip: Portland, OR 97204-3038 Fax: _____

This application will not be officially accepted until department staff has determined that the application is completed, the site plan map requirements are met, and a copy of the deed is included.

NARRATIVE: Please describe the variance request:

The applicant is requesting approval to deviate from a maximum six-foot high perimeter fence indicated in the Development Code to a seven-foot high perimeter fence.

The applicable standard is in 16.124.050.B.2: "Fences in the General Industrial (I-1) Zone may exceed six feet if necessary to protect the welfare of the general public (i.e., airport runway safety, military, coast guard, or homeland security defense facilities, etc.) but not for protection of private property (i.e., auto repair lots, equipment yards, woodworking shops, etc.)."

The request to increase the perimeter fence height from 6 feet to 7 feet will result in creating a more secure site for the subject resource center, a communications and dispatch center for gas line repair response crews, along with their fleet vehicles and materials storage needed for that purpose. In addition, construction of a seven-foot high perimeter chain link fence with PVC privacy slats fence will act as a visual screen to the surrounding properties.

Approval of the application will allow the Applicant to construct a well-designed and secure NW Natural Resource Center. The Applicant therefore respectfully requests approval of the subject fence height variance application.

Standard	Required	Proposed
Front Yard Setback	10 feet	10 feet
Rear Yard Setback	50 feet adj. to residential zone 10 feet adj. to industrial zone	50 feet adj. to residential zone 10 feet adj. to industrial zone
Side Yard Setback	50 feet adj. to residential zone 10 feet adj. to industrial zone	50 feet adj. to residential zone 10 feet adj. to industrial zone
Lot Dimension	425 feet x 546 feet (5.32 ac)	no change
Height	45 feet max.	bldg. heights varies: 22'-7" to 28'-2"
Landscaping	20% min. site area 8% min. vehicular use area	52.2% of site area 12.1% of vehicular area
Parking	14 min. staff parking no maximum	22 total staff parking spaces

SIX VARIANCE CRITERIA

1. The hardship was not created by the person requesting the variance. Please explain.

A seven-foot high property perimeter fence is one of NW Naturals' Resource Center program requirements as this height is proven to deter theft and provide overall better security.

2. The request is necessary to make reasonable use of the property. There will be an unreasonable economic impact upon the person requesting the variance if the request is denied.

The request to increase the perimeter fence height from 6 feet to 7 feet will result in creating a more secure site for the subject resource center, a communications and dispatch center for gas line repair response crews, along with their fleet vehicles and materials storage needed for that purpose.

3. The request will not substantially be injurious to the neighborhood in which the property is located. The variance will not result in physical impacts, such as visual, noise, traffic or increased potential for drainage, erosion and landslide hazards, beyond those impacts that would typically occur with development in the subject zone.

The request to increase the perimeter fence height from 6 feet to 7 feet will not result in physical impacts but will provide a better visual screen to the surrounding properties and enhanced security for NW Natural Resource Center.

4. The request is not in conflict with the Comprehensive Plan. Please explain.

See item 5.

5. The request is not in conflict with the Development Code. No variance may be granted which will result in a use not permitted in the applicable zone or which will increase the allowable residential density in any zone with the exception of individual lot size reduction. Please explain.

The applicable standard is in 16.124.050.B.2: "Fences in the General Industrial (I-1) Zone may exceed six feet if necessary to protect the welfare of the general public but not for protection of private property." However, NW Natural is an essential facility thus it is important to protect the stored parts necessary to repair gas lines via a 7 foot high perimeter fence.

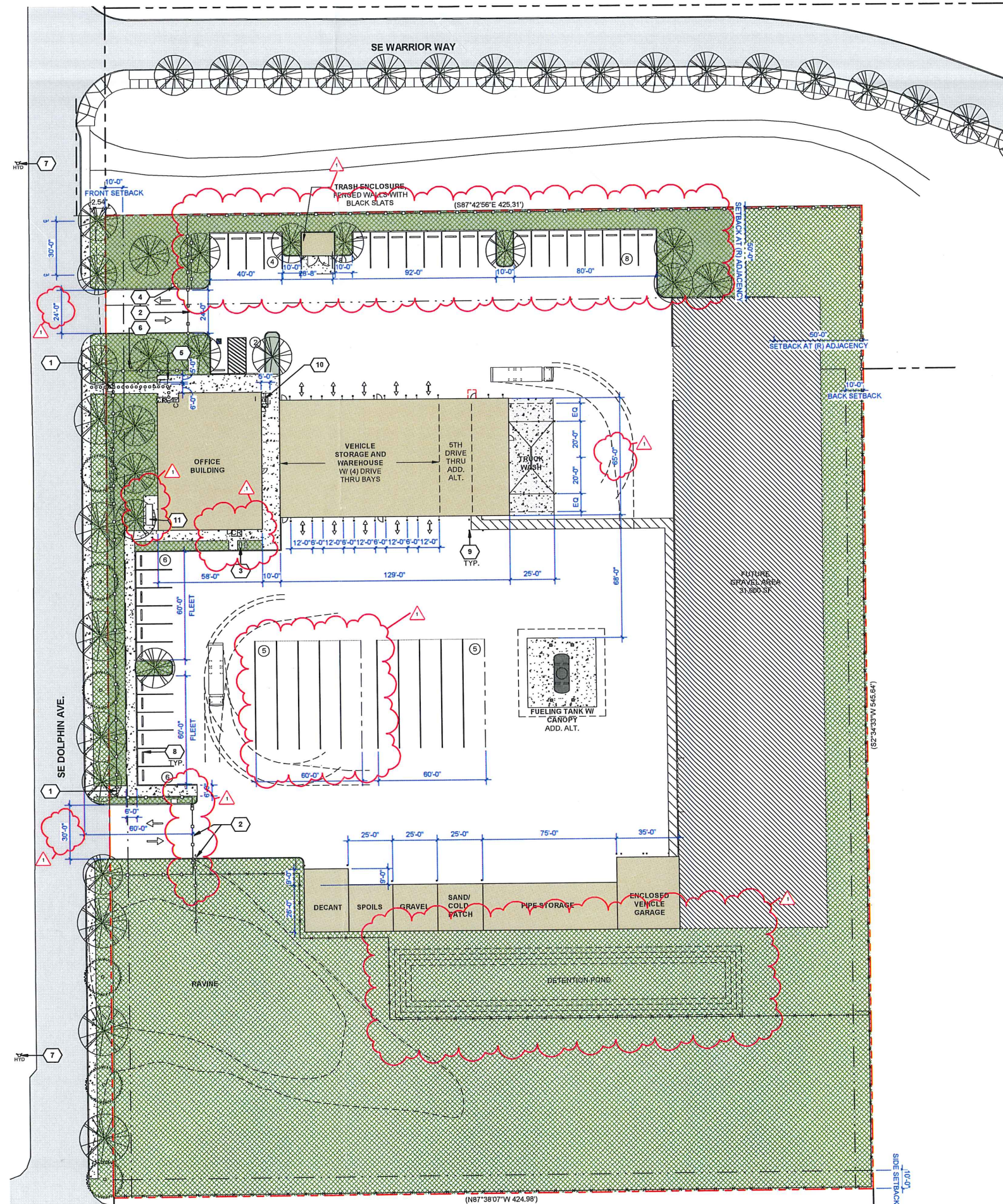
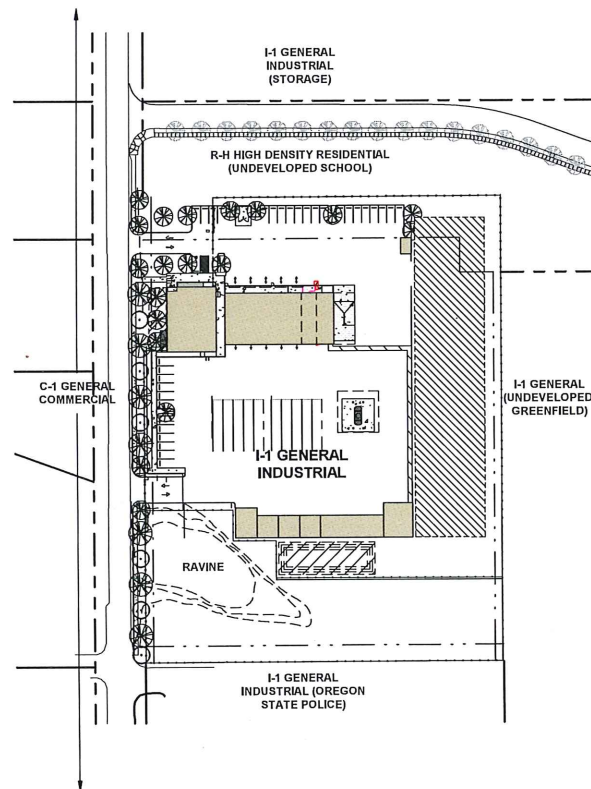
6. Physical circumstance(s) related to the property involved preclude conformance with the standard to be varied. Please explain.

The applicant does not see any physical circumstances related to the property involved preclude conformance with the standard requested to be varied.

Return Application To: City of Warrenton
Planning and Building Department
PO Box 250, 225 S. Main Street
Warrenton, Oregon 97146

SITE PLAN

A1



KEYNOTES

- 1 MAN GATE WITH CARD READER
- 2 CANTILEVERED GATE
- 3 CONDENSING UNITS, SIZE AND QUANTITY TO BE DETERMINED
- 4 PEDASTAL MOUNTED CARD READER AND KNOX BOX
- 5 SHORT TERM BICYCLE RACKS
- 6 FLAG POLE
- 7 FIRE HYDRANT
- 8 WHEEL STOPS
- 9 CONCRETE FILLED BOLLARDS WITH YELLOW PLASTIC SLEEVE
- 10 LONG TERM BICYCLE RACKS
- 11 ADD ALTERNATE GENERATOR

LEGEND

- LANDSCAPED REGIONS
- UNDEVELOPED LAND
- SIDEWALK
- GRAVEL
- PROPOSED BUILDING
- PROPERTY LINE
- SETBACK LINE
- 7'-0" FENCE WITH BLACK PRIVACY SLATS
- 7'-0" FENCE
- EXISTING CONCRETE SIDEWALK
- SITE EGRESS PATH TO RIGHT OF WAY
- ENTRY/EXIT ACCESS TO ACCESSIBLE MEANS OF EGRESS
- PUBLIC ENTRY ON ACCESSIBLE ROUTE
- SITE POLE LIGHTING
- NO. OF PARKING SPACES IN GROUP
- TRAFFIC ARROW
- FIRE HYDRANT
- EXISTING FIRE HYDRANT
- FIRE DEPARTMENT CONNECTION
- 123'-0" SPOT GRADE ELEVATION
- F.F. 123' FINISH FLOOR ELEVATION

LOT COVERAGE	TOTALS
SITE	231,847.64 SF (5.32 AC)
TOTAL IMPERMEABLE SURFACES	109,278.4 SF
PAVEMENT & DRIVES	82,351.4 SF
BUILDING TOTAL	22,434 SF
OFFICE	4,408 SF
WAREHOUSE (W/5 COVERED TRUCK BAYS)	7,740 SF
LANDA CANOPY	1,500 SF
PIPE STORAGE	1,950 SF
SPOILS AND DECANT BAYS	2,842 SF
ENCLOSED VEHICLE STORAGE GARAGE	1,398 SF
TRASH ENCLOSURE	243 SF
FUELING CANOPY	2,352 SF
TOTAL LANDSCAPE	122,569.24 SF
DESIGNED LANDSCAPE AREAS	32,794 SF
GRAVEL AREA	31,000 SF
BIOSWALE TOTAL	7,500 SF
UNDEVELOPED OPEN SPACE	70,331 SF
PARKING	
FRONT AREA	6
STAFF	16
FLEET	26
TRUCK BAYS (NOT COVERED)	5
TRUCK BAYS (COVERED)	5

REVISIONS

LUR Revision 1 August 11, 2020

Scale: As indicated

NOT FOR CONSTRUCTION

LRS ARCHITECTS
 218113 | NWN Warrenton Resource Center | 50% DESIGN DEVELOPMENT
 08/13/20



TABLE OF CONTENTS

NARRATIVE AND CODE CONFORMANCE

EXHIBIT A

SITE PLAN DESIGN REVIEW APPLICATION

GENERAL PERMIT CHECKLIST

EXHIBIT B

DRAWING SET

(ARCHITECTURAL, CIVIL AND LANDSCAPE)

EXHIBIT C

PHOTOMETRIC ANALYSIS

EXHIBIT D

IMPACT STUDY

EXHIBIT E

PRELIMINARY STORMWATER REPORT

EXHIBIT F

GEOTECHNICAL REPORT

EXHIBIT G

TRIP GENERATION REPORT

EXHIBIT H

PRE-APPLICATION MEETING NOTES

NW NATURAL WARRENTON RESOURCE CENTER

**SITE PLAN DESIGN REVIEW APPLICATION
TYPE III**

EXHIBIT A

SITE PLAN DESIGN REVIEW APPLICATION
GENERAL PERMIT CHECKLIST



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503.221.1121 • WWW.LRSARCHITECTS.COM



Community & Economic Development Department

Permit Checklist

June 2019

The following is a general checklist that applies to all Community & Economic Development Department submittals. The checklist is intended to assist prospective applicants with navigating the permit process. In each case, there may be additional submittal requirements that are identified in the pre-application notes.

The burden of proof rests with the applicant to demonstrate compliance with applicable portions of the Development Code.

- Signed Application & Fee (Site Design Review, Conditional Use, Variance, etc)
- Site Plan (3 copies | 11 x 17 or larger to scale)
- Landscape & Parking Plan (if not indicated on site plan)
- Engineering Review Form & Deposit
- Self-addressed, stamped envelopes for public notice (Type 2 & Type 3)
- Impact Study (Type 2 & Type 3)
- Pre-application Notes Response Letter
- Narrative of findings that addresses applicable criteria
- Preliminary utility and stormwater plan

After all materials have been verified, a completeness letter will be issued with a basic timeline for review. After completion, public notice will be published and mailed to the affected property owners and agencies.

The checklist itself is a requirement and must be submitted and date stamped by city staff.

NW NATURAL WARRENTON RESOURCE CENTER

SITE PLAN DESIGN REVIEW APPLICATION TYPE III

PROJECT INFORMATION:

SE Dolphin Avenue
WARRENTON, OR 97146
MAP AND TAX LOT 2300
LEGAL DESCRIPTION: TOWNSHIP 8 RANGE 10, SECTION 34
ZONING: GENERAL INDUSTRIAL (I-1)

PROPERTY OWNER / NW NATURAL GAS CO:

WAYNE PIPES
250 SW Taylor Street
PORTLAND, OR 97204-3038
503.226.4211 X2496

APPLICANT REPRESENTATIVE / LRS ARCHITECTS:

MARY FIERROS BOWER
720 NW DAVIS STREET, SUITE 300
PORTLAND, OR 97209



CONTACT INFORMATION

Property Owner:

NW Natural Gas Co

Contact: Wayne Pipes
Wayne.Pipes@nwnatural.com
250 SW Taylor Street
Portland, OR 97204-3038
503.226.4211 X2496

Applicant Representative /

Architect:

LRS Architects

Paul Boundy
pboundy@lrsarchitects.com
503.265.1561

Contact: Mary Fierros Bower
mfierrosbower@lrsarchitects.com
720 NW Davis Street, Suite 300
Portland, OR 97209
503.256.1572

Civil Engineer /

Surveyor:

A & O Engineering

Contact: Scott Morris
scottmorris@ao-engr.com

Contact: Kyle Morris
kylemorris@ao-engr.com
380 Q Street, Suite 200
Springfield, OR
541.302.9790

Landscape Architect:

Greenworks

Contact: Gill Williams, PLA, ASLA
gillw@greenworkspc.com
24 NW 2nd Avenue, Suite 100
Portland, Oregon 97209
503.222.5612

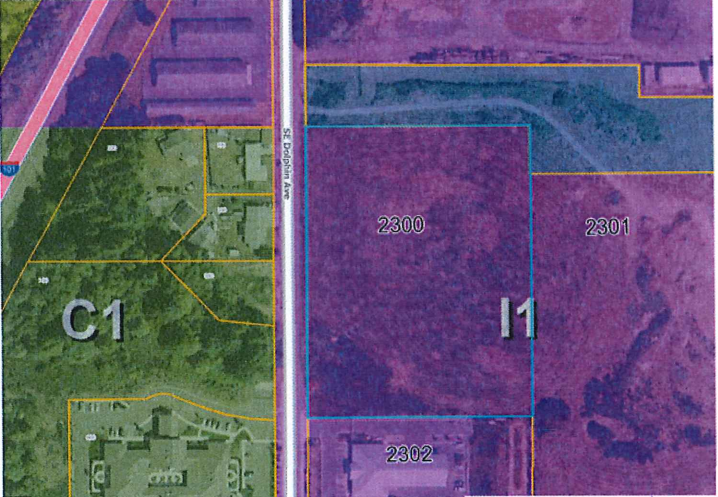


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NWN WARRENTON RESOURCE

Narrative and Zoning Code Conformance

The subject site is located on SE Dolphin Avenue, City of Warrenton. The site is approximately 231,739 SF in size and carries the I-1 General Industrial zoning district.

Site Information	NW NATURAL WARRENTON RESOURCE CENTER
Location	SE Dolphin Avenue, Warrenton, OR 97367
County	Clatsop
Tax Lot	2300
Site Size	231,739 SF (5.32 acres)
Land Use	
Jurisdiction	City of Warrenton
Base Zone	General Industrial (I-1)
Existing Use	NA (Greenfield)
Tax Map	NW ¼ Section 34, Township 8 North, Range 10 West, V.M.
Pre-Application Conference	05/13/20
Case File	
Type of Review	Site Plan Design Type III Review Application
City's Local Wetland Inventory Map	No identified wetlands on the property
Overlay Districts	No overlay districts for flood hazards or soils hazards
Surrounding land uses and zones	
North:	RH High Density Residential (SE Warrior Way access road) / I-1 General Industrial (Storage)
South:	I-1 General Industrial (Oregon State Police)
East:	RH High Density Residential (Access Road) / I-1 General Industrial (Undeveloped)
West:	C-1 General Commercial (Across Street)

Proposal Summary

PROJECT DESCRIPTION

NW Natural is proposing to build a new Resource Center on greenfield land in Warrenton on SE Dolphin Avenue that is not open to customers or the public. The subject resource center is a communications and dispatch center for gas line repair response crews, along with the fleet vehicle and materials storage needed for that purpose. The existing NW Natural facility that serves the same purpose is currently in Astoria. Relocating it will increase resiliency by moving it outside of the tsunami zone, and because it will be built to a stronger seismic standard.

All proposed buildings will be constructed with pre-engineered metal building (PEMB) structures.

The scope includes a new 1 story 4,408 SF office building designed to seismic category IV "Immediate occupancy". Including a new 7,740 SF vehicle storage & warehouse building designed to seismic category II "immediate occupancy", with drive-thru bays and truck washing equipment (Landa) room. A 5th drive thru bay is shown on the plans as an add alternate and is included in the square footage above. Site work scope includes, a covered truck washing pad, a covered trash enclosure, emergency generator, and staff and fleet parking spaces. Site structures include an enclosed vehicle storage garage for temperature sensitive vehicles, covered pipe storage metal shed, covered spoil & decant bin areas and fueling tank canopy.

DEVELOPMENT REVIEW

The applicant is requesting approval to build a new NW Natural Resource Center building and associated site infrastructure with the provisions of the City of Warrenton Municipal Code.

Title 16 Development Code, Chapter 16.60.020 lists the permitted uses and development standards for the General Industrial (I-1) Zoning District. This project is a permitted use and complies with these development standards.

The development will be subject to standards outlined in Title 16 of the Warrenton Municipal Code as well as any other applicable city, state, or federal regulations including the City of Warrenton Transportation System Plan (TSP) and City of Warrenton Public Works Design Standards. The development review application is subject to the Land Use Review – Type III Site Plan Design Review procedure.

The Land Use Review fee is (\$750) and is submitted with this application. Fees are based on Planning and Community Development Fee Schedule.

Applicable Design Standards

- Chapter 16.60 General Industrial (I-1) District
- Chapter 16.120 Design Standards: Access & Circulation
- Chapter 16.124 Design Standards: Landscaping, Street Trees, Fences, and Walls
- Chapter 16.128 Design Standards: Vehicle & Bicycle Parking
- Chapter 16.132 Design Standards: Clear Vision Areas
- Chapter 16.136 Design Standards: Public Facilities Standards
- Chapter 16.140 Design standards: Stormwater & Surface Water Management
- Chapter 16.192 Design Standards: Large Scale Developments
- Chapter 16.208 Applications and Review Procedures: Types of Applications and Review Procedures
- Chapter 16.212 Applications and Review Procedures: Site Design Review
- Chapter 16.256 Applications and Review Procedures: Traffic Impact Study

Warrenton Municipal Code, Title 16 Development Code

DIVISION 2 LAND USE DISTRICTS

CHAPTER 16.60 GENERAL INDUSTRIAL (I-1) DISTRICT

16.60.010 Purpose

The purpose of the General Industrial Zone is to provide sites for light, heavy, and airport-related industrial activities in the City of Warrenton. These areas are suitable for uses involving manufacturing, fabrication, processing, transshipment and bulk storage. General Industrial areas are near or adjacent to arterial transportation corridors.

16.60.020 Permitted Uses

The facility as proposed does not fit exactly into any of the Warrenton Development Code's use categories. Interpretation is required. In the pre-application notes, City staff has said the proposed use is "utility facility" and that it is therefore consistent with the zone.

In fact, the use allowed in the zone is "public utility facility such as power stations, sewage and water treatment plants" (WDC 16.60.020). Alternatively, the use could be defined as "storage and distribution services and facilities (i.e., truck terminals, warehouses and storage buildings and yards) or similar uses," which is also an allowed outright use in the I-1 zone (WDC 16.60.020).

RESPONSE

Despite the fact that NW Natural is a private utility and not one of the three listed examples, the description in the definitions section is expansive and therefore the closest fit for the use. The definition for "public utility facility" states, "a private business or organization such as a public service corporation, performing some public service and subject to governmental regulation; or a governmental agency performing similar public services. Such services shall include, but are not limited to, electricity, gas and telephone." (WDC 16.12.010). The proposed development conforms to the standards of a Public Utility Facilities use. Therefore, the subject project is a permitted use and complies with these development standards.

16.60.040 Development standards

The following standards are applicable in the I-1 zone:

- A. Air Quality:** The air quality standards set by the Department of Environmental Quality shall be the guiding standards in this zone, except that open burning is prohibited in any case.
- B. Noise:** As may be permitted under all applicable laws and regulations.
- C. Storage:** All materials, including wastes, shall be stored and maintained in a manner that will not attract or aid the propagation of insects or rodents or other animals or birds, or otherwise create a health hazard or nuisance.

RESPONSE

The proposed new Resource Center is not anticipated to create dust, odor, fumes, smoke and noise. Equipment and vehicle servicing will occur off site at NW Natural's Sherwood location (Eco-Biz certified location). The business activity or processes conducted at this facility are natural gas transmission / distribution maintenance support center (non-production) related. The proposed facility is "service" based and will not be "producing" or "storing" any products / chemicals / hazardous materials. The Pipe Storage shed stores polyethylene pipes for protection from UV light / weather. Pipes are stored in bulk on cantilevered racks that are seismically bolted to the concrete slab. All materials stored in the yard are proposed to be covered, stored and maintained in a manner that will not attract or aid the propagation of insects or rodents or other animals or birds (16.060.040.A / B / C). Therefore, this criterion has been met.

D. Fencing: Will be allowed inside a boundary planting screen and where it is necessary to protect property of the use concerned or to protect the public from a dangerous condition. Proposed fence locations and design shall be subject to City review.

RESPONSE

A six-foot high perimeter chain link fence is proposed for the subject property for security. The portion of the perimeter chain link fence that is located along the west property line at SE Dolphin Avenue, along the north property line at SE Warrior Way and along the east property line abutting the residential zone is provided with black privacy slats (WDC 16.60.040.D).

Therefore, this criterion has been met.

E. Buffer: Where this zone adjoins another non-industrial zone there shall be a buffer area at least 10 feet wide to provide a dense evergreen landscape buffer which attains a mature height of eight feet, or such other screening measures as may be prescribed by the City in the event differences in elevation or other circumstances should defeat the purpose of this requirement.

RESPONSE

A 10-foot wide dense evergreen landscape buffer is proposed where the zone adjoins another non-industrial zone (WDC 16.060.040.E). See Exhibit B, Landscape Plans.

Therefore, this criterion has been met.

F. Vibration: No vibration other than that caused by highway vehicles, trains and aircraft shall be permitted which is discernible without instruments at the property line of the use concerned.

G. Airport Interference: No use shall create electrical or lighting interference with the operations of the Port of Astoria Airport.

RESPONSE

The proposed new Resource Center operations is not anticipated to create vibration and electrical or lighting interference with operations of the Port of Astoria Airport (16.060.040.F / G).

Therefore, this criterion has been met.

H. Setbacks: The minimum front, side and rear setbacks shall be 10 feet. When across a street from a non-industrial zone, the setback from the property line shall be 10 feet. When a property abuts a non-industrial zone, the setback shall be as follows:

1. 50 feet for buildings and other structures more than 10 feet in height;
2. 30 feet for buildings and structures more than 6 feet high but not more than 10 feet high; and
3. 10 feet for structures no more than 6 feet high (except fences no more than 6 feet high may be on the property line)

RESPONSE

Provided building setbacks are as follows.

- Frontage Setback (west property line, across from a non-industrial zone): **10 feet**
- Side Setback (north property line, adjacent to a residential zone): **50 feet**
- Rear Setback (east property line, adjacent to a residential zone): **50 feet**
- Rear Setback (east property line, adjacent to an industrial zone): **10 feet**
- Side Setback (south property line, adjacent to an industrial zone): **10 feet**

Required building setbacks have been indicated on the site plan (16.060.040.H). See Exhibit B, Architectural Site Plan.

Therefore, this criterion has been met.

J. Building Height. The maximum building height shall be 45 feet, except that it may be lower under either of the following circumstances:

1. Within 100 feet of a non-residential zone, where the maximum building height shall be the same as the maximum building height in that zone. (RH Residential Zone, max. bldg. height 40 feet)

RESPONSE

Provided building heights are as follows.

Office Building Height: **24'-0"**

Warehouse Height: **28'-2"**

Enclosed Vehicle Storage Garage: **23'-11"**

Site Buildings: **27'-0"** (Height Varies 22'-7" to 27'-0")

Compliant building heights are indicated on the exterior elevations (WDC 16.60.040.J). See Exhibit B, Exterior Elevations.

Therefore, this criterion has been met.

DIVISION 3 DESIGN STANDARDS

CHAPTER 16.116 DESIGN STANDARDS

As acknowledged in the pre-application notes, aesthetic design standards for commercial development do not appear to apply to this property. Those standards begin "the City's development design standards are for the commercial district along Highway 101, SW Dolphin, and SE Marlin Avenues." (WDC 16.116.030) This property is not on any of those streets, and therefore the design standards do not apply.

CHAPTER 16.120 DESIGN STANDARDS: ACCESS & CIRCULATION

16.120.010 Purpose

The purpose of this chapter is to ensure that developments provide safe and efficient access and circulation, for pedestrians and vehicles. Section 16.120.020 provides standards for vehicular access and circulation. Section 16.120.030 provides standards for pedestrian access and circulation. Planning and design standards for improvements to public and private transportation facilities and utilities are provided in Chapter 16.136.

16.120.020 Vehicular Access and Circulation

F. Access Options. When vehicle access is required for development (i.e., for off-street parking, delivery, service, drive-through facilities, etc.), access shall be provided by one of the following methods (a minimum of 10 feet per lane is required). These methods are "options" to the developer/subdivider, unless one method is specifically required under Division 2, or through conditions required by the hearings body.

- 3. Option 3.** Access is from a public street adjacent to the development parcel. If practicable, the owner/developer may be required to close or consolidate an existing access point as a condition of approving a new access. Street accesses shall comply with the access spacing standards in subsection G of this section, and require an access permit in accordance with subsection C of this section.

RESPONSE

There are two access points located appropriately from adjacent driveways and future streets that will allow for safe ingress/egress for staff vehicles, fleet and delivery trucks. The northern driveway is 30 feet wide and the southern driveway is 24 feet wide. Access is from a public street adjacent to the development parcel (WDC 16.120.F.3). Therefore, this criterion has been met.

G. Access Spacing. Driveway accesses shall be separated from other driveways and street intersections in accordance with the following standards and procedures:

1. **Local Streets.** A minimum of 25 feet separation (as measured from the sides of the driveway/street) shall be required on local streets (i.e., streets not designated as collectors or arterials) for all single-family detached dwellings, except as provided in paragraph 3 of this subsection. A minimum of 20 feet separation shall be required on local streets for all single-family attached dwellings, duplexes, and triplexes, except as provided in paragraph 3 of this subsection.

RESPONSE

The northern proposed driveway is separated from the SE Warrior Way street intersection by approximately 80 feet. The northern driveway is separated from the southern proposed driveway by at least 261 feet. The southern driveway is separated from the Oregon State Police driveway by at least 193 feet. Access spacing between the two driveways and to the next nearest opening (SE Warrior Way to the north) is at least 25 feet per the regulation (WDC 16.120.020.G.1).

Therefore, this criterion has been met.

H. Number of Access Points. The number of street access points for multiple family, commercial, industrial, and public/institutional developments shall be minimized to protect the function, safety and operation of the street(s) and sidewalk(s) for all users.

RESPONSE

There are two access points proposed for this development and they are located appropriately from adjacent driveways and future streets that will allow for safe ingress/egress for staff vehicles, fleet and delivery trucks. Therefore, this criterion has been met.

J. Street Connectivity and Formation of Blocks Required. In order to promote efficient vehicular and pedestrian circulation throughout the City, land divisions and large site developments shall produce complete blocks bounded by a connecting network of public and/or private streets, in accordance with the following standards:

2. **Street Standards.** Public and private streets shall conform to the standards of Chapter 16.136, Public Facilities Standards; Section 16.120.030, Pedestrian Circulation; applicable Americans With Disabilities Act (ADA) design standards; City construction standards for streets; and other applicable Development Code sections.

RESPONSE

See Section 16.136, Public Facilities Standards for street standards compliance. Therefore, this criterion has been met.

K. Driveway Openings and Widths. Driveway openings (or curb cuts) shall be the minimum width necessary to provide the required number of vehicle travel lanes (10 feet for each travel lane). The following standards (i.e., as measured where the front property line meets the sidewalk or right-of-way) are required to provide adequate site access, minimize surface water runoff, and avoid conflicts between vehicles and pedestrians:

4. Access widths for all other uses shall be based on 10 feet of width for every travel lane, except that driveways providing direct access to parking spaces shall conform to the parking area standards in Chapter 16.128.

5. **Setback Required.** A minimum five-foot setback from the edge of driveway to any property line is required. The setback area shall be kept free of impervious surfaces at all times and shall be vegetated to minimize surface water runoff to adjoining properties. These requirements may be increased if the Community Development Director, building official, City-appointed engineer, or Planning Commission

determines that topography, soil conditions, or other circumstances dictate the need for additional protection measures.

6. Driveway Aprons. Driveway aprons shall meet City construction standards and be installed between the street right-of-way and the private drive, as shown in Figure 16.120.020.K. Driveway aprons shall conform to ADA standards for sidewalks and pathways, which require a continuous route of travel that is a minimum of three feet in width, with a cross slope not exceeding two percent.

7. Driveway Approaches. Driveway approaches should be designed and located to provide an existing vehicle with an unobstructed view. Construction of driveways along acceleration or deceleration lanes or tapers should be avoided due to potential for vehicle conflicts.

8. Loading Area Design. The design of driveways and on-site maneuvering and loading areas for commercial and industrial developments shall consider the anticipated storage length for entering and exiting vehicles, in order to prevent vehicles from backing into the flow of traffic on the public street or causing unsafe conflicts with on-site circulation.

RESPONSE

There are two proposed driveway openings that will allow for safe ingress/egress of staff vehicles, fleet and delivery trucks. The northern driveway is 30 feet wide and the southern driveway is 24 feet wide sized to accommodate large delivery truck turning radii. The site has been designed to provide efficient and safe access and circulation to meet City of Warrenton code requirements. A 12-foot setback from the edge of driveway to the property line is provided. Proposed driveways conform to City construction and ADA standards. A designated truck loading or unloading space is not provided because the proposed use is a private utility operation consisting of a service yard with ample circulation space for large fleet vehicles to maneuver and park (16.120.020.K). Therefore, this criterion has been met.

L. Fire Access and Circulation. The City of Warrenton adopts the Uniform Fire Code, as amended, including administrative sections and all appendices and all the State of Oregon revisions. All development in the City of Warrenton is required to meet these minimum adopted standards.

1. Required Access. A fire equipment access drive that meets City construction standards shall be provided for any portion of an exterior wall of the first story of a building that is located more than 150 feet from an improved public street or approved fire equipment access drive. Plans for fire apparatus access roads shall be submitted to the Warrenton Fire Department and Warrenton City-appointed engineer for review and approval prior to issuance of building permits, grading permits, or start of construction. When fire apparatus access road(s) are required, the road(s) shall be installed and made serviceable prior to and during time of construction. Fire department access roads shall be provided and maintained in accordance with the fire department access requirements of the Uniform Fire Code, as amended.

2. Dimensions. Fire apparatus roads shall have an unobstructed width of not less than 20 feet and unobstructed vertical clearance of not less than 13 feet 6 inches. Fire apparatus roads shall be designed and maintained to support the imposed loads of fire apparatus and shall be provided with a surface so as to provide all-weather driving capabilities.

3. Turnaround Required. Dead-end fire apparatus roads in excess of 150 feet shall be provided with approved provisions for the turning around of fire apparatus. See Table 16.136.010 for minimum standards.

4. Grade. The gradient for a fire apparatus access road shall not exceed 12% except that isolated segments no longer than 250 feet may have grades up to 15% upon approval by the Warrenton Fire Chief. Non-fire apparatus access roads (driveways and private streets) shall maintain a maximum grade of 15% unless otherwise approved by the Warrenton City-appointed engineer. See Table 16.136.010 for other applicable standards.

5. **Parking Areas.** Parking areas shall provide adequate aisles or turn-around areas for service and delivery vehicles so that all vehicles may enter the street in a forward manner. See also Chapter 16.136, Public Facilities Standards.

RESPONSE

There are two proposed driveway openings that will allow for safe ingress/egress of staff vehicles, fleet and delivery trucks. The northern driveway is 30 feet wide and the southern driveway is 24 feet wide sized to accommodate large delivery truck turning radii. The internal circulation pattern has been designed to accommodate the largest size truck utilized by NW Natural, the pipe trailer delivery truck is 68 feet in length, which is adequate for emergency vehicles and trash collection. Two access points to the public right of way eliminates the need for an internal emergency turnaround (16.120.020.L).

Therefore, this criterion has been met.

M. Vertical Clearances. Driveways, private streets, aisles, turn-around areas and ramps shall have a minimum vertical clearance of 13 feet 6 inches for their entire length and width.

N. Vision Clearance. No signs, structures or vegetation in excess of three feet in height shall be placed in vision clearance areas, as shown in Figure 16.120.020.N. The minimum vision clearance area may be increased by the Community Development Director, City-appointed engineer, or Planning Commission upon finding that more sight distance is required (i.e., due to traffic speeds, roadway alignment, etc.). See also Chapter 16.132 for additional requirements.

O. Construction. The following development and maintenance standards shall apply to all driveways, parking areas, and private streets in the City of Warrenton:

- 1. Surface Options.** All driveways, parking areas, aisles, and turn-a-rounds in the City of Warrenton shall be paved with asphalt, concrete, or other comparable surfacing. A durable non-paving material may be used for driveways and private streets that serve three or fewer residential dwelling units and in other instances where the need to reduce surface water runoff and protect water quality can be demonstrated through adequate findings of fact submitted by the applicant and/or property owner as part of the development proposal. All paving and non-paving surfaces shall meet City construction standards and shall be subject to review and approval by the Community Development Director, City-appointed engineer, and/or Planning Commission.
- 2. Surface Water Management.** All driveways, parking areas, aisles and turn-a-rounds shall have on-site collection or infiltration of surface waters to eliminate sheet flow of such waters onto public rights-of-way and abutting property. Surface water facility plans shall be prepared by a qualified person and constructed in conformance with City standards. Such plans shall attempt to follow the principle that water falling on a given site should be absorbed or retained on-site to the extent that the quantity and rate of water leaving the site after the development would not be significantly different than if the site had remained undeveloped.
- 3. Driveway Aprons.** When driveway approaches or "aprons" are required to connect driveways to the public right-of-way, they shall be paved with concrete surfacing and meet City construction standards. (Ord. 1225 § 5, 2019)

RESPONSE

All drivable surfaces interior to the site are proposed to be paved. There is a potential future gravel area on the east side as shown on the site plan. The use of this area is currently undefined. Required vertical clearances will be provided within the site circulation areas. The storm management plan has been designed to process all impervious areas within the site to meet City of Warrenton code requirements. A storm drainage report with calculations is included with this Site Plan Design Review submittal (16.120.020.M/N/O). See Exhibit B, Drawing Set and Exhibit E, Preliminary Stormwater Report.

Therefore, this criterion has been met.

16.120.030 Pedestrian Access and Circulation

A. Pedestrian Access and Circulation. To ensure safe, direct and convenient pedestrian circulation, all developments, except single-family detached housing, duplexes, or triplexes on individual lots, shall provide a continuous pedestrian and/or multi-use pathway system. (Pathways only provide for pedestrian circulation. Multi-use pathways accommodate pedestrians and bicycles.) The system of pathways shall be designed based on the standards in paragraphs 1 through 3 of these subsections:

1. **Continuous Pathways.** The pathway system shall extend throughout the development site, and connect to all future phases of development, adjacent trails, public parks and open space areas whenever possible. The developer may also be required to connect or stub pathway(s) to adjacent streets and private property, in accordance with the provisions of Section 16.120.020, Vehicular Access and Circulation, and Chapter 16.136, Public Facilities Standards.
2. **Safe, Direct, and Convenient Pathways.** Pathways within developments shall provide safe, reasonably direct and convenient connections between primary building entrances and all adjacent streets and existing or planned transit stops, based on the following definitions:
 - a. **Reasonably Direct.** A route that does not deviate unnecessarily from a straight line or a route that does not involve a significant amount of out-of-direction travel for likely users.
 - b. **Safe and Convenient.** Bicycle and pedestrian routes that are reasonably free from hazards and provide a reasonably direct route of travel between destinations.
 - c. For commercial, industrial, mixed use, public, and institutional buildings, the "primary entrance" is the main public entrance to the building. In the case where no public entrance exists, street connections shall be provided to the main employee entrance.
3. **Connections Within Development.** For all developments subject to site design review, pathways shall connect all building entrances to one another. In addition, pathways shall connect all parking areas, storage areas, recreational facilities and common areas (as applicable), and adjacent developments to the site, as applicable.
4. **Street Connectivity.** Pathways (for pedestrians and bicycles) shall be provided at or near mid-block where the block length exceeds the length required by Section 16.120.020. Pathways shall also be provided where cul-de-sacs or dead-end streets are planned, to connect the ends of the streets together, to other streets, and/or to other developments, as applicable. Pathways used to comply with these standards shall conform to all of the following criteria:
 - a. Multi-use pathways (i.e., for pedestrians and bicyclists) are no less than six feet wide.
 - c. Stairs or switchback paths using a narrower right-of-way/easement may be required in lieu of a multi-use pathway where grades are steep.
 - d. The City may require landscaping within the pathway easement/right-of-way for screening and the privacy of adjoining properties.
 - e. The Planning Commission or Community Development Director may determine, based upon facts in the record, that a pathway is impracticable due to: physical or topographic conditions (e.g., freeways, railroads, extremely steep slopes, sensitive lands, and similar physical constraints); buildings or other existing development on adjacent properties that physically prevent a connection now or in the future, considering the potential for redevelopment; and sites where the provisions of recorded leases, easements, covenants, restrictions, or other agreements recorded as of the effective date of the ordinance codified in this chapter prohibit the pathway connection.

RESPONSE

The proposed site plan includes a safe design of pedestrian activity patterns within the service yard. Staff vehicular and bicycle parking is located directly adjacent to the building's main entrance behind secured perimeter fencing. ADA parking is located in front of the secured perimeter fencing. The fleet vehicle parking is located directly adjacent to the rear building entrance and service yard structures. This proposal configures on-site pedestrian circulation with safe and direct pathways directly to the building entrances.

Note this site is not a public service center, thus there is no customer / public parking provided. Furthermore, pedestrian paths are not included in the fleet parking area and service yard of the proposed site plan as these areas are not open and accessible to customers or the public. However pedestrian paths are provided for safe egress from the building to the public right-of-way.

See complete land use application and application attachments demonstrating how the proposed development complies with Section (16.120.030.A). See Exhibit B, Drawing Set.
Therefore, this criterion has been met.

B. Design and Construction. Pathways shall conform to all of the standards in paragraphs 1 through 5 of these subsections:

- 1. Vehicle/Pathway Separation.** Where pathways are parallel and adjacent to a driveway or street (public or private), they shall be raised six inches and curbed, or separated from the driveway/street by a five-foot minimum strip with bollards, a landscape berm, or other physical barrier. If a raised path is used, the ends of the raised portions must be equipped with curb ramps.
- 3. Crosswalks.** Where pathways cross a parking area, driveway, or street ("crosswalk"), they shall be clearly marked with contrasting paving materials, humps/raised crossings, or painted striping. An example of contrasting paving material is the use of a concrete crosswalk through an asphalt driveway. If painted striping is used, it shall consist of thermo-plastic striping or similar type of durable application.
- 4. Pathway Surface.** Pathway surfaces shall be concrete, asphalt, brick/masonry pavers, or other durable surface, at least six feet wide, and shall conform to ADA requirements. Multi-use paths (i.e., for bicycles and pedestrians) shall be the same materials, at least six feet wide. (See also Chapter 16.136, Public Facilities Standards, for public multi-use pathway standards.)
- 5. Accessible Routes.** Pathways shall comply with the Americans with Disabilities Act, which requires accessible routes of travel. (Ord. 1225 § 5, 2019)

RESPONSE

Note this site is not a public service center, thus there is no customer / public parking provided. Furthermore, pedestrian paths are not included in the fleet parking area and service yard of the proposed site plan as these areas are not open and accessible to customers or the public. Proposed pathways are 6-foot wide, ADA compliant and designed with concrete surfaces. ADA compliant 6-foot wide egress routes from the building to the public-of-way are provided. Lastly crosswalks are clearly marked with painted striping consisting of thermo-plastic striping.

See complete land use application and application attachments demonstrating how the proposed development complies with Section (16.120.030.B). See Exhibit B, Drawing Set.
Therefore, this criterion has been met.

CHAPTER 16.124 DESIGN STANDARDS: LANDSCAPING, STREET TREES, FENCES, AND WALLS

16.124.010 Purpose

The purpose of this chapter is to promote community health, safety and welfare by protecting significant natural vegetation, and setting development standards for landscaping, street trees, fences and walls. Together, these elements of the natural and built environment contribute to the visual quality, environmental health and character of the community. Trees provide climate control through shading during summer months and wind screening during winter. Trees and other plants can also buffer pedestrians from traffic. Walls, fences, trees and other landscape materials also provide vital screening and buffering between land uses. Landscaped areas help to control surface water drainage and can improve water quality, as compared to paved or built surfaces.

16.124.020 Landscape Conservation

Prevents the indiscriminate removal of significant trees and other vegetation, including vegetation associated with streams, wetlands and other protected natural resource areas.

RESPONSE

The site does not contain significant trees and other vegetation, including vegetation associated with streams, wetlands and other protected natural resource areas (16.124.020).

Therefore, this criterion is not applicable.

16.124.030 New Landscaping

Sets standards for and requires landscape plans for all new development in the City requiring a City permit. This section also requires buffering for parking and maneuvering areas, and between different land use districts. Note that other landscaping standards are provided in Division 2, Land Use Districts, for specific types of development.

RESPONSE

Landscape plans have been prepared and submitted for review with this application (16.124.030). See Exhibit B, Landscape Plans.

Therefore, this criterion has been met.

16.124.040 Street Trees

Sets standards for and requires planting of trees along all streets for shading, comfort and aesthetic purposes.

RESPONSE

See a detailed Landscape Plan and Architectural Site Plan for the entire site showing compliance with all applicable portions of this code. Street trees required by code will also be placed behind the sidewalk in the Dolphin Avenue half street improvement. Proposed street tree locations are shown on the paving and landscape plans (16.124.040). Specific design will be completed during the PIP permit process. See Exhibit B, Drawing Set.

Therefore, this criterion has been met.

16.124.050 Fences and Walls

Sets standards for new fences and walls, including maximum allowable height and materials, to promote security, personal safety, privacy, and aesthetics. The following standards shall apply to all fences and walls:

A. General Requirements. All fences and walls shall comply with the standards of this section. The City may require installation of walls and/or fences as a condition of development approval, in accordance with Chapter 16.220, Conditional Use Permits, or Chapter 16.212, Development Review and Site Design Review. Walls built for required landscape buffers shall comply with Section 16.124.030.

B. Dimensions.

1. The maximum allowable height for fences and walls in the City of Warrenton is six feet, as measured from the lowest grade at the base of the wall or fence, except that retaining walls and terraced walls may exceed six feet when permitted as part of a site development approval, or as necessary to construct streets and sidewalks. Refer to paragraph 4 of this subsection for additional fence standards for residential uses.
2. Fences in the General Industrial (I-1) Zone may exceed six feet if necessary to protect the welfare of the general public (i.e., airport runway safety, military, coast guard, or homeland security defense facilities, etc.) but not for protection of private property (i.e., auto repair lots, equipment yards, woodworking shops, etc.). Barbed and razor wire fencing is prohibited in all zones except as necessary to enclose livestock or to protect the welfare of the general public (not private property).
3. A building permit is required for walls exceeding four feet in height and fences exceeding six feet in height, in conformance with the Uniform Building Code.
5. Walls and fences to be built for required buffers shall comply with Section 16.124.030.
6. Fences and walls shall comply with the vision clearance standards of Section 16.120.020.

C. Maintenance. For safety and for compliance with the purpose of this chapter, walls and fences required as a condition of development approval shall be maintained in good condition, or otherwise replaced by the owner.

RESPONSE

A six-foot high perimeter chain link fence is proposed for the subject property for security. The portion of the perimeter chain link fence that is located along the west property line at SE Dolphin Avenue, along the north property line at SE Warrior Way and along the east property line abutting the residential zone is provided with black privacy slats (16.124.050). See Exhibit B, Drawing Set.

Therefore, this criterion has been met.

16.124.060 Landscape Conservation

RESPONSE

The site does not contain significant trees and other vegetation, including vegetation associated with streams, wetlands and other protected natural resource areas (16.124.060).

Therefore, this criterion is no applicable.

16.124.070 New Landscaping

A. Applicability. This section shall apply to all developments within the City of Warrenton.

B. Landscaping Plan Required. For every new development in the City of Warrenton requiring a City permit, a landscape plan is required. All landscape plans shall include the following minimum required details (see Section 16.212.040 for additional landscape plan requirements for projects requiring site design review):

1. Legal description (e.g., assessor parcel number, copy of warranty deed, etc.) for the subject property;
2. Property lines with the location and general description (height and type of material) of existing and proposed fences and other buffering or screening materials;
3. The location of existing and proposed terraces or retaining walls;
4. The location of existing and proposed plant materials;
5. Wetland and/or riparian area boundaries on the property, if any;
6. Existing and proposed structures;
7. Driveway and adjoining roadway widths, descriptions, and locations; and
8. Prevailing drainage patterns for the property.

9. Other information as deemed appropriate by the Community Development Director. An arborist's report may be required for sites with mature trees that are protected under this chapter and/or Chapter 16.156 of this Code.

C. Landscape Area Standards. The minimum percentage of required landscaping equals:

3. General industrial districts: a minimum of 20% of the site shall be landscaped.

The use of mature, native vegetation within developments is a preferred alternative to removal of vegetation and re-planting.

D. Landscape Materials. Landscape materials include trees, shrubs, groundcover plants, turf grasses (e.g. grass sod or seed), and outdoor hardscape features, as described below:

1. **Natural Vegetation.** Natural vegetation shall be preserved or planted where practicable.

2. **Plant Selection.** A combination of deciduous and evergreen trees, shrubs, turf grasses, and groundcovers shall be used for all planted areas, the selection of which shall be based on local climate, exposure, water availability, and drainage conditions. As necessary, soils shall be amended to allow for healthy plant growth.

3. **"Non-native, invasive" plants,** as per Section 16.124.020, shall be prohibited.

4. **Hardscape features (i.e., patios, decks, plazas, etc.) may cover up to 50% of the required landscape area.** Swimming pools, sports courts and similar active recreation facilities may not be counted toward fulfilling the landscape requirement.

5. **Non-plant Groundcovers.** Bark dust, chips, aggregate or other non-plant groundcovers may be used, but shall cover no more than 50% of the area to be landscaped. "Coverage" is measured based on the size of plants at maturity or after five years of growth, whichever comes sooner.

6. **Tree Size.** Trees shall have a minimum caliper size of one and one-half inches or greater (two inches for required street trees) at time of planting.

7. **Shrub Size.** Shrubs shall be balled and burlapped and sized to fit in multi-gallon containers.

8. **Groundcover Size.** Groundcover plants shall be sized and spaced so that they grow together to cover a minimum of 30% of the underlying soil within two years.

9. **Significant Vegetation.** Significant vegetation preserved in accordance with Section 16.124.020 may be credited toward meeting the minimum landscape area standards. Credit shall be granted on a per square foot basis. The street tree standards of Section 16.124.040 may be waived when trees preserved within the front yard provide the same or better shading and visual quality as would otherwise be provided by street trees.

10. **Stormwater Facilities.** Stormwater facilities (e.g., detention/retention ponds and swales) shall be landscaped with water tolerant, native plants.

E. Landscape Design Standards. All yards, parking lots and required street tree planter strips shall be landscaped in accordance with the provisions of Sections 16.124.010 through 16.124.050. Landscaping shall be installed with development to provide erosion control, visual interest, buffering, privacy, open space and pathway identification, shading and wind buffering, and to help control surface water drainage and improvement of water quality, based on the following standards:

1. **Yard Setback Landscaping.** Landscaping shall satisfy the following criteria:

- a. Provide visual screening and privacy within side and rear yards while leaving front yards and building entrances mostly visible for security purposes.
- b. Use shrubs and trees as wind breaks, as appropriate.
- c. Retain natural vegetation, as practicable.
- d. Define pedestrian pathways and open space areas with landscape materials.
- e. Provide focal points within a development, such as signature trees (i.e., large or unique trees), hedges and flowering plants.

- f. Use trees to provide summer shading within common open space areas, and within front yards when street trees cannot be provided.
- g. Use a combination of plants for year-long color and interest.
- h. Use landscaping to screen outdoor storage and mechanical equipment areas, and to enhance graded areas such as berms, swales and detention/retention ponds.

2. **Parking Areas.** A minimum of eight percent of the combined area of all parking areas, as measured around the perimeter of all parking spaces and maneuvering areas, shall be landscaped. Such landscaping shall consist of an evenly distributed mix of shade trees with shrubs and/or groundcover plants. "Evenly distributed" means that the trees and other plants are distributed around the parking lot perimeter and between parking bays to provide a partial canopy. At a minimum, one tree per five parking spaces total shall be planted to create a partial tree canopy over and around the parking area. All parking areas with more than 20 spaces shall include landscape islands with trees to break up the parking area into rows of not more than 12 contiguous parking spaces. All landscaped areas shall have minimum dimensions of four feet by four feet to ensure adequate soil, water, and space for healthy plant growth.

3. **Buffering and Screening Required.** Buffering and screening are required under the following conditions:

- a. **Parking/Maneuvering Area Adjacent to Streets and Drives.** Where a parking or maneuvering area is adjacent and parallel to a street or driveway, a decorative wall (masonry or similar quality material), arcade, trellis, evergreen hedge, or similar screen shall be established parallel to the street or driveway. The required wall or screening shall provide breaks, as necessary, to allow for access to the site and sidewalk by pedestrians via pathways. The design of the wall or screening shall also allow for visual surveillance of the site for security. Evergreen hedges used to comply with this standard shall be a minimum of 36 inches in height at maturity, and shall be of such species, number and spacing to provide the required screening within one year after planting. Any areas between the wall/hedge and the street/driveway line shall be landscaped with plants or other groundcover. All walls shall be maintained in good condition, or otherwise replaced by the owner.
- b. **Parking/Maneuvering Area Adjacent to Building.** Where a parking or maneuvering area, or driveway, is adjacent to a building (other than a single-family, two-family, or three-family dwelling), the area shall be separated from the building by a raised pathway, plaza, or landscaped buffer not less than four feet in width. Raised curbs, bollards, wheel stops, or other design features shall be used to protect buildings from being damaged by vehicles.
- c. **Screening of Mechanical Equipment, Outdoor Storage, Service and Delivery Areas, and Automobile-Oriented Uses.** All mechanical equipment, outdoor storage and manufacturing, and service and delivery areas, shall be screened from view from all public streets and residential districts. Screening shall be provided by one or more the following: decorative wall (i.e., masonry or similar quality material), evergreen hedge, non-see-through fence, or a similar feature that provides a non-see-through barrier. Walls, fences, and hedges shall comply with the vision clearance requirements and provide for pedestrian circulation, in accordance with Chapter 16.120, Access and Circulation. (See Section 16.124.050 for standards related to fences and walls.)

F. Maintenance and Irrigation. The use of drought-tolerant plant species is encouraged, and may be required when irrigation is not available. Irrigation shall be provided for plants that are not drought-tolerant. If the plantings fail to survive, the property owner shall replace them with healthy plantings of shrubs, flowering plants and/or trees. All other landscape features required by this Code shall be maintained in good condition, or otherwise replaced by the owner.

G. Additional Requirements. Additional buffering and screening may be required for specific land uses, as identified by Division 2, and the City may require additional landscaping through the conditional use permit process (Chapter 16.220).

RESPONSE

A detailed Landscape Plan is included with this package showing (16.124.070. A / B / C / D / E / F / G):

- **52.2%** of the site is landscaped, thus exceeding the **20%** minimum requirement
- required perimeter landscaping of all parking spaces and maneuvering areas, including buffering and screening of parking or maneuvering areas, mechanical equipment and outdoor storage areas
- **12.1%** of vehicular use area is landscaped, thus exceeding the **8% minimum** of the combined area of all parking areas, as measured around the perimeter of all parking spaces and maneuvering areas is proposed to be landscaped
- stormwater facilities (e.g., detention/retention ponds and swales) are shown landscaped with water tolerant, native plants

See Exhibit B, Landscape L1 Site Plan and L2 Landscape Plan.

Therefore, this criterion has been met.

16.124.080 Street Trees

Street trees shall be planted for all developments that are subject to land division or site design review. Requirements for street tree planting strips are provided in Chapter 16.136, Public Facilities Standards. Planting of unimproved streets shall be deferred until the construction of curbs and sidewalks. Street trees shall conform to the following standards and guidelines:

A. Growth Characteristics. Trees shall be selected based on growth characteristics and site conditions, including available space, overhead clearance, soil conditions, exposure, and desired color and appearance. The following should guide tree selection:

1. Provide a broad canopy where shade is desired.
2. Use low-growing trees for spaces under utility wires.
3. Select trees which can be "limbed-up" where vision clearance is a concern.
4. Use narrow or "columnar" trees where awnings or other building features limit growth, or where greater visibility is desired between buildings and the street.
5. Use species with similar growth characteristics on the same block for design continuity.
6. Avoid using trees that are susceptible to insect damage, and avoid using trees that produce excessive seeds or fruit.
7. Select trees that are well-adapted to the environment, including soil, wind, sun exposure, and exhaust. Drought-resistant trees should be used in areas with sandy or rocky soil.
8. Select trees for their seasonal color, as desired.
9. Use deciduous trees for summer shade and winter sun.

B. Caliper Size. The minimum caliper size at planting shall be one and one-half inches diameter breast height (dbh) (two inches for required street trees), based on the American Association of Nurserymen Standards.

C. Spacing and Location. Street trees shall be planted within existing and proposed planting strips, and in sidewalk tree wells on streets without planting strips. The Community Development Director or Planning Commission may approve planting of street trees in other areas upon submission of a landscaping plan that demonstrates comparable (or greater) benefits to the neighborhood. Street tree spacing shall be based upon the type of tree(s) selected and the canopy size at maturity. In general, trees shall be spaced no more than 30 feet apart, except where planting a tree would conflict with existing trees, retaining walls, utilities and similar physical barriers.

Industrial Uses	Parking Spaces Required
Public utilities (gas, water, telephone, etc.)	1 space per 2 employees on the largest shift, plus 1 space per company vehicle; a minimum of 2 spaces is required

D. Soil Preparation, Planting and Care. The developer shall be responsible for planting street trees, including soil preparation, groundcover material, staking, and temporary irrigation for two years after planting. The developer shall also be responsible for tree care (pruning, watering, fertilization, and replacement as necessary) during the first two years after planting.

E. Assurances. The City shall require the developer to provide a performance and maintenance bond, or cash deposit, in an amount determined by the City-appointed engineer, to ensure the planting of the tree(s) and care during the first two years after planting.

RESPONSE

See a detailed Landscape Plan and Architectural Site Plan for the entire site showing compliance with all applicable portions of this code. Street trees required by code have been placed behind the sidewalk in the SE Dolphin Avenue half street improvement. Proposed street tree locations are shown on the paving plan and landscape plans (16.124.080 A / B / C / D / E). Specific design will be completed during the PIP permit process. See Exhibit B, Drawing Set.

Therefore, this criterion has been met.

CHAPTER 16.128 DESIGN STANDARDS: VEHICLE & BICYCLE PARKING

16.128.010 Purpose

The purpose of this chapter is to provide basic and flexible standards for development of vehicle and bicycle parking. The design of parking areas is critically important to the viability of some commercial areas, pedestrian and driver safety, the efficient and safe operation of adjoining streets, and community image and livability. Historically, some communities have required more parking than is necessary for some land uses, paving extensive areas of land that could be put to better use. Because vehicle parking facilities can occupy large amounts of land, they must be planned and designed carefully to use the land efficiently while maintaining the visual character of the community. This chapter recognizes that each development has unique parking needs by providing a flexible approach for determining parking space requirements (i.e., “minimum” and “performance-based” standards). This chapter also provides standards for bicycle parking because many people use bicycles for recreation, commuting, and general transportation. Children as well as adults need safe and adequate spaces to park their bicycles throughout the community.

16.128.020 Applicability

All developments in the City of Warrenton shall comply with the provisions of this chapter

16.128.030 Vehicle Parking Standards

At the time a structure is erected or enlarged, or the use of a structure or parcel of land is changed within any zone in the City, off-street parking spaces shall be provided in accordance with requirements in this section, chapter, and Code, unless greater requirements are otherwise established. The minimum number of required off-street vehicle parking spaces (i.e., parking that is located in parking lots and garages and not in the street right-of-way) shall be determined based on the standards in Table 16.128.030.A.

A. General Provisions.

1. Groups of four or more off-street parking spaces shall be served by a driveway or aisle so that no backing movements or other maneuvering within a street or right-of-way, other than an alley, will be required. Section 16.120.020 contains driveway opening and width standards.
2. Service drives or aisles to off-street parking areas shall be designed and constructed to facilitate the flow of traffic and to provide maximum safety to pedestrian, bicycle, and vehicular traffic on the site.

3. Service drives or aisles shall be clearly and permanently marked and defined through the use of bumper rails, fences, buildings, walls, painting, or other appropriate markers.
4. Fractional space requirements shall be counted as a whole space.
5. All parking lots shall be designed and constructed to meet the City standards of Section 16.120.020, this chapter, Chapter 16.136, and this Code.
6. Uses not specifically listed above shall furnish parking as required by the Community Development Director, who shall consider uses similar to those listed in Table 16.128.030.A and the Institute of Traffic Engineers *Parking Generation* as guides for determining requirements for other uses.
7. Parking spaces and parking areas may be used for transit related uses such as transit stops and park-and-ride/rideshare areas, provided minimum parking space requirements can still be met.
8. Parking areas that have designated employee parking and more than 20 automobile parking spaces shall provide at least 10% of the employee parking spaces (minimum two spaces) as preferential carpool and vanpool parking spaces. Preferential carpool and vanpool parking spaces shall be closer to the employee entrance of the building than other parking spaces, with the exception of ADA accessible parking spaces.

B. Parking Location and Shared Parking.

1. **Location.** Vehicle parking is allowed only on approved parking shoulders (streets), within garages, carports and other structures, or on driveways or parking lots that have been developed in conformance with this Code. Parking and loading areas shall not be located in required yards adjacent to a street unless otherwise specifically permitted in this ordinance. Side and rear yards that are not adjacent to a street may be used for such areas when developed and maintained as required in this chapter. See also Chapter 16.120, Access and Circulation.

C. Parking Stall Standard Dimensions and Compact Car Parking. All off-street parking stalls shall be improved to conform to City standards for surfacing, stormwater management, and striping. See Section 16.120.020 for parking lot construction standards. Up to 40% of the required spaces may be sized to accommodate compact cars. Standard parking spaces shall conform to the dimensions in Figure 16.128.030.C. Disabled person parking spaces shall conform to the standards (and dimensions) in this section.

D. Disabled Person Parking Spaces. The following parking shall be provided for disabled persons, in conformance with the Americans With Disabilities Act. Disabled parking is included in the minimum number of required parking spaces in this subsection.

Total Number of Parking Spaces Provided (per lot)	Total Minimum Number of Accessible Parking Spaces (60" & 96" aisles)	Van Accessible Parking Spaces with Minimum 96" Wide Access Aisle	Accessible Parking Spaces with Minimum 60" Wide Access Aisle
	Column A		
1 to 25	1	1	0

RESPONSE

There is no parking space maximum. Provided parking is located and developed in conformance with this Code (16.128.030).

Proposed employee parking consists of 16 parking spaces, less than 20 parking spaces thus preferential carpool and vanpool parking spaces have not been provided (16.128.030.A).

As discussed, the parking of construction equipment and service fleet vehicles can be designated separately from day to day employee parking and be considered outdoor storage.

All off-street parking stalls conform to City standards for surfacing, stormwater management, and striping.

The new building will have a total of 9 occupants. Each employee is assumed to have (1) company vehicle. The same employees that will occupy the office will utilize the warehouse. The warehouse will be utilized to house 4-5 fleet vehicles to protect from the harsh environment.

A total of twenty-two (22) staff parking spaces, twenty-six (26) fleet company vehicle parking spaces and five (5) long truck / trailer parking spaces are proposed.

REQUIRED STAFF PARKING SPACES

1 parking space per 2 employees + 1 parking space per 1 company vehicle:

$(1 \times 5) + (1 \times 9) = 14$ parking spaces

14 TOTAL STAFF PARKING SPACES ARE REQUIRED

PROVIDED STAFF PARKING SPACES

22 TOTAL STAFF PARKING SPACES ARE PROVIDED

- All proposed staff parking spaces are 10 feet in width and 20 feet in length. Parking spaces conform to the minimum dimensions shown in Figure (16.128.030.C). Six (6) staff parking spaces are proposed in the front of the building outside the secured perimeter fencing along the north west side of the property and sixteen (16) staff parking spaces are proposed within the secured perimeter fencing along the northern edge of the property.
- One (1) van accessible - disabled person parking space is proposed based on 1-25 total parking spaces provided (16.128.030.D). Thus, conforming to the standards (and dimensions) in this section and the OSSC – Table 1106.1.

See Exhibit B, Architectural Site Plan.

Therefore, this criterion has been met.

16.128.040 Bicycle Parking Requirements

A. All uses shall provide bicycle parking in conformance with the following standards which are evaluated during development review or site design review.

B. Number of Bicycle Parking Spaces. The minimum number of bicycle parking spaces required for uses is provided in Table 16.128.040.A.

Where an application is subject to Conditional Use Permit approval or the applicant has requested a reduction to the vehicle parking standard, pursuant to 16.128.030(A)(10), the City may require bicycle parking spaces in addition to those in Table 16.128.040.A.

Other uses	2 spaces per primary use or 1 per 10 vehicle spaces, whichever is greater	50% long term 50% short term
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Required parking spaces for Industrial use are 2 spaces per primary use or 1 per 10 vehicle spaces, whichever is greater.

- 50% long term
- 50% short term

C. Design and Location.

1. All bicycle parking shall be securely anchored to the ground or to a structure.
2. All bicycle parking shall be lighted for theft protection, personal security and accident prevention.

3. All bicycle parking shall be designed so that bicycles may be secured to them without undue inconvenience, including being accessible without removing another bicycle. Bicycle parking spaces shall be at least six feet long and two-and-one-half feet wide, and overhead clearance in covered spaces should be a minimum of seven feet. A five-foot aisle for bicycle maneuvering should be provided and maintained beside or between each row/rack of bicycle parking.
4. Bicycle parking racks shall accommodate locking the frame and both wheels using either a cable or U-shaped lock.
5. Direct access from the bicycle parking area to the public right-of-way shall be provided at-grade or by ramp access, and pedestrian access shall be provided from the bicycle parking area to the building entrance.
6. Bicycle parking shall not impede or create a hazard to pedestrians or vehicles and shall not conflict with the vision clearance standards of Chapter 16.132.
7. All bicycle parking should be integrated with other elements in the planter strip when in the public right-of-way.
8. Short-term bicycle parking.
 - a. Short-term bicycle parking shall consist of a stationary rack or other approved structure to which the bicycle can be locked securely.
 - b. If more than 10 short-term bicycle parking spaces are required, at least 50% of the spaces must be sheltered. Sheltered short-term parking consists of a minimum 7-foot overhead clearance and sufficient area to completely cover all bicycle parking and bicycles that are parked correctly.
 - c. Short-term bicycle parking shall be located within 50 feet of the main building entrance or one of several main entrances, and no further from an entrance than the closest automobile parking space.
9. Long-term bicycle parking. Long-term bicycle parking shall consist of a lockable enclosure, a secure room in a building onsite, monitored parking, or another form of sheltered and secure parking.

RESPONSE

Based on twenty-two (22) staff parking spaces proposed. Required bicycle parking spaces for Industrial use are two (2) spaces per primary use or (1 per 10) vehicle spaces, whichever is greater.

- 50% long term
- 50% short term

REQUIRED BICYCLE PARKING SPACES

22/10=2.3 bicycle parking spaces

3 TOTAL BICYCLE PARKINGS SPACES ARE REQUIRED

PROVIDED

2 bike racks = (4) bicycle parking spaces

4 TOTAL BICYCLE PARKING SPACES ARE PROVIDED

- One (1) Bicycle rack is proposed adjacent to the main office building entrance, consisting of two (2) short term bicycle parking spaces
- One (1) Bicycle rack is proposed along the breezeway, consisting of two (2) long term bicycle parking spaces. The bicycle rack is located behind a secured perimeter fence, in a well lite area and in view of security cameras.

See Exhibit B, Architectural Site Plan.

Therefore, this criterion has been met.

CHAPTER 16.132 DESIGN STANDARDS: CLEAR VISION AREAS

- A.** A clear vision area shall be maintained on the corner of property adjacent to the intersection of two streets, or adjacent to the intersection of a street and a railroad.
- B. A clear-vision area shall consist of a triangular area.** Two sides of the triangle are lot lines measured from the corner intersection of the street lot lines for a distance specified in this section or, where the lot lines have rounded corners, the lot lines extended in a straight line to a point of intersection and so measured. The triangle's third side is a line across the corner of the lot joining the non-intersecting ends of the other two sides.
- C. A clear-vision area shall contain no planting, fence, wall, structure, or temporary or permanent obstruction exceeding 36 inches in height measured from the top of the curb or, where no curb exists, from the established street centerline grade, except:**
1. Trees exceeding this height may be located in this area provided all branches and foliage are removed to a height of eight feet above the grade;
 2. Open-wire fencing that does not obscure sight more than 10% may be a maximum of 48 inches high.
- D. The following dimensional requirements govern clear vision areas:**
1. The minimum length of street sides of the clear vision triangle shall be 15 feet. See Figure 16.132.010.
 2. The minimum vision clearance area may be increased by the Community Development Director, City-appointed engineer, or Planning Commission upon finding that more sight distance is required (i.e., due to traffic speeds, roadway alignment, etc.).

RESPONSE

Vision clearance is provided at proposed driveways. Nothing over 30" in height is proposed to be placed within these vision clearance areas at each driveway (16.132.A / B / C / D). See Exhibit B, Civil and Landscape Plans. Therefore, this criterion has been met.

CHAPTER 16.136 DESIGN STANDARDS: PUBLIC FACILITIES STANDARDS

16.136.010 Purpose and Applicability.

A. Purpose. The purpose of this chapter is to provide planning and design standards for public and private transportation facilities and utilities. Streets are the most common public spaces, touching virtually every parcel of land. Therefore, one of the primary purposes of this chapter is to provide standards for attractive and safe streets that can accommodate vehicle traffic from planned growth, and provide a range of transportation options, including options for driving, walking, bus transit, and bicycling. This chapter implements portions of the City's Transportation System Plan.

16.136.020 Transportation Standards.

- A. Development Standards.** No development shall occur unless the lot or parcel abuts a public or private street, other than an alley, for at least 25 feet and is in conformance with the provisions of Chapter 16.120, **Access and Circulation, and the following standards are met:**
1. Streets within or adjacent to a development shall be improved in accordance with the Comprehensive Plan, Transportation System Plan, and the provisions of this chapter;
 2. Development of new streets (public or private), and additional street width or improvements planned as a portion of an existing street, shall be improved in accordance with this section, and public streets shall be dedicated to the applicable City, County or State jurisdiction;
 3. New streets and drives connected to a City collector or arterial street shall be paved; and

4. The City may accept a future improvement guarantee [e.g., owner agrees not to remonstrate (object) against the formation of a local improvement district in the future] in lieu of street improvements if one or more of the following conditions exist:

- a. A partial improvement may create a potential safety hazard to motorists or pedestrians,
- b. Due to the developed condition of adjacent properties it is unlikely that street improvements would be extended in the foreseeable future and the improvement associated with the project under review does not, by itself, provide increased street safety or capacity, or improved pedestrian circulation,
- c. The improvement would be in conflict with an adopted capital improvement plan, or
- d. The improvement is associated with an approved land partition on property zoned residential and the proposed land partition does not create any new streets.

B. Variances. Variances to the transportation design standards in this section may be granted by means of a Class 2 variance, as governed by Chapter 16.272, Variances. A variance may be granted under this provision only if a required improvement is not feasible due to topographic constraints or constraints posed by sensitive lands (see Chapter 16.156).

C. Creation of Rights-of-Way for Streets and Related Purposes. Streets shall be created through the approval and recording of a final subdivision or partition plat; except the City may approve the creation of a street by acceptance of a deed, provided that the street is deemed essential by the City Commission for the purpose of implementing the Transportation System Plan, and the deeded right-of-way conforms to the standards of this Code. All deeds of dedication shall be in a form prescribed by the City Attorney and shall name "the public," as grantee.

D. Creation of Access Easements. The City may approve an access easement established by deed when the easement is necessary to provide for access and circulation in conformance with Chapter 16.120, Access and Circulation. Access easements shall be created and maintained in accordance with the Uniform Fire Code, as amended.

E. Street Location, Width and Grade. Except as noted below, the location, width and grade of all streets shall conform to the Transportation System Plan and Comprehensive Plan, as applicable; and an approved street plan or subdivision plat. Street location, width and grade shall be determined in relation to existing and planned streets, topographic conditions, public convenience and safety, and in appropriate relation to the proposed use of the land to be served by such streets:

1. Street grades shall be approved by the City-appointed engineer in accordance with the design standards in subsection N of this section; and
2. Where the location of a street is not shown in an existing street plan (see subsection H of this section), the location of streets in a development shall either:
 - a. Provide for the continuation and connection of existing streets in the surrounding areas, conforming to the street standards of this chapter, or
 - b. Conform to a street plan adopted by the City, if it is impractical to connect with existing street patterns because of particular topographical or other existing conditions of the land. Such a plan shall be based on the type of land use to be served, the volume of traffic, the capacity of adjoining streets and the need for public convenience and safety.

F. Minimum Rights-of-Way and Street Sections. Street rights-of-way and improvements shall conform to the design standards in Table 16.136.010. A variance shall be required in accordance with Chapter 16.272 of this Code to vary the standards in Table 16.136.010. Where a range of width is indicated, the width shall be determined by the decision-making authority based upon the following factors:

1. Street classification in the Transportation System Plan or Comprehensive Plan;
2. Anticipated traffic generation;

3. On-street parking needs;
4. Sidewalk and bikeway requirements based on anticipated level of use;
5. Requirements for placement of utilities;
6. Street lighting;
7. Street tree location, as provided for in Chapter 16.124;
8. Protection of significant vegetation and wetland and riparian areas, as provided for in Chapters 16.124 and 16.156;
9. Safety and comfort for motorists, bicyclists, and pedestrians;
10. Street furnishings (e.g., benches, lighting, bus shelters, etc.), when provided;
11. Access needs for emergency vehicles; and
12. Transition between different street widths (i.e., existing streets and new streets), as applicable.

RESPONSE

Pre-application notes from the City state that the abutting SE Dolphin Avenue must be improved to city standards, without defining specifics. SE Dolphin Avenue is a "local road" on the functional classifications map of the Transportation System Plan (TSP, page 36). The City will also require adding two feet of pavement to bring the paved section road closer to its ultimate dimension of 36 feet. Per a meeting with Public Works during the due diligence effort, Public Works indicated that a 36-foot paving width would be required for this street.

However, the applicant would like to point out that there is a discrepancy with what is shown in the "local road" cross section standard, it indicates a 36-foot curb-to-curb pavement dimension in the table (TSP, page 41, Table 9) and when adding up the dimensions in the drawing (TSP, page 41, Figure 13) it shows a 40-foot width. A 40-foot width will not work with the existing catch basins already installed, which are set up for a 36-foot paving width. Currently, there is an existing 18-foot paving width to work with given the existing catch basin locations. Thus, the applicant requests approval of a 36-foot curb-to-curb pavement width.

Both the zoning code in this chapter (Table 16.136.010) and the Transportation System Plan (TSP, page 41, Table 9) require a five-foot planting strip on local roads, between the sidewalk and the vehicle travel lanes. However, the zoning code standard also says, "planting strips are optional depending on surrounding land use and available right of way" (Table 16.136.010, footnote 3). The applicant requests approval to not provide a planting strip for the following reasons:

There are some existing utility boxes that would be required to move for an offset sidewalk that adds cost to the project.

- A fairly steep grade up from the street to the site would need to be provided due to the natural grade on the property.
- The slope cannot be started until we get behind the sidewalk. An offset sidewalk would shorten the distance we have to work with and require the slope to be steeper.
- The existing sidewalk we are tying into at the south end is already curbside.

The pre-application notes mention the possibility of connecting to SE Warrior Way, a new east-west street on school district property to the north of the site. This right of way is entirely on the north side of that abutting property, approximately 50 feet north of the NW Natural property line, so there is no practical or reasonable way to connect to it directly. It should be noted that the new road has a sidewalk on its south side that will tie into SE Dolphin Avenue.

A half street improvement is required on SE Dolphin Avenue, and the proposed improvement is included on the paving plan. There are no other public access points proposed with this site plan. Minimum access standard for

spacing from SE Warrior Way is 25 feet. Driveway approaches and pedestrian access standards are located in Section 16.120. All proposed pedestrian facilities will meet current ADA requirements. The proposed half street improvement meets the local road design requirements as follows (16.136.020. A / B / C / D / F).

Standard Requirements

Type of Street: Local Road

Right-of-Way Width: 60 feet Provided (60 feet required)

Curb-to-Curb Pavement Width: 36 feet provided after future west side improvement (36 feet required)

Motor Vehicle Travel Lanes: 11 feet Provided (12 feet required)

Median / Flex Lane: None Provided (None Required)

Bike Lanes (both sides): None Provided (None Required)

On-Street Parking (both sides): 7 feet provided – unmarked on improved side (8 feet required)

Curb: Yes

Planting Strip: No planter strip is proposed in this section (5 feet required)

Sidewalks: 5.5 feet provided – curbside on improved side (5 feet minimum)

Street Lights: Yes

See Exhibit B Drawing Set.

Therefore, this criterion has been met.

16.136.040 Sanitary Sewer and Water Service Improvements

A. Sewers and Water Mains Required. Sanitary sewers and water mains shall be installed to serve each new development and to connect developments to existing mains in accordance with the City's construction specifications and the applicable Comprehensive Plan policies. Where City sanitary sewers are not physically or legally available to service the site, the applicant must demonstrate provisions for a suitable on-site disposal system permitted by DEQ prior to issuance of City permits. All development within a growth management (GM) zone, as identified on the official Warrenton Zoning Map, shall comply with the growth management zone standards of Chapter 16.112.

B. Sewer and Water Plan Approval. Development permits for sewer and water improvements shall not be issued until the City-appointed engineer has approved all sanitary sewer and water plans in conformance with City standards.

RESPONSE

The existing sanitary sewer system is located south of the property on the west side of Dolphin Road. The 8" sanitary sewer mainline is being extended across the entire frontage of the NW Natural property as part of the Warrenton Middle School project currently being permitted for construction this summer. As part of that project, a new 6" sanitary sewer lateral will be installed to serve the NW Natural development. All permitting for this sanitary sewer mainline extension is being completed by KPFF. Extension of the lateral to the NW Natural site will be permitted during the PIP permit process for the development.

There is an existing 12" water line running the entire length of the NW Natural property located on the west side of Dolphin Road. There is an existing 6" pvc conduit sleeve at Sta 14+10 that is proposed to be utilized to extend water service to the NW Natural development. The design and sizing of this waterline will be completed during the PIP and building permit phase of the project.

Therefore, this criterion has been met.

16.136.050 Storm Drainage Improvements.

A. General Provisions. The City shall issue a development permit only where adequate provisions for stormwater and floodwater runoff have been made in conformance with Chapter 16.140, Stormwater and Surface Water Management.

B. Accommodation of Upstream Drainage. Culverts and other drainage facilities shall be large enough to accommodate potential runoff from the entire upstream drainage area, whether inside or outside the development. Such facilities shall be subject to review and approval by the City-appointed engineer.

C. Effect on Downstream Drainage. Where it is anticipated by the City-appointed engineer that the additional runoff resulting from the development will overload an existing drainage facility, the City shall withhold approval of the development until provisions have been made for improvement of the potential condition or until provisions have been made for storage of additional runoff caused by the development in accordance with City standards.

D. Easements. Where a development is traversed by a watercourse, wetland, drainage way, channel or stream, the City may require a dedication of a stormwater easement or drainage right-of-way conforming substantially with the lines of such watercourse and such further width as will be adequate for conveyance and maintenance.

RESPONSE

See Chapter 16.140 for response.

Therefore, this criterion has been met.

16.136.060 Utilities.

A. Underground Utilities. All utility lines including, but not limited to, those required for electric, communication, lighting and cable television services and related facilities shall be placed underground, except for surface mounted transformers, surface mounted connection boxes and meter cabinets which may be placed above ground, temporary utility service facilities during construction, and high capacity electric lines operating at 50,000 volts or above. The following additional standards apply to all new land divisions, in order to facilitate underground placement of utilities:

1. The developer shall make all necessary arrangements with the serving utility to provide the underground services. Care shall be taken to ensure that all above ground equipment does not obstruct circulation and access aisles or impede vision clearance areas for vehicular traffic (Chapters 16.120 and 16.132);
2. The City reserves the right to approve the location of all surface mounted facilities;
3. All underground utilities, including sanitary sewers and storm drains installed in streets by the developer, shall be constructed prior to the surfacing of the streets; and
4. Stubs for service connections shall be long enough to avoid disturbing the street improvements when service connections are made.

B. Easements. Easements shall be provided for all underground utility facilities.

C. Exception to Undergrounding Requirement. The standard applies only to proposed land divisions and large-scale developments. An exception to the undergrounding requirement may be granted due to physical constraints, such as steep topography or existing development conditions.

RESPONSE

All utility lines including those required for electric, communication, lighting and cable television services and related facilities are proposed to be placed underground.

Therefore, this criterion has been met.

CHAPTER 16.140 DESIGN STANDARDS: STORMWATER & SURFACE WATER MANAGEMENT STANDARDS

- A.** To the extent practicable, all development must conform to the natural contours of the land and natural and pre-existing man-made drainage ways must remain undisturbed.
- B.** To the extent practicable, lot boundaries created by partition or subdivision must coincide with natural and pre-existing man-made drainage ways to avoid the creation of lots that can be built upon only by altering such drainage ways.

16.140.010 Natural Drainage System Maintained to Extent Feasible

16.140.020 Developments Must Drain Properly

16.140.030 Surface Water Management

RESPONSE

The site has been designed to utilize the existing drainage ravine at the southwest corner of the property. The historic drainage path then heads west through a public culvert under SE Dolphin Avenue. The site design includes a detention pond sized to process the 100-year storm event and keep the post development peak flow rate at or below the existing condition. Therefore, this development will not cause additional strain on the public system or cause change to the existing natural drainage ways.

Therefore, this criterion has been met.

16.140.040 Erosion and Sediment Control

- A.** For projects that disturb over one acre, applicants must apply to Oregon Department of Environmental Quality (DEQ) for a National Pollutant Discharge Elimination Control System (NPDES) 1200(C) permit.
- B.** Erosion and sediment control plans are required by the City as a component of the site plan for all plats and all projects which require site plan review. Erosion control plans must be designed to the specifications as outlined in this chapter.
- C.** Development of the land may not begin (and no building permits may be issued) until the City-appointed engineer approves the erosion control plan.
- D.** For purposes of this section, "disturb" means any use of the land by any person in any development, and/or road construction and maintenance that results in a change in the natural cover or topography that may cause or contribute to sedimentation. Sedimentation occurs whenever solid particulate matter, mineral or organic, is transported by water, air, gravity or ice from the site of its origin.

RESPONSE

Erosion and Sediment Control plans are included for Site Design Review. See Exhibit B, Civil Plans.

Therefore, this criterion has been met.

16.140.050 Stormwater System Design

RESPONSE

The stormwater system has been designed to collect run-off from all impervious surfaces shown on the site plan and directed to a new detention pond at the south end of the development. The pond has been designed to process the 100-year design storm event and limit the outflow to pre-development peak flow rate.

The outflow is routed to an existing ravine at the southwest corner of the site that routes water through an existing public culvert under Dolphin Avenue. This flow then routes west to the existing historic drainage route. See Exhibit E for Preliminary Stormwater Report.

Therefore, this criterion has been met.

16.140.060 Illegal Discharge of Materials into the Stormwater System

- A. Storm sewers constructed within the street will be sized by the developer's engineer and will consider all potential runoff requirements within the site and upstream of the site.
1. The storm sewer will be sized for 100-year design recurrence criteria for storm drainage facilities.
 2. The minimum size of storm sewers is eight inches in diameter.
 3. Spacing of catch basins along the street must conform to published engineering recommendations, which consider profile of the street and street width.
- B. On-site detention shall be required for new development where downstream deficiencies exist or are anticipated to exist. The square footage considers the total development of the property including the future potential impervious surface. Required design recurrence criteria for a commercial or residential storm drainage detention facility is a 10-year interval. Development that has less than 5,000 square feet of impervious surface is exempt from detention requirements.
- C. Pervious pavement, and pavement and roofs that drain to an infiltration facility will not be counted when sizing a detention facility to handle the stormwater design.

RESPONSE

The storm sewer within Dolphin Avenue is existing and was sized for the fully improved Dolphin Road and existing peak flow from adjacent properties.

The onsite storm management system has been designed to collect run-off from impervious areas and direct to a detention facility located on the south end of the property. The detention pond is designed to process the 100-year storm event to keep post construction peak flow into the public system at or below the pre-development condition. A full stormwater report with calculations is include with the Land Use Review submittal packet. See Exhibit E, Preliminary Stormwater Report.

Therefore, this criterion has been met.

CHAPTER 16.192 DESIGN STANDARDS: LARGE SCALE DEVELOPMENTS

16.192.010 Approval Process

A. **Large-Scale Development.** A development which is:

1. A planned unit development, manufactured dwelling park, recreational vehicle park, or campground; or
2. A multifamily housing development or row house/townhouse (single-family attached) development which within two calendar years will have 10 or more dwelling units; or
3. A commercial, industrial, public or institutional development which within two calendar years will use two or more acres of land or will have buildings with 10,000 square feet or more of floor area; or
4. Dependent on the expansion of City utility system(s) to service the development, including, but not limited to, development (or improvement) of transportation facilities or water and/or sewer mainline extensions.

B. **Review Type.**

1. **Type III:** "1, 2, 3, or combined 1 and 4."
2. **Type I:** "4." (Ord. 1175-A § 17, 2013)

RESPONSE

The applicant acknowledges the approval process for a Type III Site Plan Design Review Application. Therefore, this criterion has been met.

16.192.020 General Provisions

RESPONSE

The applicant acknowledges the general provisions.

Therefore, this criterion has been met.

16.192.030 Soil Suitability

RESPONSE

A geotechnical report has been prepared and submitted for review for this development by a registered geotechnical engineer that is licensed in the State of Oregon. No hazardous soils were found on site with liquid fraction soils present. See Exhibit F, Geotechnical Report.

Therefore, this criterion has been met.

16.192.040 Stormwater Management

RESPONSE

A preliminary stormwater management report has been prepared and submitted for review. See Exhibit E, Preliminary Stormwater Report

Therefore, this criterion has been met.

16.192.050 Utilities

RESPONSE

A detailed utility plan has been prepared and submitted for the proposed site utilities including water, sewer and transportation. See Exhibit B, Drawing Set.

Therefore, this criterion has been met.

16.192.060 Schools

RESPONSE

The subject site is not a school.

Therefore, this criterion is not applicable

16.192.070 Landscape Suitability

RESPONSE

The proposed development complies with the provisions of a landscape plan which is consistent with Chapter 16.124 of this code.

See Chapter 16.124 response above.

Therefore, this criterion has been met.

16.192.080 Signs

RESPONSE

The applicant does not propose to provide signage for the site.

Therefore, this criterion is not applicable

16.192.090 Additional Provisions

A. The City of Warrenton may charge the applicant additional fees, as necessary, to cover the cost of reviewing surveys, reports, plans, or construction methods required to comply with the provisions of this Code.

B. The City of Warrenton may require the owner or developer to post a performance bond to assure that improvements required to comply with the provisions of this section are completed in accordance with the plans and specifications as approved by the Community Development Director, and/or hearings body.

C. Proposals for large-scale developments shall be reviewed for consistency with all applicable sections of this Code prior to issuance of a development permit, including grading, filling, or building permits.

D. The standards of this section are required in addition to development review (Type I and II) and site design review (Type III) standards of Chapter 16.212

RESPONSE

The applicant acknowledges the additional provisions.

DIVISION 4 APPLICATIONS AND REVIEW PROCEDURES

CHAPTER 16.208 APPLICATIONS AND REVIEW PROCEDURES: TYPES OF APPLICATIONS AND REVIEW PROCEDURES

RESPONSE

A public facilities impact study is required for Type III applications. Applicant has prepared a Public Facilities impact study for the subject property and is included with this application. See Exhibit D, for Impact Study.

CHAPTER 16.212 APPLICATIONS AND REVIEW PROCEDURES: SITE DESIGN REVIEW

16.212.010 Purpose

The purpose of this chapter is to:

- A. Provide rules, regulations and standards for efficient and effective administration of site development review;
- B. Carry out the development pattern and plan of the City and its Comprehensive Plan policies;
- C. Promote the public health, safety and general welfare;
- D. Lessen or avoid congestion in the streets, and secure safety from fire, flood, pollution and other dangers;
- E. Provide adequate light and air, prevent overcrowding of land, and facilitate adequate provision for transportation, water supply, sewage and drainage;
- F. Encourage the conservation of energy resources; and
- G. Encourage efficient use of land resources, full utilization of urban services, mixed uses, transportation options, and detailed, human-scaled design.

16.212.020 Applicability

- A. Site design review shall be required for all new developments and modifications of existing developments, except for regular maintenance, repair and replacement of materials (e.g., roof, siding, awnings, etc.), parking resurfacing, and similar maintenance and repair shall be exempt. This applies to all development within the city limits of Warrenton. Site design review ensures compliance with the basic development standards of the land use district (building setbacks, lot coverage, maximum building height, etc.), as well as the more detailed design standards and public improvement requirements in Divisions 2 and 3.
- B. Site design review shall be conducted by the Community Development Director with public notice (Type II); or the Planning Commission with a public notice and hearing (Type III). (See Chapter 16.208 for review procedure.)

16.212.040 Site Design Review

A. Application Review Procedure.

1. **Site Design Review—Determination of Type II and Type III Applications.** Applications for site design review shall be subject to Type II or **Type III review**, based on the following criteria:
 - b. Commercial, **industrial**, public/semi-public, and institutional buildings (including building additions) with:

- ii. More than 10,000 square feet of gross floor area or developing two or more acres of land shall be reviewed as a Type III application.
- c. Developments involving the clearing and/or grading of two acres or more shall be reviewed as Type III applications.

B. **Application Submission Requirements.** All of the following information (subsections (B)(1) through (7) of this section) is required for site design review application submittal:

1. **Proposed Site Plan.** The site plan shall contain the following information:
 - a. The proposed development site, including boundaries, dimensions, and gross area.
 - b. Natural land features identified which are proposed to be removed or modified by the development, including modifications to existing drainage patterns.
 - c. The location and dimensions of all proposed public and private streets, drives, rights-of-way, and easements.
 - d. The location and dimensions of all existing and proposed structures, utilities, pavement and other improvements on the site. Setback dimensions for all existing and proposed buildings shall be provided on the site plan.
 - e. The location and dimensions of entrances and exits to the site for vehicular, pedestrian, and bicycle access.
 - f. The location and dimensions of all parking and vehicle circulation areas (show striping for parking stalls and wheel stops, as applicable), and proposed paving materials.
 - g. Pedestrian and bicycle circulation areas, including sidewalks, internal pathways, pathway connections to adjacent properties, and any bicycle lanes or trails.
 - h. Loading and service areas for waste disposal, loading and delivery.
 - i. Outdoor recreation spaces, common areas, plazas, outdoor seating, street furniture, and similar improvements.
 - j. Location, type, and height of outdoor lighting.
 - k. Location of mail boxes, if known.
 - l. Locations, sizes, and types of signs (shall comply with Chapter 16.144).
 - m. The Community Development Director may require studies or exhibits prepared by qualified professionals to address specific site features (e.g., traffic, noise, environmental features, site drainage, natural hazards, etc.).
 - n. The applicant's entire tax lot and the surrounding property to a distance sufficient to determine the location of the development in the City, and the relationship between the proposed development site and adjacent property and development. The property boundaries, dimensions and gross area shall be identified.
 - o. Identification of slopes greater than 10%.
 - p. The location, condition (paved, gravel unimproved, etc.) and width of all public and private streets, drives, sidewalks, pathways, rights-of-way, and easements on the site and adjoining the site.
 - q. Any areas identified as located in a designated floodplain and/or floodway.
 - r. Depict any wetland and riparian areas, streams and/or wildlife habitat areas.
 - s. Site features such as pavement, areas having unique views, and drainage ways, canals and ditches.
 - t. Any designated historic and cultural resources areas on the site and/or adjacent parcels or lots.
 - u. The location, size and type of trees and other vegetation on the property.
 - v. North arrow, scale, names and addresses of all property owners.

w. Name and address of applicant, project designer, engineer, architect, surveyor, and/or planner, if applicable.

2. **Architectural Drawings.** Architectural drawings shall be submitted showing the following information from subparagraphs a through c of this paragraph 2, and shall comply with Division 3:

- a. Building elevations with building height and width dimensions.
- b. Building materials, color and type.
- c. The name of the architect or designer.

3. **Preliminary Grading Plan.** A preliminary grading plan prepared by a registered engineer shall be required for developments which would result in the grading (cut or fill) of 1,000 cubic yards or greater. The preliminary grading plan shall show the location and extent to which grading will take place, indicating general changes to contour lines, slope ratios, slope stabilization proposals, and location and height of retaining walls, if proposed. Surface water detention and treatment plans may also be required, in accordance with Chapter 16.140.

4. **Landscape Plan.** A landscape plan is required and shall comply with Chapter 16.124.

5. Proposed sign(s) shall be required in conformance with the City's Sign Code (Chapter 16.144).

6. Copies of all existing and proposed restrictions or covenants.

7. Letter or narrative report documenting compliance with the applicable approval criteria contained in subsection C of this section.

C. **Review Criteria.** The Community Development Director shall make written findings with respect to all of the following criteria when approving, approving with conditions, or denying an application:

1. The application is complete, as determined in accordance with Chapter 16.208 and subsection B of this section.

2. The application complies with all of the applicable provisions of the underlying land use district (Division 2), including building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other special standards as may be required for certain land uses.

3. The applicant shall be required to upgrade any existing development that does not comply with the applicable land use district standards, in conformance with Chapter 16.276, Nonconforming Uses and Development.

4. The application complies with the applicable design standards contained in Division 3. (Ord. 1175-A § 21, 2013)

RESPONSE

Applicant has acknowledged criterion. A designated truck loading or unloading space is not provided because the proposed use is a private utility use consisting of a service yard with ample circulation space for large fleet vehicles to maneuver and park. Thus criterion B.1.h is not applicable.

The applicant proposes a trash enclosure that is separate from the main building and is not visible to the neighboring properties or public right-of-way. The trash enclosure consists of a 6-foot high chain link fence with black colored privacy slats, chain link swing gates, a continuous wheel stop for fence protection along the back side and a 6-inch concrete pad. The trash enclosure is obscured from the adjacent property by a chain link fence with privacy slats enclosure.

Proposed lighting used to illuminate the off-street parking areas is arranged to not project light rays directly upon any adjoining properties. See Exhibit B for off-street parking area lighting plan and type of light fixture proposed. See Exhibit C, for Photometric Analysis diagrams. A Site Plan Design Review drawing package is included with this application to reference all required criterion. See Exhibit B, Drawing Set.

Therefore, this criterion has been met.

CHAPTER 16.256 APPLICATIONS AND REVIEW PROCEDURES: TRAFFIC IMPACT STUDY

The city has indicated it will not require a traffic study in the pre-application notes. The objective thresholds for needing a traffic study do not appear to be triggered (WDC 16.256.030). Additionally, the project does not anticipate to increase site traffic volume generation by 300 average daily trips (ADT) or more.

RESPONSE

The applicant has provided a traffic profile accounting for anticipated site trips based on the proposed Resource Center operations.

The new building will have a total of 9 occupants generating a total of 30 trips per day. Number of anticipated truck deliveries are 1 every two weeks. This facility is not open to customers or the public. No additional vehicular trips to the site are anticipated. See Exhibit G, for Trip Generation Report.

APPLICANT IS ENCOURAGED TO COORDINATE PUBLIC IMPROVEMENTS WITH WARRENTON SCHOOL DISTRICT'S NEW CAMPUS

RESPONSE

Applicant has acknowledged this suggestion. Applicant has held a coordination meeting and coordinated public improvements with the Warrenton School District's new campus Project Manager.

Conclusion

The above narrative and the attached exhibits set forth evidence meeting all applicable standards and requirements set forth in the Warrenton Development code standards. Approval of the application will allow the Applicant to construct a well-designed NW Natural Resource Center. The Applicant therefore respectfully requests approval of the subject Type III Site Plan Design Review application.

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